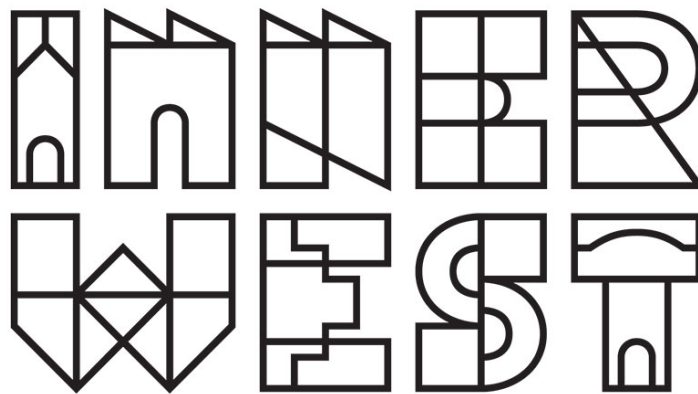


AGENDA



LOCAL TRAFFIC COMMITTEE MEETING

MONDAY 18 NOVEMBER 2024

11:00 AM

Function of the Local Traffic Committee

Background

Roads and Maritime Services (RMS) is legislated as the Authority responsible for the control of traffic on all NSW Roads. The RMS has delegated certain aspects of the control of traffic on local roads to councils. To exercise this delegation, councils must establish a local traffic committee and obtain the advice of the RMS and Police. The Inner West Council Local Traffic Committee has been constituted by Council as a result of the delegation granted by the RMS pursuant to Section 50 of the Transport Administration Act 1988.

Role of the Committee

The Local Traffic Committee is primarily a technical review and advisory committee which considers the technical merits of proposals and ensures that current technical guidelines are considered. It provides recommendations to Council on traffic and parking control matters and on the provision of traffic control facilities and prescribed traffic control devices for which Council has delegated authority. These matters are dealt with under **Part A** of the agenda and require Council to consider exercising its delegation.

In addition to its formal role as the Local Traffic Committee, the Committee may also be requested to provide informal traffic engineering advice on traffic matters not requiring Council to exercise its delegated function at that point in time, for example, advice to Council's Development Assessment Section on traffic generating developments. These matters are dealt with under **Part C** of the agenda and are for information or advice only and do not require Council to exercise its delegation.

Committee Delegations

The Local Traffic Committee has no decision-making powers. The Council must refer all traffic related matters to the Local Traffic Committee prior to exercising its delegated functions. Matters related to State Roads or functions that have not been delegated to Council must be referred directly to the RMS or relevant organisation.

The Committee provides recommendations to Council. Should Council wish to act contrary to the advice of the Committee or if that advice is not supported unanimously by the Committee members, then the Police or RMS have an opportunity to appeal to the Regional Traffic Committee.

Committee Membership & Voting

Formal voting membership comprises the following:

- one representative of Council as nominated by Council;
- one representative of the NSW Police from each Local Area Command (LAC) within the LGA, being Newtown, Marrickville, Leichhardt and Ashfield LAC's.
- one representative from the RMS; and
- State Members of Parliament (MP) for the electorates of Summer Hill, Newtown, Heffron, Canterbury, Strathfield and Balmain or their nominees.

Where the Council area is represented by more than one MP or covered by more than one Police LAC, representatives are only permitted to vote on matters which effect their electorate or LAC.

Informal (non-voting) advisors from within Council or external authorities may also attend Committee meetings to provide expert advice.

Committee Chair

Council's representative will chair the meetings.

Public Participation

Members of the public or other stakeholders may address the Committee on agenda items to be considered by the Committee. The format and number of presentations is at the discretion of the Chairperson and is generally limited to 3 minutes per speaker. Committee debate on agenda items is not open to the public.

AGENDA

1	Apologies	
2	Disclosures of Interest	
3	Confirmation of Minutes	Page
	Minutes of 21 October 2024 Local Traffic Committee	5
4	Matters Arising from Council's Resolution of Minutes	
5	Part A – Items Where Council May Exercise Its Delegated Functions	
ITEM		Page
LTC1124(1) Item 1	Illawarra Road, Wharf Street and Thornley Street, Marrickville – Traffic safety review of streets around Steel Park (Midjuburi-Marrickville Ward /Summer Hill Electorate /Inner West LAC)	32
LTC1124(1) Item 2	Station Street, Marrickville - Proposed bicycle Excepted signage (Midjuburi-Marrickville Ward / Summer Hill Electorate / Inner West PAC)	39
LTC1124(1) Item 3	Le Clos Lane, Marrickville - Proposed 30 Tonne weight limit on the Le Clos Lane Bridge (Midjuburi-Marrickville Ward / Summer Hill Electorate / Inner West PAC)	42
LTC1124(1) Item 4	Illawarra Road at Thornley Street, Marrickville – Proposed raised pedestrian crossing - Design Plan 10304 (Midjuburi-Marrickville Ward / Summer Hill Electorate / Inner West PAC)	80
LTC1124(1) Item 5	Illawarra Road at Warburton Street, Marrickville - Proposed 'Zig Zag' line marking for the pedestrian crossing (Midjuburi-Marrickville Ward / Summer Hill Electorate / Inner West PAC)	85
LTC1124(1) Item 6	St Peters Interchange Active Transport Works - Part 1 (Midjuburi-Marrickville Ward/Heffron Electorate/Inner West PAC)	87
LTC1124(1) Item 7	Abergeldie Street at Arlington Street, Dulwich Hill – Proposed installation of statutory 'No Stopping' continuous yellow line marking restrictions at the intersection (Djarrawunang - Ashfield Ward/ Summer Hill Electorate/ Inner West PAC)	103
LTC1124(1) Item 8	Carrington Street, Lilyfield - Construction Traffic Management Plan (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)	106
LTC1124(1) Item 9	Balmain Road, Lilyfield - Orange Grove Raised Pedestrian Crossing (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)	145
LTC1124(1) Item 10	Curtis Road, Balmain - Proposed Pedestrian Improvement Works Curtis Road, Balmain - Proposed Pedestrian Improvement Works (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)	150
LTC1124(1) Item 11	Emmerick Street, Lilyfield - Proposed New Kerb Blister (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)	159
LTC1124(1) Item 12	Darling Street Between Mort Street and Booth Street, Balmain - Road Occupancy - Anzac Day Dawn Service (Baludarri - Balmain Ward/ Balmain Electorate/ Leichhardt PAC)	162

LTC1124(1) Item 13 Audley Street, Audley Lane, Sadlier Crescent, Fisher Street, and eastbound parking lanes on New Canterbury Road Petersham – Temporary Full Road Closures For Special Event On Saturday 22 March 2025 – Petersham Festival Bairro Portuguese (Damum - Stanmore Ward / Newtown Electorate / Inner West PAC)	187
LTC1124(1) Item 14 Bruce Street, Ashfield - Proposed 'shared path' (Djarrawunang-Ashfield Ward/Summer Hill Electorate/Burwood PAC)	220
LTC1124(1) Item 15 Lackey Street & Smith Street (between Moonbie Street and Nowranie Street), Summer Hill-ENRC/2024/0062 Temporary Full Road Closure- 'Summer Hill Social' event on Sunday 9 March 2025 (Djarrawunang-Ashfield Ward/Summer Hill Electorate/Burwood PAC)	223

Parking Matters

LTC1124(1) Item 16 Mackey Park and Carrington Road Survey Area, Marrickville - Request for extension of M2 Residential Parking Scheme - Resident Parking Questionnaire Survey Results (Midjuburi-Marrickville Ward/Summer Hill Electorate/Inner West PAC)	252
LTC1124(1) Item 17 Tempe Reserve - Parking Study (Midjuburi-Marrickville Ward/Heffron Electorate/Inner West PAC)	280
LTC1124(1) Item 18 Leichhardt West - Proposed Residential Parking Scheme Expansion (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)	283

Late Items

Nil at time of printing.

6 Part B - Items for Information Only

ITEM	Page
LTC1124(1) Item 19 Traffic Committee Schedule 2025	289

7 Part C - Items for General Advice

Nil at the time of printing.

8 General Business

9 Close of Meeting

Minutes of Meeting held on 21 October 2024

Meeting commenced at 11:01 AM

ACKNOWLEDGEMENT OF COUNTRY BY CHAIRPERSON

I acknowledge the Gadigal and Wangal people of the Eora nation on whose country we are meeting today, and their elders past and present.

COMMITTEE REPRESENTATIVES PRESENT

Manod Wickramasinghe	IWC's Traffic and Transport Planning Manager (Chair)
Graeme McKay	Representative for Jo Haylen MP, Member for Summer Hill
Eleanor Nurse	Representative for Jenny Leong MP, Member for Newtown
Sgt Charles Buttrose	NSW Police – Leichhardt Police Area Command
Nina Fard	Transport for NSW (TfNSW)

NON VOTING MEMBERS IN ATTENDANCE

Shannon Burns	NSW Police – Leichhardt Police Area Command
Colin Jones	Representative for the Inner West Bicycle Coalition (IWBC)
Michael Takla	Representative for Transit Systems
Sunny Jo	IWC's Coordinator Traffic Engineering Services (North)
George Tsaprounis	IWC's Coordinator Traffic Engineering Services (South)
Jason Scoufis	IWC's Coordinator Traffic Studies & Road Safety
Ken Welsh	IWC's Coordinator Strategic Transport Planning
James Nguyen	IWC's Traffic Engineer
Christy Li	IWC's Business Administration Officer

VISITORS

Adrian King	Resident (Item 5)
David Benham	Resident (Item 11)
Brendan Jones	Resident (Item 11)
Barry Ireland	Resident (Item 11)
Benjamin Borger	Transport for NSW (TfNSW) (Item 15)

APOLOGIES:

Bill Holliday	Representative for Kobi Shetty MP, Member for Balmain
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DISCLOSURES OF INTERESTS:

Nil.

CONFIRMATION OF MINUTES

That the Minutes of the Local Traffic Committee held on Monday, 16 September 2024 be confirmed.

MATTERS ARISING FROM COUNCIL'S RESOLUTION OF MINUTES

Nil.

LTC1024(1) Item 1 Intersection of Evans Street and Nelson Street, Rozelle - Proposed kerb extensions (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)

SUMMARY

In October 2023, Council adopted the Balmain Local Area Traffic Management (LATM) Study for the Balmain precinct. One of the recommendations from the study was to further investigate the provision of kerb extensions at the intersection of Evans Street and Nelson Street, Rozelle. The proposal aims to improve traffic safety for pedestrian by shortening the crossing distance and enhancing visibility as well as enhancing driver sight distance for traffic exiting Nelson Street. Additionally, the work will provide opportunities for landscaping and kerbspace for on-street parking.

Officers Recommendation:

1. That the proposed kerb extensions in Evans Street, at Nelson Street, Rozelle be supported in principle and included in Council's Capital Works Program.
2. That the detailed design for the proposed kerb extensions be brought back to the Traffic Committee for consideration, including the results of Community Engagement.

DISCUSSION:

The Representative for the Inner West Bicycle Coalition advised there were not enough bike logos in the design. Council Officers advised they will take on that feedback and will consider the comments during the development of the detailed design.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

1. That the proposed kerb extensions in Evans Street, at Nelson Street, Rozelle be supported in principle and included in Council's Capital Works Program.
2. That the detailed design for the proposed kerb extensions be brought back to the Traffic Committee for consideration, including the results of Community Engagement.

For Motion: Unanimous

LTC1024(1) Item 2 Traffic Management Plan for the 2024 New Year's Eve Event (Baludarri-Balmain Ward/ Balmain Electorate/ Leichhardt Pac)

SUMMARY

As part of the annual New Years Eve celebrations, Council implements a Traffic Management Plan to support NSW Police operations in the Balmain Peninsula.

This report outlines the traffic management plan for the 2024 New Year's Eve event including temporary road closures, 'Bus Zones' and 'No Stopping' zones. Following last year's event, there was a need to modify the TMP to address the following matters:

- Lack of an emergency access route during the Balmain NYE closures for NSW Ambulances
- Safety issues with crowds conflicting with buses turning around at the roundabout at the intersection of Darling Street and Curtis Road.

These matters are detailed in the report and resulted in the addition of an emergency access route via Mackenzie Street, Rozelle and a Bus Loop via Booth Street/Darvall Street/Eaton Street, Balmain as an alternative to turning buses at the congested Darling Street/Curtis Road roundabout.

Officers Recommendation:

1. That the Traffic Management Plan (*Attachment 1*) detailing the traffic arrangements for the 2024 New Year's Eve be supported with the temporary modifications to bus stops being revised to also include:
 - a) On Eaton Street:
 - i. Install a temporary 27m and 41m length 'Bus Zone' between Darling Street and Gladstone Street on the east and west side respectively;
 - ii. Install a temporary 65m and 54m length 'Bus Zone' between Gladstone Street and Darvall Street on the east and west side respectively;
2. That the Traffic Management Plan (*Attachment 1*) be forwarded to Council's Parks and Streetscapes Coordinator, Transport Management Centre and the Major Events & Incidents Group (NSW Police).

DISCUSSION:

Council Officers tabled concerns from a local business owner regarding the previous New Year's Eve event and related closures advising that in the previous year, their restaurant had been fully booked however due to the closures, half of the bookings had been cancelled due to patrons not being able to enter the peninsula. Council Officers requested clarification on how the Police deal with rideshare vehicles wanting to enter on New Year's Eve.

The representative for the Leichhardt Police Area Command advised that there is difficulty with identifying carshare vehicles as there is no regulation on how a carshare vehicle is marked. It was noted that some carshare vehicles display the company they drive for on the back of their vehicle however this is not compulsory. The Representative for the Leichhardt Police Area Command advised they usually do not let rideshare vehicles into the peninsula after 7pm however there is the exception of rideshare vehicles occupied with a resident of the area or the elderly. It was noted that after 7pm, rideshare vehicles can drop off passengers and take bus services into Balmain. The Representative for the Leichhardt Police Area Command advised that VMS boards and notification letters allow for residents, and visitors to make alternative arrangements. There were also concerns tabled that ferry services were allegedly not running during prior years New Year's Eve.

Council Officers advised they will follow up with the relevant stakeholder to confirm the public transport arrangements.

Council Officers advised that there were amendments in the current Traffic Management Plan noting the proposal to install a temporary Bus Zone on Eaton Street to cater for the bus movements. It was noted that the Police have previously raised concerns with buses using

the Darling Street/Curtis Road roundabout to turn around and the potential risk of interaction with pedestrians and other buses. The temporary Bus Stop restrictions on Darling Street will remain unchanged as per previous years and will be reviewed for future events.

**** Please see Appendix 1 at the end of the minutes for updated TGS with temporary bus zones in Eaton Street that was tabled at the meeting****

Council Officers also noted that Mackenzie Street, will have barricades and a traffic controller to allow emergency vehicles through MacKenzie Street.

**** Please see Appendix 2 at the end of the minutes for updated TGS with traffic controller on Mackenzie Street that was tabled at the meeting****

The Representative for Transit Systems noted that the existing bus stop on Darling Street between Booth and Eaton Streets, were not highlighted in the reference provided on page 23 of the agenda.

The Representative for Transit Systems queried whether there will be a temporary bus stop on Crescent Street as per previous years. The Representative for the Leichhardt Police Area Command noted that they are still in discussion as to whether or not that bus stop will be used as a checkpoint this year and advised he will provide an update to the stakeholders outside the meeting. It was noted that signage was not previously installed at this check point and an amendment to the TMP would be required to have this included.

The Representative for Transit Systems questioned if it would still be possible to have that bus stop if the Police do not use that stop as a checkpoint. The Representative for the Leichhardt Police Area Command noted that would not be an issue on their end. Council Officers advised they will reach out to the Representative for Transit Systems for additional signage requirements on Robert Street.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

1. That the Traffic Management Plan (*Attachment 1*) detailing the traffic arrangements for the 2024 New Year's Eve be supported with the temporary modifications to bus stops being revised to also include:
 - a) On Eaton Street:
 - i. Install a temporary 27m and 41m length 'Bus Zone' between Darling Street and Gladstone Street on the east and west side respectively;
 - ii. Install a temporary 65m and 54m length 'Bus Zone' between Gladstone Street and Darvall Street on the east and west side respectively;
2. That the Traffic Management Plan (*Attachment 1*) be forwarded to Council's Parks and Streetscapes Coordinator, Transport Management Centre and the Major Events & Incidents Group (NSW Police).

For Motion: Unanimous

**LTC1024(1) Item 3 Intersection of Glassop Street and White Street, Balmain -
Proposed Treatment of Intersection for Improved Sight Lines
(Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)**

SUMMARY

Council has received concerns from the community regarding insufficient visibility at the intersection of Glassop Street and White Street, Balmain.

This location formed part of the Rozelle North precinct where a Local Area Traffic Management (LATM) study was undertaken in 2022. The study recommended the installation of kerb extensions at the location at a medium priority, which was adopted by Council at the meeting held on 13 September 2022. As this project was not an immediate priority under the Council's Capital Works Program, an interim treatment has been proposed to address the safety issue.

The proposal sees the installation of painted kerb buildouts at the corners of Glassop Street at its intersection with White Street, and the introduction of 'Give Way' control for the White Street leg. The proposal aims to improve visibility at this intersection by bringing the give way line forward toward the Glassop Street travel lanes.

Officers Recommendation:

That the following treatment at the intersection of Glassop Street and White Street, Balmain be approved as follows:

- a) Linemark chevron islands on Glassop Street at the intersection of Glassop Street and White Street;
- b) Install a 'Give Way' (R1-2A) sign and supporting give way lines (TB & TB1) across White Street at Glassop Street; and
- c) Install 10m length double barrier lines (BB) in White Street commencing from Glassop Street.

DISCUSSION:

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That the following treatment at the intersection of Glassop Street and White Street, Balmain be approved as follows:

- a) Linemark chevron islands on Glassop Street at the intersection of Glassop Street and White Street;
- b) Install a 'Give Way' (R1-2A) sign and supporting give way lines (TB & TB1) across White Street at Glassop Street; and
- c) Install 10m length double barrier lines (BB) in White Street commencing from Glassop Street.

For Motion: Unanimous

LTC1024(1) Item 4 Lilyfield Road, Lilyfield - Proposed Raised Pedestrian and Bicycle Crossings (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)

SUMMARY

Detailed design plans for the two Raised Pedestrian and Bicycle crossings along Lilyfield Road has been prepared to improve pedestrians and bicycles connectivity to and from the Rozelle Parklands. There is ongoing work undertaken by Council to finalise the Plan of Management and revising the Masterplan for Rozelle Parklands and Eastern Park, which amongst many elements, includes pedestrian accessibility and safety in the vicinity at these parks. With the recognized need from the community to improve road safety, Council has expediated the design and development of two key crossing facilities in advance of the finalization of the Masterplan and the Lilyfield Road Cycleway design. The proposed two facilities will address key concerns from the community and Bicycle User Groups regarding the need to provide a safe interface point from Lilyfield Road into Rozelle Parklands and vice versa.

Officers Recommendation:

That the attached detailed design plan (Attachment 1) for the proposed Raised Pedestrian and Bicycle crossings on Lilyfield Road at Ryan Street and Lilyfield Road approximately 80m east of Denison Street, Lilyfield be approved.

DISCUSSION:

Council Officers noted that they have received a number of concerns regarding pedestrian safety from pedestrians and cyclists accessing Rozelle Parklands from Lilyfield Road and the residential areas north of the Rozelle Parklands. It was noted that was an urgent matter and that Council will be looking to expedite the installation process where possible.

Transport for NSW had noted that the threshold width at 5.6m would need to be at 6m minimum. Council Officers also noted that there the 'Give Way' lines required relocation to be immediately adjacent to the bicycle crossing linemarking and relocation of the 'Give Way' signposting to be on the same stem as the Pedestrian Crossing signage which would require the threshold to be 6.3m wide.

Council Officers requested that the Representative for Transport for NSW sends some examples of similar treatments done at the City of Sydney Council to ensure the treatment is consistent with what has been implemented around Sydney. Council Officers acknowledged that this was a relatively new treatment and that refinements to the standard design may need to be made overtime.

The Committee members agreed with the amended recommendation.

COMMITTEE RECOMMENDATION:

That the attached detailed design plan (Attachment 1) for the proposed Raised Pedestrian and Bicycle crossings on Lilyfield Road at Ryan Street and Lilyfield Road approximately 80m east of Denison Street, Lilyfield be approved subject to the design being amended to: widen the threshold width to 6.3m; relocate 'Give Way' lines to be immediately adjacent to the bicycle crossing; and relocation of the 'Give Way' signposting to be on the same stem as the 'Pedestrian Crossing' signage.

For Motion: Unanimous

LTC1024(1) Item 5 Moyes Street, Marrickville - Proposed 'No Stopping' relocation (Marrickville-Midjuburi/Summer Hill Electorate/Inner West PAC)

SUMMARY

This report outlines current parking and road safety issues on Moyes Street, Marrickville between Greenbank and Jersey Streets. It outlines key findings from an assessment carried out and proposes parking changes to improve safety.

Officers Recommendation:

That the existing 'No Stopping' restriction on the western side of Moyes Street, Marrickville (located approximately 17m from the bend) between Greenbank and Jersey Streets, be extended a further 13.5m (to the power pole adjacent to the driveway for 37 Moyes Street) to improve road safety in the vicinity of the bend.

DISCUSSION:

Public Speaker Adrian King entered the meeting at 11.03am.

Mr King opposed the recommendation advising that Moyes Street is a common rat-run and that the parked cars on Moyes Street assist with traffic calming. Mr King noted that there is high pedestrian and cyclist traffic from McNeilly Park and raised concerns that (1) the extension of the 'No Stopping' restriction and removal of parking will allow for vehicles to travel at higher speeds jeopardising pedestrian and cyclist safety; (2) vehicles will still have to cross over to the other side of the road to pass through due to the narrowness of Moyes Street; and (3) the potential increase in speeds from vehicles due to the extension of the 'No Stopping' zone will increase the risk to pedestrians and cyclists who use that intersection.

Public Speaker Adrian King left the meeting at 11.09am.

Council Officers noted that the proposed relocation of the existing 'No Stopping' restriction in Moyes Street will improve sightlines for pedestrians and removes the current issue with road users crossing onto the wrong side of the road just before a bend. It was also noted that the proposed relocation of the existing 'No Stopping' restriction will formalise the typical traffic rules. Council Officers advised that if there are parked cars on both sides of Moyes Street, it reduces the road width for passing cars to 2.7 metres for both directions which is not recommended. It was also noted that 2.7metres in width would not be wide enough for emergency vehicles. Council Officers noted that having parking on the eastern side of the street is the current typical parking arrangement and will allow for 4.7 meters in width of road which allow for two vehicles to pass. The Committee further noted that cyclist safety will improve under the proposal.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That the existing 'No Stopping' restriction on the western side of Moyes Street, Marrickville (located approximately 17m from the bend) between Greenbank and Jersey Streets, be extended a further 13.5m (to the power pole adjacent to the driveway for 37 Moyes Street) to improve road safety in the vicinity of the bend.

For Motion: Unanimous

LTC1024(1) Item 6 Pilgrim Avenue, Marrickville - Proposed 'No Parking' restrictions on the inner bend to improve access (Midjuburi - Marrickville Ward / Summer Hill Electorate / Inner West PAC)

SUMMARY

Currently large vehicles, including Council Waste Services vehicles, are having difficulty in manoeuvring around the bend in Pilgrim Avenue, Marrickville due to vehicles being parked near the bend therefore Council is proposing to install a 20-metre length of 'No Parking' restrictions on the inner side of the bend in Pilgrim Avenue, Marrickville to improve access in the street.

Officers Recommendation:

That a 20-metre length 'No Parking' zone be installed on the inner bend in Pilgrim Avenue, Marrickville (eastern side starting from the boundary of No's 2 and 3 Pilgrim Avenue and extending 20m north around the bend) in order to improve access in the street.

DISCUSSION:

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That a 20-metre length 'No Parking' zone be installed on the inner bend in Pilgrim Avenue, Marrickville (eastern side starting from the boundary of No's 2 and 3 Pilgrim Avenue and extending 20m north around the bend) in order to improve access in the street.

For Motion: Unanimous

LTC1024(1) Item 7 Church Street, Ashfield, between Knocklayde Street and Alt Street-Proposed Raised Thresholds & Kerb Blister Islands (Traffic Calming)(Gulgadya-Leichhardt Ward/Summer Hill Electorate/Burwood PAC).

SUMMARY

Council is planning to provide traffic calming measures to improve safety in Church Street, Ashfield by constructing new raised thresholds & landscaped kerb blisters along Church Street near Knocklayde St, Goenka St, Tawa St & Taringa Lane. The traffic calming proposal aims to slow traffic & improve road safety in this part of Church Street and helps address concerns relating to pedestrian safety and driver behaviour.

Officers Recommendation:

That the amended detailed design plans (10284-A) sheets 1-4 to propose raised concrete thresholds and kerb blister islands with associated line and marking in Church Street, Ashfield, between Knocklayde Street and Alt Street, as shown in Attachment 1 be approved.

DISCUSSION:

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That the amended detailed design plans (10284-A) sheets 1-4 to propose raised concrete thresholds and kerb blister islands with associated line and marking in Church Street, Ashfield, between Knocklayde Street and Alt Street, as shown in Attachment 1 be approved.

For Motion: Unanimous

LTC1024(1) Item 8 Ramsay Street & Dalhousie Street, Haberfield - 40km/h High Pedestrian Activity Area (HPAA) (Gulgadya-Leichhardt Ward/Summer Hill Electorate/Burwood PAC)

SUMMARY

Council is planning to improve safety for all road users in Ramsay Street and Dalhousie Street, Haberfield by establishing a 40km/h High Pedestrian Activity Area (HPAA) for the core commercial and retail areas of Haberfield.

The proposal feature a number of linemarking treatments to indicate a reduced 40km/h speed limit, with accompanying entry, repeater and exit HPAA signs. Additionally, a landscaped kerb blister island will be constructed at the intersection of Dalhousie Street and Winchcombe Avenue which will complement the proposed treatments for a lower speed road environment.

The project will not impact on any legal on-street parking spaces in the area.

Officers Recommendation:

1. That the attached detailed design plan (Design Plan No.10312) for the proposed 40km/h High Pedestrian Activity Area (HPAA) treatments in Ramsay Street, Dalhousie Street, Gillies Avenue, Rawson Street, Dickson Street, Winchcombe Avenue, St Davids Road, and Kingston Street, Haberfield be approved.
2. That it should be noted that Council has submitted a Traffic Management Plan for the proposed 40km/h HPAA to Transport for NSW approval.

DISCUSSION:

Council Officers noted that Transport for NSW had requested Council to remove the proposed 50km area signs which advises the default speed limit. Council Officers advised they were happy to amend the plans accommodate the request.

**** Please see Appendix 3 at the end of the minutes for amended plans with the 50km/h speed limit signs removed that were tabled at the meeting ****

The Representative for the Member for Summer Hill questioned why this proposal for 40km/h zones were undertaken as part of the InnerWest@40 project. Council Officers advised that Ramsay Street is a Regional Road and therefore does not fall under the criteria for the InnerWest@40 study.

The Representative for the Member for Summer Hill questioned whether the treatments proposed will remain once the suburb converts to 40km/h. Council Officers advised the treatments will remain.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

1. That the attached detailed design plan (Design Plan No.10312) for the proposed 40km/h High Pedestrian Activity Area (HPAA) treatments in Ramsay Street, Dalhousie Street, Gillies Avenue, Rawson Street, Dickson Street, Winchcombe Avenue, St Davids Road, and Kingston Street, Haberfield be approved.
2. That it should be noted that Council has submitted a Traffic Management Plan for the proposed 40km/h HPAA to Transport for NSW approval.

For Motion: Unanimous

LTC1024(1) Item 9 Clissold Street, between Queen Street and Victoria Street, Ashfield-Proposed removal of existing at-grade crossing and provide a new raised pedestrian(zebra) crossing- Deffered Item for additional information (Djarrawunang-Ashfield Ward/Summer Hill Electorate/Burwood PAC)

SUMMARY

The detailed design plan for this proposed treatment was raised at the Local Traffic Committee (LTC) on the 16 September 2024 and deferred to provide additional information on the concerns raised by the representatives from Transit Systems Australia (TSA) and TFNSW on the existing Bus Stops and reduced 'No Stopping' length on approach to the new crossing. This additional information is provided below under Discussion-Additional information in deferral of the item. It should be noted that the remainder of the report is that which was presented at the September 2024 LTC meeting and is presented for information and context with regards to the recommendation.

Council at its meetings on the 18 March 2024 (through its Traffic Committee 11 December 2023) approved in principle, subject to detailed design, a series of proposed pedestrian (zebra) crossings and kerb extension treatments (under concept) for improved pedestrian and road safety around and near to the Cardinal Freeman (Retirement) Village, Ashfield.

This report describes and shows the detailed design plan of one of the proposed treatments, i.e. install a proposed new raised pedestrian (zebra) crossing in Clissold Street, just east of gate No.11 to the Cardinal Freeman Village (near William Street), and the removal of the existing at-grade pedestrian crossing further east in Clissold Street, between Queen Street and Victoria Street. This work is programmed and envisaged to be constructed in the 2024/2025 financial year, subject to funding.

Officers Recommendation:

That the detailed design plan (10296) for the proposed new raised pedestrian (zebra) crossing in Clissold Street, just east of gate No.11 to the Cardinal Freeman Village (near William Street), and the removal of the at-grade pedestrian crossing further east in Clissold Street, between Queen Street and Victoria Road, Ashfield, as shown in Attachment 1 be approved.

DISCUSSION:

The Representative for the Member for Summer Hill queried if comments vision impaired residents were taken into consideration for the design. Council Officers advised this crossing

had been treated with tactiles on the lead up to the crossing and the discussions with the designers have concluded that there were not many additional treatments that could be implemented due to the nature of the raised pedestrian crossing and the narrowness of the road and footpath.

The Representative for the Member for Summer Hill asked if Council liaises with the Guide Dogs Australia for feedback on their designs. Council Officers advised that they do liaise with the Guide Dogs Australia however they did not liaise with them for this particular design. Council Officers did indicate that the mobility and inclusion coordinator will be requested to provide comments on pedestrian crossing designs in future.

The Representative for Transit Systems questioned whether there will be bus diversions from Clissold Street when construction works begin. Council Officers advised they will liaise with the Project Manager for installation of the proposed pedestrian crossing on the issue of timing of works and advised that they will pass on the details of the Representative for Transit Systems to the Project Manager so the Project Manager can reach out to discuss the timing of works and proposed arrangements.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That the detailed design plan (10296) for the proposed new raised pedestrian (zebra) crossing in Clissold Street, just east of gate No.11 to the Cardinal Freeman Village (near William Street), and the removal of the at-grade pedestrian crossing further east in Clissold Street, between Queen Street and Victoria Road, Ashfield, as shown in *Attachment 1* be approved.

For Motion: Unanimous

LTC1024(1) Item 10 Victoria Street, Ashfield - Installation of a 'Keep Clear' zone at the front of Ashfield Police Station (Djarrawunang-Ashfield Ward/Summer Hill Electorate/Burwood PAC)

SUMMARY

This report outlines current emergency vehicle accessibility issues at Victoria Street between Liverpool Road and Norton Street and proposes the appropriate road treatment to improve police operations of Ashfield Police Station.

Officers Recommendation:

That the installation of a 'Keep Clear' zone on Victoria Street, Ashfield at the main driveway for Ashfield Police Station be approved.

DISCUSSION:

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That the installation of a 'Keep Clear' zone on Victoria Street, Ashfield at the main driveway for Ashfield Police Station be approved.

For Motion: Unanimous

LTC1024(1) Item 11 Proposed EV Kerbside Charging Locations (All Wards / All Electorates / All PACs)

SUMMARY

Council is committed to supporting and encouraging the use of electric vehicles (EVs) in the Inner West. Consequently, Council chose three kerbside public charging providers, as partners, through a Request for Quotation (RFQ) process. Subsequently, Council supported these partners in successfully applying for State Government funding.

Council's charging partners secured grant funding to roll-out 136 public EV charging ports; 95 of which are "dedicated" with signs and line marking, across our community by mid-April 2025.

The proposed initial network of EV kerbside charging port locations was publicly exhibited in July and August 2024, to gain community feedback. The overall proposal received 84% support from the community. In response to community feedback, on specific locations, the number of EV charging ports with dedicated kerbside spaces is proposed to be reduced from 95 to 81.

The community consultation included both dedicated and undedicated charging locations so that the community could understand the overall charging network proposed. In considering this it is important to note that; as chargers without dedicated spaces do not alter kerbside conditions and would be installed on existing Ausgrid infrastructure, they could be installed without Council approval. Their installation could be carried out under the Transport and Infrastructure State Environmental Planning Policy (SEPP).

Proposed signs and line marking plans for the chargers with dedicated kerbside spaces are included in Attachment 1.

Officers Recommendation:

1. That the EV charging sites identified in Table 3 be supported for installation as per the signage and line marking plans provided in Attachment 1.
2. That the EV charging sites identified in Table 2 be supported subject to targeted engagement and TfNSW approval as required.

DISCUSSION:

Public Speaker David Benham entered the meeting at 11.09 am.

Mr Benham was opposed to the proposed EV charging site for Alice Street, Newtown noting that there is currently inadequate on-street parking on Alice Street, and the installation of the EV charging site will add to the already difficult parking situation. Mr Benham question why the proposed charging site was a 'dedicated' space rather than a 'non dedicated' space which is proposed for some other areas. Mr Benham noted that there is currently a '2P' Resident Parking Scheme in the area however the scheme is rarely enforced further adding to the parking difficulties in the area. Mr Benham also noted that the report did not mention the objections raised by himself and his neighbour and only included a summary of submissions submitted to Your Say. Mr Benham also noted that the responses provided in the report did not indicate whether the respondents were residents or visitors. Mr Benham advised that none of his neighbouring residents own an electric vehicle and that the EV charging site will not be fulfilling the needs of the residents and will only be beneficial for visitors in the area. Mr Benham also noted that the report proposed that overstaying users

would be charged idle fees however he did not believe that would be a deterrent for users unless the idle fees were exorbitant.

Public Speaker David Benham left the meeting at 11.17 am.

Public Speaker Brendan Jones entered the meeting at 11.18 am.

Mr Jones supported the proposed recommendation for installation of EV charging sites advising that the number of battery electric vehicles is rapidly growing in the Inner West Council area and that as of 30 September 2024, there were 2,740 electric vehicles registered in the Inner West Council area. Mr Jones emphasised the importance of EV charging sites in the Inner West area due to the rapid growth of electric vehicles and the lack of off-street parking and charging opportunities within the area. Mr Jones advised that the parking restriction signage needs to be made clear noting that the current proposed signage is ambiguous. Mr Jones suggested that dedicated charging spaces should have the pavement marked and painted before the charging stations become available for use by the public, noting that the painted pavement acts as a visual aid and dramatically reduces instances of non-electric vehicles parking in dedicated EV spots and was more effective than just signposting and linemarking alone. Mr Jones also noted the importance of Council monitoring the utilisation and turnover of 'dedicated' versus 'opportunity' EV charging spaces as the data will assist with providing feedback on the usage of spots and whether or not the electric vehicles are able to get a chance to use the 'opportunity' charging spaces to charge. Mr Jones also suggested that Council do a letterbox drop to notify all residents in all residents in the vicinity of each dedicated kerbside charging location, once they are operational.

Public Speaker Brendan Jones left the meeting at 11.22 am.

Public Speaker Barry Ireland entered the meeting at 11.23 am.

Mr Ireland proposed to relocate the proposed EV charging site at Rowntree Street, Birchgrove to Spring Street, Birchgrove near St Johns Church. Mr Ireland queried if Council had conducted site visits to the proposed sites. He also noted that he was unable to find his response in the report and was advised by a Council staff member that the response in the report were generated from Your Say and were anonymous. Mr Ireland noted there was an addendum to the report with responses from residents and questioned if those responses were taken into consideration.

Council Officers advised that Council had gone out to review the site along with Council's traffic engineers, and representatives from the service provider and all contentious issues raised were discussed with the stakeholders involved. Council Officers advised in relation to the addendum, Council's engagement team has advised that it was Council policy to not publish verbatim emails to prevent sensitive information being disclosed. Council Officers noted that the strategic planning team have read all the residents' responses and advised those responses were taken into consideration when writing the outcomes report.

Mr Ireland also noted that there are vehicles that park dangerously around Rowntree Street, such as parking in the 'No Stopping' zone, parking within 5 meters of the intersection as well as across his driveway hindering the access to and from his property and expressed his concern of the proposed EV charging site exacerbating the existing issues. Mr Ireland noted his concerns regarding the proposed site's usage of electricity and advised that there is a supply issue of electricity on the street noting that him and his neighbours experience flickering lights and expressed his concern regarding the increased demand of electricity further adding to the existing problem.

Council Officers advised they have spoken to Ausgrid regarding the electricity issue and was

advised that the network in the area was being investigated for potential future upgrades.

Public Speaker Barry Ireland left the meeting at 11.28 am.

Council Officers tabled 2 additional submissions from 2 separate residents. One resident noted their concerns regarding the placement of charging infrastructure in front on residential dwellings which may lead to potential noise complaints, property damage and verbal/physical disputes. It was noted that there were concerns of loss of parking amenity in high demand areas, and that internationally, similar instances of charging ports being located outside residential properties did not work. The resident suggested that if chargers were to be placed outside of residential dwellings they should operate on a curfew and that chargers' placements be considered for relocation to areas with less residential properties.

Council Officers advised that chargers with dedicated spaces would have a curfew to provide opportunities for non- electric vehicles to use the space outside of curfew hours. Council Officers also noted that in the 2023 Electric Vehicle Encouragement strategy, it was noted that it was best to have the chargers placed outside of parks and community buildings with the last choice being residential properties, however, in areas where there are only residential buildings, there are no other options than to place chargers outside of residential dwellings.

Council Officers tabled in the addition submission from a resident requesting opportunities be provided to private individuals to charge their vehicles kerbside rather than providing public charging sites. The resident noted the economic and environmental benefits of being able to charge from solar energy and that it fits with Council's sustainability and net zero policies. It was also noted that adding public charging is beneficial for residents in apartments and similar but for homeowners, usually with limited off-street parking, the ability to charge safely from solar energy on the street would be greatly beneficial.

The Representative for the Member of Newtown questioned what the decision-making process was when determining whether a car space was going to be 'dedicated' or 'non-dedicated.'

Council Officers advised that the decisions were made by the providers themselves. It was noted that there was only one supplier that was putting in 'non-dedicated' spaces in conjunction with 'dedicated' spaces in order to gather data as to what type of space was more appropriate for the Inner West Council area.

Council Officers noted that these charging ports received grant funding and that the selection criteria for the grants included, capacity of electrical grid, areas with projected high EV uptake, areas with limited opportunity for on-site charging as well as safety and accessibility requirements.

Council Officers noted that the report provided to the Local Traffic Committee dealt only with the proposed 'dedicated' spaces as 'non-dedicated' or 'opportunity' chargers without dedicated spaces do not have any direct impact on kerbside allocation and under the Transport and Infrastructure State Environmental Planning policy, chargers can be installed without Council consent.

The Representative for the Member of Summer Hill questioned if a booking system will be in place for users to book in time at a dedicated space. Council Officers advised that the providers are currently setting up their platforms for their booking system to allow for pre-bookings. It was noted that the providers are examining systems to send push notifications to users to advise when their car is around 90% charge and prompting them to know they will have to return to their car shortly to avoid a overstay fee.

The Representative for the Member of Summer Hill questioned if Council would receive revenue from the charging spaces. Council Officers advised Council will not be receiving

revenue on kerbside charging spaces until the providers hit their breakeven point. Once the agreed breakeven point has been reached, Council will start negotiations on a potential leasing scheme.

The Representative for the Inner West Bicycle Coalition noted the public speaker's suggestion to paint the pavement to indicate the space as a dedicated EV charging space and questioned if Council had the intention of implementing that suggestion. Council Officers advised that the current approved treatments are linemarking and signposting noting that it was cost efficient and easily implemented, however Council will monitor the spaces to see if the proposed treatments are effective and if additional measures need to be considered.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

1. That the EV charging sites identified in *Table 3* be supported for installation as per the signage and line marking plans provided in *Attachment 1*.
2. That the EV charging sites identified in *Table 2* be supported subject to targeted engagement and TfNSW approval as required.

For Motion: Unanimous

LTC1024(1) Item 12 Metropolitan Road, Enmore - Proposed changes to mobility parking (Damun-Stanmore Ward/Newtown Electorate/Inner West PAC)

SUMMARY

Council is proposing to adjust the existing angle parking layout and restrictions on the eastern side of Metropolitan Road, Enmore to improve access to the mobility parking space.

Officers Recommendation:

That the drawing showing additional timed parking restriction, changes to the existing mobility parking space, kerb extension, 'STOP' control line and kerb ramps on Metropolitan Road, Enmore (drawing no. 10300 sheet 1 of 1 – Metropolitan Road, Enmore - proposed adjustment to accessible parking space) be approved.

DISCUSSION:

Council Officers advised the purpose of the amendments to the proposal is to improve sight lines for vehicles exiting the laneway. Council Officers advised the 'STOP' line will be moved forward to its existing position, and to add edge linemarking from the existing kerb blister (extending out 1m from the edge of the kerb blister) into Enmore Lane (up to the first driveway).

The Committee members agreed with the amended recommendation.

COMMITTEE RECOMMENDATION:

That the drawing showing additional timed parking restriction, changes to the existing mobility parking space, kerb extension, 'STOP' control line and kerb ramps on Metropolitan Road, Enmore (drawing no. 10300 sheet 1 of 1 – Metropolitan Road, Enmore - proposed adjustment to accessible parking space) be approved with the

following changes:

The 'STOP' linemarking be moved forward to its previous position.

Edge linemarking be installed on the northern side of Enmore Lane, from the existing kerb blister (extending out 1m from the edge of the kerb blister) into Enmore Lane (up to the first driveway).

For Motion: Unanimous

LTC1024(1) Item 13 Meeks Lane, Marrickville - Proposed 'No Stopping' and 'No Parking' restrictions (Midjuburi-Marrickville Ward/Summer Hill Electorate/Inner West PAC)

SUMMARY

This report outlines parking issues in Meeks Lane, Marrickville and identifies the appropriate parking treatment to address them. The proposed parking treatment will improve access to Meeks Lane for adjacent properties.

Officers Recommendation:

That 'No Stopping' and 'No Parking' restrictions on the western side of Meeks Lane, Marrickville (i.e., along Boundary of 135 Meek Road) be installed as follows;

- 'No Stopping' restrictions extending for first 10m north of Meeks Road, and
- 'No Parking' restrictions extending 45m north from the proposed 'No Stopping' restrictions.

DISCUSSION:

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That 'No Stopping' and 'No Parking' restrictions on the western side of Meeks Lane, Marrickville (i.e., along Boundary of 135 Meek Road) be installed as follows;

- 'No Stopping' restrictions extending for first 10m north of Meeks Road, and
- 'No Parking' restrictions extending 45m north from the proposed 'No Stopping' restrictions.

For Motion: Unanimous

LTC1024(1) Item 14 Hutchinson Street, St Peters - Proposed 15-minute timed parking restrictions (Marrickville-Midjuburi Ward/Heffron Electorate/Inner West PAC)

SUMMARY

This report outlines parking issues in Hutchinson Street, St Peters and proposes 15-minute timed parking restrictions to provide short-term parking opportunities for nearby businesses.

Officers Recommendation:

That the proposed timed parking space (5.1m) signposted as 'P15 minute 7am-5pm Mon-Fri; 7am-12.30pm Sat' on the northern side of Hutchinson Street as shown in Attachment 1 be

approved.

DISCUSSION:

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That the proposed timed parking space (5.1m) signposted as 'P15 minute 7am-5pm Mon-Fri; 7am-12.30pm Sat' on the northern side of Hutchinson Street as shown in Attachment 1 be approved.

For Motion: Unanimous

LTC1024(1) Item 15 Temporary speed reductions to 40 km/h for Southwest Link

SUMMARY

During the 12-month closure of the T3 line (Sydenham to Bankstown), rail replacement Southwest Link bus services are expected to carry 56,000 (and up to 72,000) passengers a day, with the majority of passengers expected to transfer at Sydenham Station to access Metro and Train services to and from the CBD.

Temporary bus stops are being established at each station from Sydenham to Bankstown, resulting in a significant increase in pedestrian activity and footpath use.

As part of the Sydney Metro City & Southwest project, Transport for NSW is introducing temporary speed reductions at key locations to support safer streets around Southwest Link bus stops, cycling routes and to reduce noise impact on residents from replacement buses.

To support the temporary speed reductions in the Inner West Council area, Transport for NSW have procured traffic guidance schemes for the delivery of these changes.

Officers Recommendation:

That it be noted that TfNSW will be implementing Traffic Guidance Schemes (TGS) to support the delivery of temporary speed zone reductions for the Southwest Link as outlined in the report.

DISCUSSION:

Public Speaker Benjamin Borger (Representative for TfNSW) entered the meeting at 11.29 am.

Mr Borger advised that as part of the Southwest Link and Metro works, TfNSW is proposing to reduce mixed traffic to 40km/hr to enhance pedestrian and cyclist safety.

Council Officers advised that Council are currently implementing the newly adopted InnerWest@40 study and will work with and advise TfNSW of which locations will have permanent signage installed.

Public Speaker Benjamin Borger (Representative for TfNSW) left the meeting at 11.33 am.

The Committee members agreed with the Officer's recommendation.

COMMITTEE RECOMMENDATION:

That it be noted that TfNSW will be implementing Traffic Guidance Schemes (TGS) to support the delivery of temporary speed zone reductions for the Southwest Link as outlined in the report.

For Motion: Unanimous

General Business:

Item 16: Review of Mobility Parking near Henson Park for the upcoming AFLW events

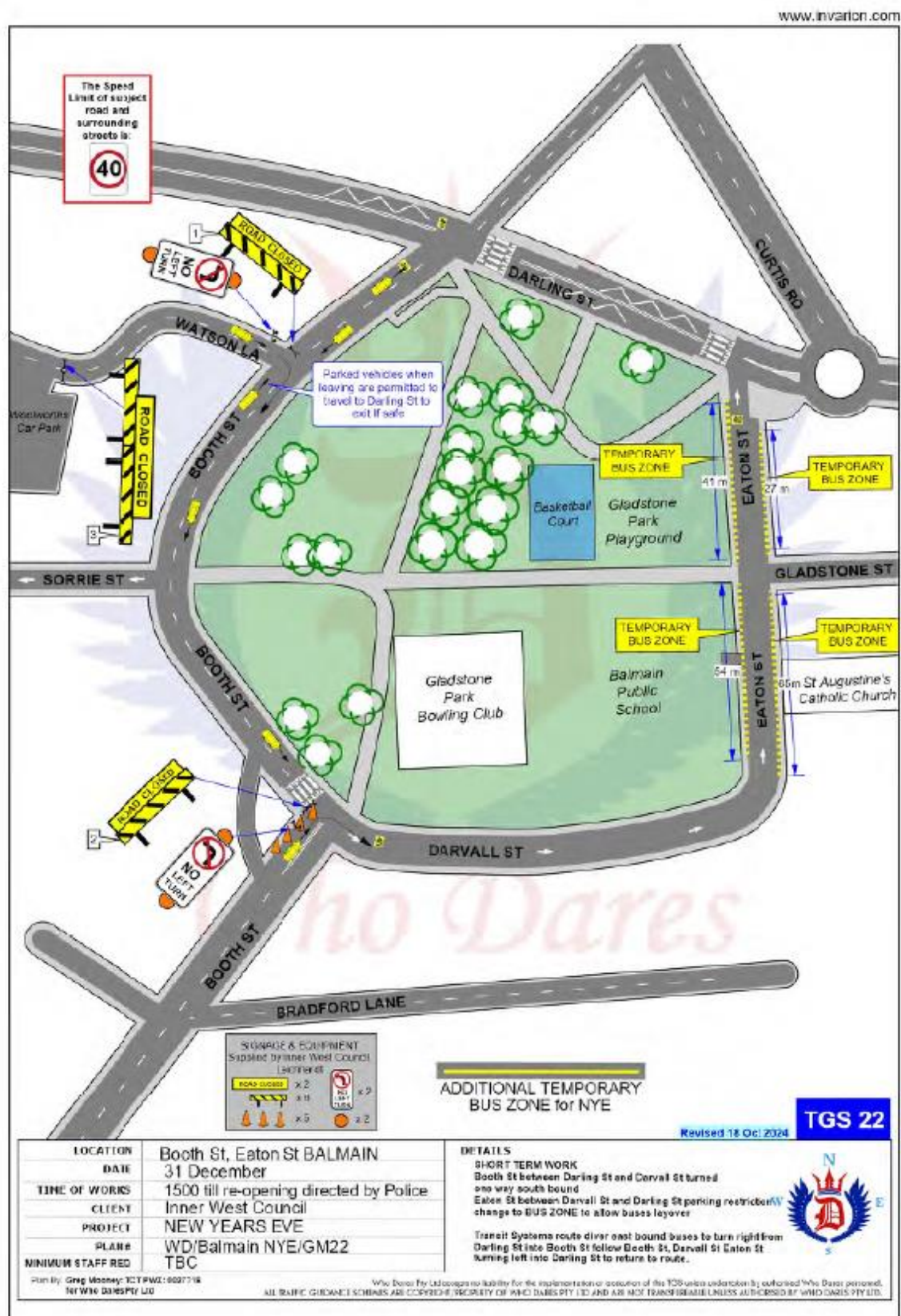
The Representative for the Member for Summer Hill requested a review of Mobility Parking near Henson Park for the upcoming AFLW events. Council Officers advised they will take the request on notice and will look into discussing the possibility of putting aside spaces for mobility parking with the event organisers. It was noted that such request will need to be investigated and incorporated into the Traffic Management Plan.

Meeting closed at 12.55 pm.

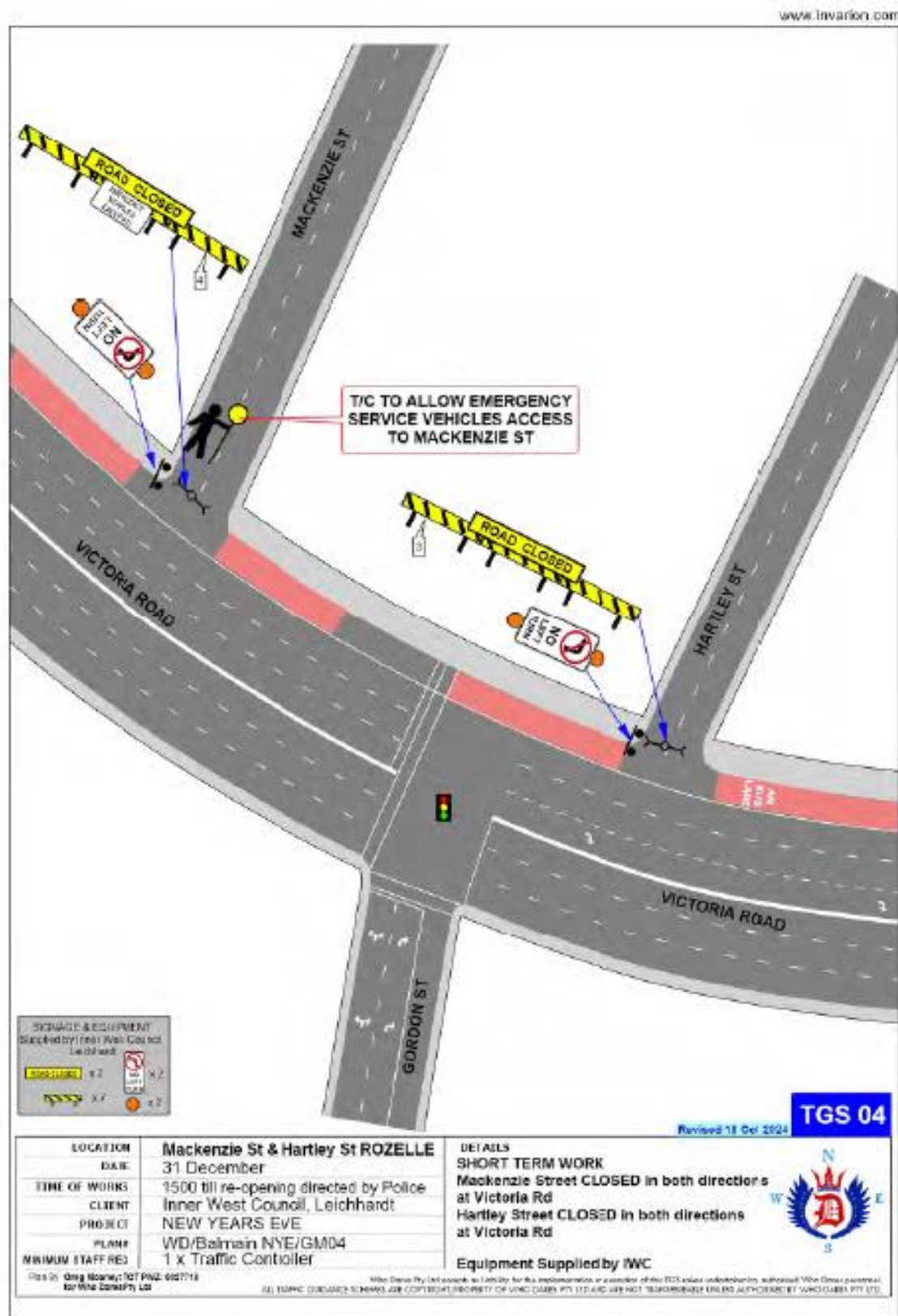
CHAIRPERSON

Manod Wickramasinghe

Appendix 1 – Item 2 Traffic Management Plan for the 2024 New Year's Eve Event



Appendix 2 - Item 2 Traffic Management Plan for the 2024 New Year's Eve Event

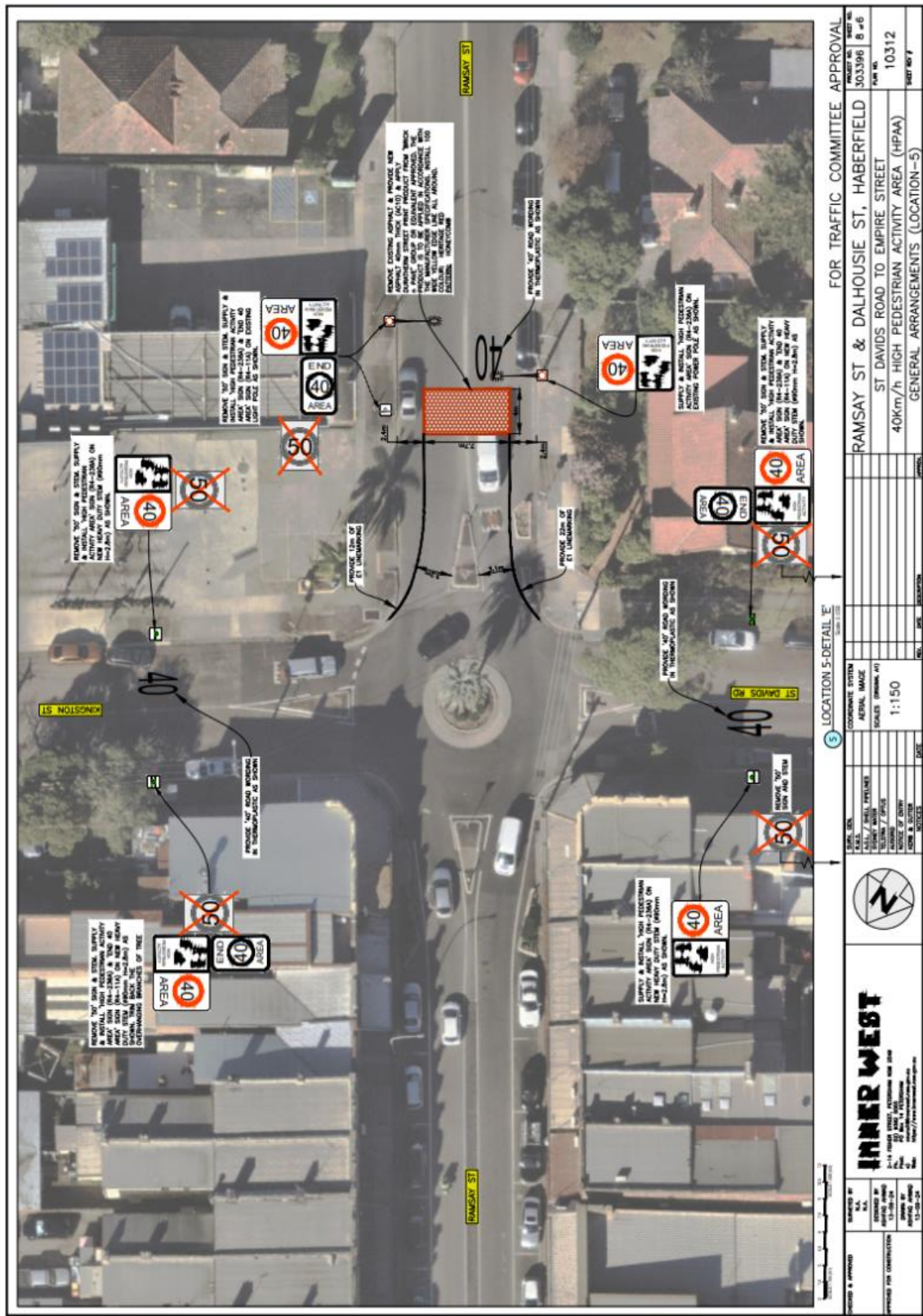


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LINEMARKING SPECIFICATIONS

TF

BB

TB

TB1

C1

C3

C4

S1

L1

L7

E1

E4

PCW

PX

ZIG ZAG LINE

DRAGON TEETH

UNLINED

UNLINED

40

FOR TRAFFIC COMMITTEE APPROVAL

RAMSAY ST & DALHOUSIE ST, HABERFIELD

ST DAVIDS ROAD TO EMPIRE STREET

40km/h HIGH PEDESTRIAN ACTIVITY AREA (HPAA)

LINEMARKING SPECS

Item No: LTC1124(1) Item 1
Subject: ILLAWARRA ROAD, WHARF STREET AND THORNLEY STREET, MARRICKVILLE – TRAFFIC SAFETY REVIEW OF STREETS AROUND STEEL PARK (MIDJUBURI-MARRICKVILLE WARD /SUMMER HILL ELECTORATE /INNER WEST LAC)
Prepared By: Jennifer Adams - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

1. That this report be received and noted.
2. That the following works to be completed in 2024/25 be noted:
 - a) Upgrade of pedestrian crossing on Illawarra Road, immediately west of Thornley Street to a raised crossing facility; and
 - b) Reduction to a 40 km/h speed limit for Thornley Street and other local streets in the Marrickville area as part of the InnerWest@40 project rollout (Area 6 Enmore & Marrickville East; Area 7 Marrickville & Tempe).
3. That Police be requested to undertake regular Police patrols/enforcement of Illawarra road in the vicinity of Steel Park.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

At the Council Meeting held 25 June 2024 a Notice of Motion (NoM) for Steel Park Dog Off-Leash Area and Amenities (Item C0624(2) Item 22) was resolved. Part 2 was for consideration of determining whether any additional traffic calming measures are required in the busy area bounded by Thornley and Wharf Streets. This report provides an assessment of the locality and also lists recent and future works completed or planned.

BACKGROUND

At the Council Meeting held on 25 June 2024, Council resolved the following in part:

2. *That Council undertakes an investigation to determine the need for additional traffic calming measures to reduce risk to pedestrians and dogs in the busy area bounded by Thornley and Quarry Streets. The area includes pedestrian entry and exit points adjacent to PCYC car park and the entry and exit of the PCYC / Debbie and Abbey Borgia Centre car park and the Yirran Gumal Early Learning Centre. Council to report the outcome of the investigation to the Local Traffic Committee.*

It should be noted that Quarry Street is located in the Tempe area which is some distance away from the facilities mentioned within the NoM. Wharf Street has instead been taken as the boundary as it bounds the facilities stated. This report provides an assessment of whether any

additional traffic calming measures are required in the busy area bounded by Thornley and Wharf Streets near Steel Park and also lists recent works completed or planned in the area.

DISCUSSION

Local road network

Illawarra Road in the vicinity of Steel Park is a Regional Road that carries around 14,500 vehicles per day. It is a 13.1 metre wide two-way road with one travel lane in each direction, in addition to kerbside parking on both sides. It has a posted speed limit of 50km/h however the 85th percentile speed (ie design speed) is 52.2 km/h. Heavy vehicle volume is around 3%. It is a bus route.

Thornley Street is a local residential street running west-east between Illawarra Road and Premier Street, Marrickville. It provides one travel lane in each direction, in addition to kerbside parking lanes. Parking along the western section is unrestricted. Carriage width at the Illawarra Road end is 8.3 metres. It carries around 700 vehicles per day and the 85th percentile speed (ie design speed) is 41.4km/h.

Wharf Street is a two-way local residential street running east-west between Illawarra Road and Beauchamp Street, Marrickville. It has a carriageway width of approximately 13.1 metres, with a small narrow section opposite properties No. 3 to 7 having a width of 7.9 metres. It carries around 1,150 vehicles per day and the 85th percentile speed is 47.8km/h.

On review of the volume data, Illawarra Road being one of the few roads connecting to the Canterbury Bankstown LGA has a volume relatively high of traffic for a regional road, however both Thornley and Wharf Streets have traffic volumes which are relatively low and are typical of local roads. In relation to speed, whilst both local roads are below the posted speed limit, Illawarra Road is slightly higher. Given that Illawarra Road is one of the main routes through the Marrickville area and carries a large volume of traffic, it is not unusual to have a slightly higher 85th percentile speed over the posted speed limit. This can be managed through regular enforcement of the speed limit.

Pedestrian facilities previously constructed or in process of being constructed in locality

In 2005, as part of Council's 2005/2006 Capital Works Program, Council implemented traffic and pedestrian safety measures in Wharf Street, Marrickville. A central pedestrian median island was installed in Wharf Street, Marrickville at the intersection of Illawarra Road to improve pedestrian and traffic safety at this location. In 2012 this pedestrian refuge was upgraded funded by Black Spot funding.



In 2010 Council upgraded the pedestrian refuge island on Illawarra Road at its junction with Wharf Street, Marrickville as part of the refurbishment works in Steel Park. The refurbishment works included construction of a salt marsh, relocation of the cycleway, relocation of the car park and construction of a water play facility. Upgrading the pedestrian refuge to current RTA/TfNSW guidelines was a consent condition so as to provide satisfactory pedestrian access at the intersection of Illawarra Road and Wharf Street.



In 2012 as part of its Traffic Capital Works Program, Council implemented traffic and pedestrian safety measures in Illawarra Road near Wallace Street, Marrickville. A pedestrian refuge island in Illawarra Road just north of Wallace Street was installed to improve pedestrian and traffic safety at this location. Council received funding from the Federal Government as part of the Nation Building Black Spot Funding Program for 2011/12 for this project.



There is currently a pedestrian crossing on Illawarra Road, immediately west of Thornley Street (and adjacent to the Yirran Gumal Early Learning Centre and Debbie and Abbey Borgia Community Recreation Centre). It is an at-grade crossing facility with kerb blisters and a

centre refuge island. The facility is well used by the adjacent facilities and by bus patrons as bus stops are present (nearby) on either side of this existing crossing facility.



Item 1

It is noted that Council is in the process of upgrading this pedestrian crossing on Illawarra Road near Thornley Street to a raised crossing facility. A report went to Local Traffic Committee at its 15 April 2024 meeting (Item 6) recommending that the upgrade of the existing pedestrian (zebra) crossing on Illawarra Road near Thornley Street to a raised crossing facility be approved in principle and that the detail design be brought back to the Traffic Committee for consideration.

A separate report within this agenda details the design plan (Design Plan 10304) for those upgrade works. The proposed works will improve safety of pedestrians at this crossing and addresses community concerns which have been raised.

Pedestrian crossing facilities have been located along Illawarra at or very close to intersections with side streets and also close to entry points into the Steel Park or the Debbie and Abbey Borgia Centre. In this respect, these facilities would attract the highest volume of pedestrian traffic and provide a safe place for pedestrians to cross.

Steel Park

Steel Park is a large recreational area that is home to a Waterplay Park, sports grounds, covered picnic tables with barbecues and a playground featuring slides and swings. The park includes the Debbie & Abbey Borgia Community Recreation Centre, outdoor gyms and access to the Cooks River Cycleway. Steel Park is bounded by Thornley Street to the north, residential properties and the Cooks River Foreshore Park to the east, the Cooks River to the south and Illawarra Road to the west.



The character of Steel Park is dominated by three major elements; the Debbie and Abbey Borgia Community Recreation Centre (DAB), the sports fields and the river edge with its vegetation and play facilities. The play facilities themselves contribute to the parks character with their high visitation and close proximity to the car park and access points to the park.

The pedestrian pathways within Steel Park are mostly located around the children's play area, the riverside cycle way and as access paths to the DAB Community Recreation Centre. The pathways are constructed of in situ concrete and vary in width depending on function. The pathways in Steel Park are in good condition.

Steel Park has three car parking areas. One is accessed off Illawarra Road providing car parking for the children's playground and picnic areas. Another car park adjacent the DAB is accessed off Illawarra Road and is for users of the DAB facility. The third car park is accessed off Thornley Street and is primarily used by users of the DAB facility. All carparks are paved and have good pedestrian access.

1. Debbie and Abbey Borgia Recreation Centre carpark accessed off Illawarra Road, Marrickville. It has approximately 85 spaces (4 mobility) with no parking restrictions.
2. Debbie and Abbey Borgia Recreation Centre carpark accessed off Thornley Street, Marrickville. It has 27 spaces (4 mobility) with '2P 8am-6pm Mon – Sun' restrictions. In 2024 Council formalised a '15-minute Parking 7.00am-9.30am and 4.00pm-6.30pm Monday to Friday' zone on the southern side of Thornley Street car park outside Yirran Gumal Early Learning Centre to assist parents/carers with the drop-off and pick-up of children.
3. Steel Park carpark accessed off Illawarra Road, Marrickville. It has 25 spaces (2 Mobility) with '4P 6am-9pm, Free Parking Area closed 9pm-6am' restrictions.

Inspections of the pedestrian entry and exit points adjacent to PCYC car park / Debbie and Abbey Borgia Centre car park and the Yirran Gumal Early Learning Centre have been undertaken and are considered suitable. Footpaths are around 2 metres wide at each entryway, sight lines are good for both pedestrians and motorists and the footpath / driveway surfaces are in good condition.

Further, the riverside pedestrian / share pathways travelling through Steel Park are well maintained and in good condition. The cycle connection links to an on-road cycle link along Illawarra Road to Marrickville and across the Cooks River to Earlwood. A further connection crosses Illawarra Road and links through the Marrickville Golf Course along Wharf Street. In the 350-metre length between Thornley Street and Wharf Street there are 3 pedestrian facilities which are considered adequate.

Crash history / road safety investigations

In the last 5 years of recorded TfNSW crash data there have been two injury crashes along Illawarra Road between Thornley Street and Wharf Street, Marrickville.

In December 2020 there was a cross traffic (RUM 10) incident on Illawarra Road at Cahill Place, Marrickville. A car exiting Cahill Place did not give way to a vehicle travelling north along Illawarra Road. In April 2023 there was an off-road (RUM 71) incident involving a vehicle travelling north along Illawarra Road losing control and running off the road mid-way between Wallace Street and Wharf Street, Marrickville.

A check of the crash history to 2009 reveals only one pedestrian related crash in the last 14 years of recorded TfNSW crash data. In June 2009 a pedestrian on the Illawarra Road carriageway (RUM 3) at Wallace Street was hit by a vehicle travelling south. The pedestrian refuge built after this incident had addressed the matter.

In 2013 Council investigated pedestrian safety adjacent Steel Park in depth after representations were made to Council in regard to pedestrian safety in the area. At that time traffic and pedestrian data was collected along Illawarra Road between Wharf Street and the Cooks River on both a typical weekday as well as on a Saturday to reflect weekend usage. Warrants were not met for the installation of a pedestrian zebra crossing. Further, it was considered the pedestrian refuge was the safest option at that location due to low pedestrian numbers crossing the road. The pedestrian refuge allows a two staged crossing manoeuvre where pedestrians only have to judge gaps in one direction of traffic flow at a time. Also, a reduced carriageway width at the crossing point minimizing exposure time in the traffic lane.

Recent traffic and pedestrian works completed in the area

In July 2024 Council received concerns regarding vehicles being parked too close to existing barrier lines in Thornley Street, Marrickville and not having adequate road space for traffic exiting out of Thornley Street to Illawarra Road. To alleviate this, the existing BB lines were shortened and 'No Stopping' restrictions were installed on the southern side of Thornley Street at its intersection with Illawarra Road, Marrickville making the intersection safer.

In early 2024 Council installed a '15-minute Parking 7.00am-9.30am and 4.00pm-6.30pm Monday to Friday' zone on the southern side of Thornley Street car park outside Yirran Gumal Early Learning Centre to assist parents/carers with the drop-off and pick-up of children. A report regarding this matter went to Local Traffic Committee at its 11 December 2023 meeting (Item 7).

Moreover, the recommended works to upgrade the pedestrian crossing on Illawarra Road at Thornley Street will improve safety of pedestrians at this crossing and addresses community concerns which have been raised.

It should also be noted that Council is planning to introduce a 40 km/h speed limit for local streets in the Marrickville area as part of the InnerWest@40 project which will have some effect on the local roads surrounding the park and this may also transfer over to a lower speed along Illawarra Road which will remain sign posted at 50km/h. Moreover, regular police enforcement will more likely result in a slight reduction in the traffic speeds along Illawarra Road. The speed limit reduction in this area is expected to be completed in the 2024/25 financial year.

Conclusion

Currently, pedestrian safety in the vicinity of Steel Park is considered satisfactory. There have been no TfNSW recorded pedestrian incidents for some time along Illawarra Road and/or the vicinity of the other streets surrounding Steel Park. Thus, at this time, no other additional traffic calming measures that could reduce risk to pedestrians are considered necessary in the locality.

As noted, the current situation will also improve with the upgrading of the pedestrian crossing on Illawarra Road near Thornley Street to a raised crossing facility. It is anticipated that the raising of the crossing will effectively slow traffic down acting as a proxy speed hump on Illawarra Road adjacent to the Yirran Gumal Early Learning Centre. In addition, the reduced speed limits to be introduced in local streets in the 2024/25 financial year will improve safety for all road users.

ATTACHMENTS

Nil.

Item No: LTC1124(1) Item 2

Subject: STATION STREET, MARRICKVILLE - PROPOSED BICYCLE EXCEPTED SIGNAGE (MIDJUBURI-MARRICKVILLE WARD / SUMMER HILL ELECTORATE / INNER WEST PAC)

Prepared By: Daniel Li - Student/Graduate Traffic Engineer

Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the installation of 'Bicycle Excepted' signage at the entry of Station Street and Leofrene Avenue from Schwebel Street, Marrickville as well as Leofrene Avenue at Station Street (below the existing 'One Way' and 'No Entry' signage) be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

BACKGROUND

As part of the TfNSW Southwest Metro project, the T3 Bankstown Line from Sydenham to Bankstown is being converted to Metro standards to improve the reliability of Sydney's transportation network. To aid affected commuters along the T3 Line, TfNSW has implemented temporary bike riding routes as part of its Temporary Transport Plan (TTP) to encourage people to travel by bike during the conversion of the T3 Bankstown Line.

To assist with active transport users, the TTP provides a cycleway route between Hurlstone Park to Sydenham via Dulwich Hill and Marrickville, see *Attachment 1*.

Council has received concerns from active transport users and local residents about the route through Station Street, Marrickville.

DISCUSSION

Currently, Station Street, Marrickville operates with one directional traffic flow starting from Schwebel Street and terminating at Leofrene Avenue. Council have received requests from local residents and active transport users to address cyclists going against the one way directional traffic flow thereby resulting in illegal and unsafe movements in Station Street.

To resolve this problem and to ensure that all bicycle activities are legal and safe in Station Street and Leofrene Avenue, Council proposes the installation of 'Bicycles Excepted' signage at the locations which contain regulatory signage ('One-way' or 'No Entry') as seen in the proposed concept plan (Figure 1).

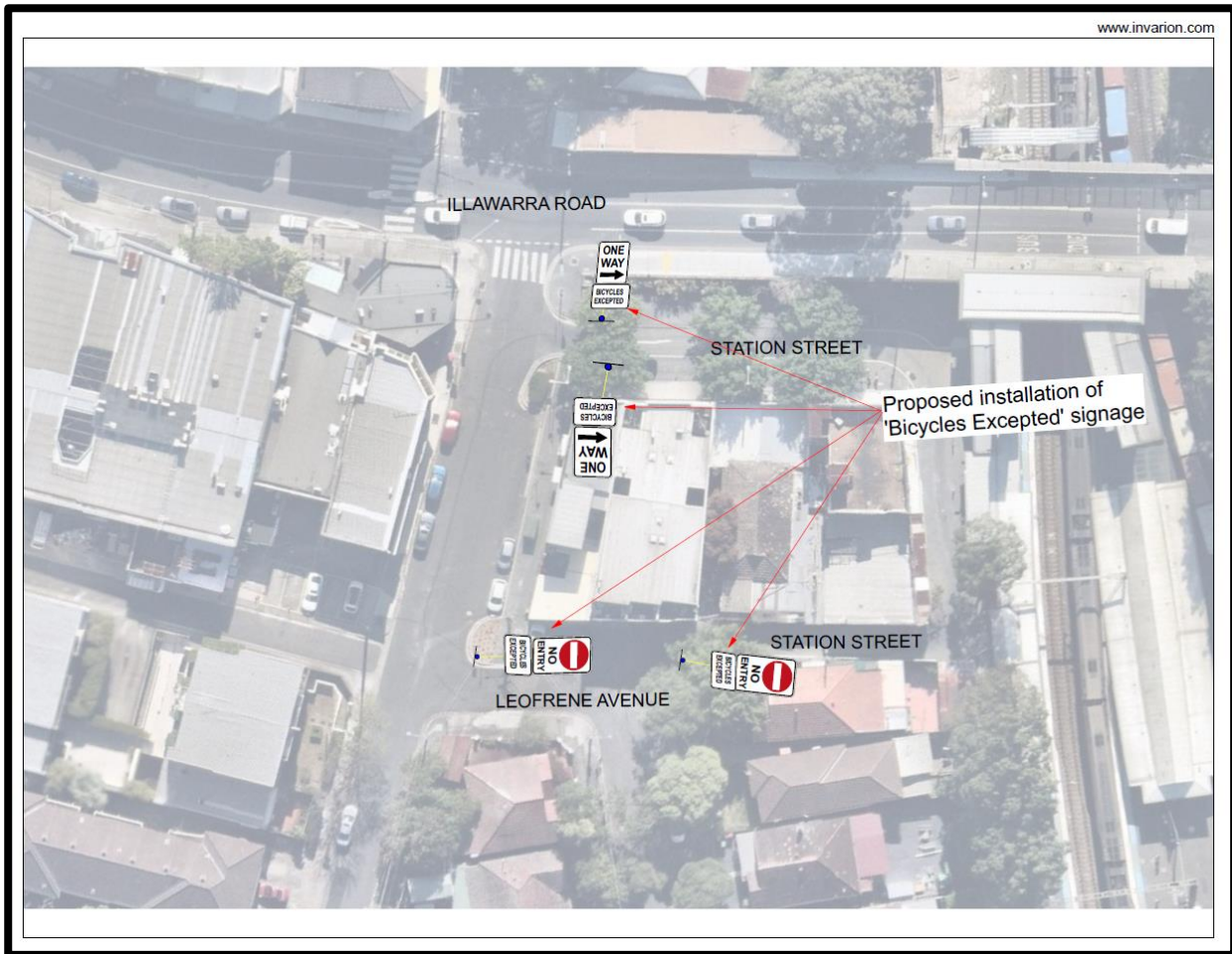


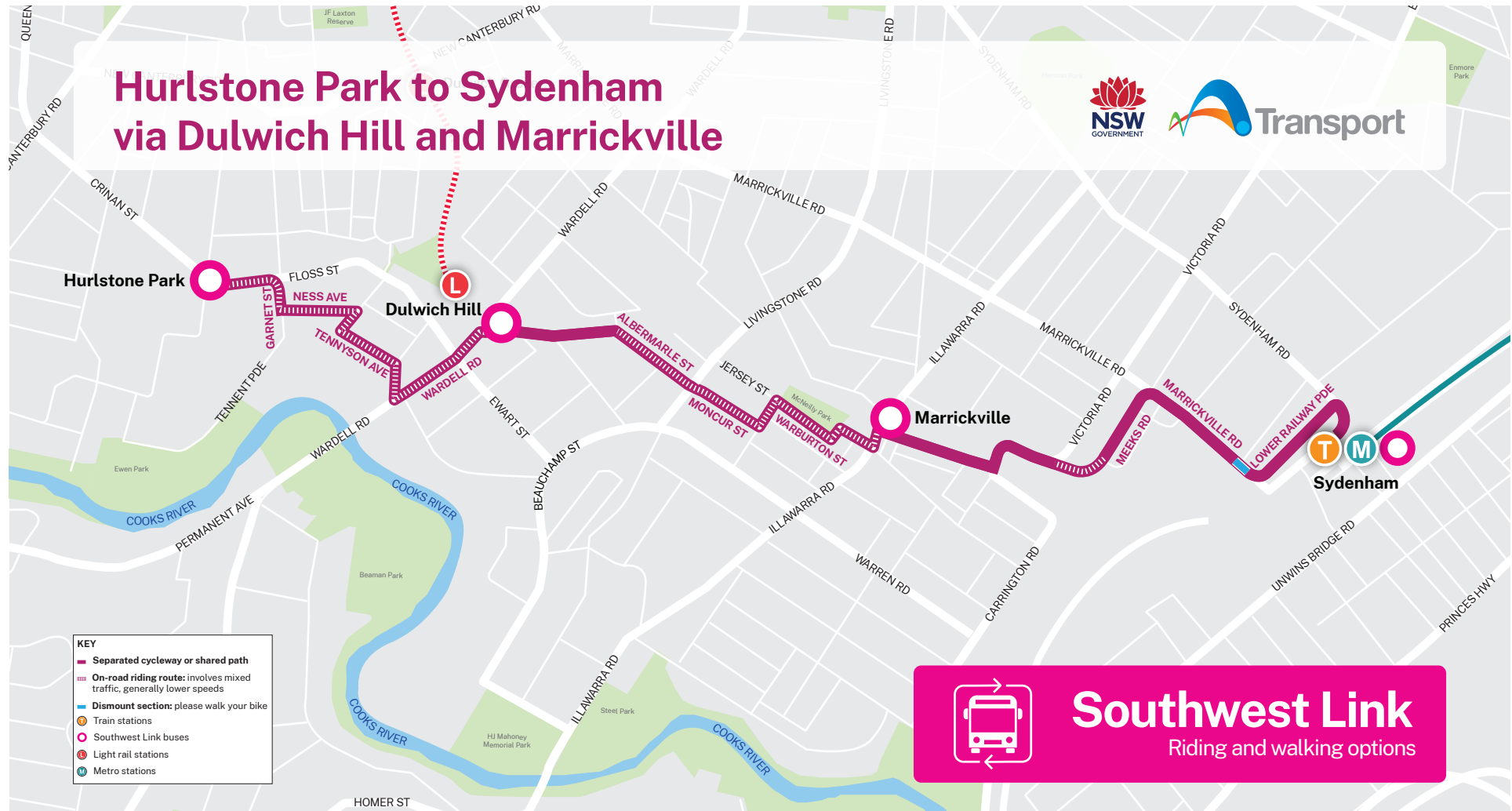
Figure 1. Proposed Concept Plan

FINANCIAL IMPLICATIONS

The cost of the installation of 'Bicycle Excepted' signage is to be funded from Council's operating budget for signs and line marking.

ATTACHMENTS

1. [TfNSW Southwest Link - Bicycle Riding Map](#)



Item No: LTC1124(1) Item 3
Subject: LE CLOS LANE, MARRICKVILLE - PROPOSED 30 TONNE WEIGHT LIMIT ON THE LE CLOS LANE BRIDGE (MIDJUBURI-MARRICKVILLE WARD / SUMMER HILL ELECTORATE / INNER WEST PAC)
Prepared By: Daniel Li - Student/Graduate Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That a 'Bridge Load Limit 30T Gross' weight limit at the entry to Le Clos Lane and at the Le Clos Lane bridge in Marrickville be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

This report outlines the current structural engineering concerns associated with the Le Clos Lane bridge in Marrickville and proposes the appropriate traffic treatments to maintain the structural integrity of the bridge following the completion of maintenance works.

BACKGROUND

Council has engaged consultants BG&E to perform a Level 3 Bridge Assessment of five short span bridges in Marrickville. Following the bridge assessment, BG&E have provided a structural engineering report which can be seen in *Attachment 1*, featuring recommendations and ameliorations for the bridge in Le Clos Lane, Marrickville. BG&E have recommended the installation of a 30T load limit signage and a speed restriction for Council waste trucks travelling across the bridge to prevent substantial degradation of the structure.

DISCUSSION

Council Officers undertook a traffic tube survey between 18 September 2024 to 2 October 2024 to identify the average speed and type of vehicles travelling across the bridge.

Given that Le Clos Lane primarily functions as a laneway for residential access, the majority of the collected data was derived from small vehicles travelling in the laneway to access residential garages. These small vehicles have a maximum gross limit of 5T, thereby providing minimal impact to the bridge. As such, the proposed recommendations of reduced speed restrictions and load limit is not applicable to these vehicles.

In comparison, based on the traffic tube surveys, there were only two recorded counts of Council waste vehicles, both of which were travelling within the desirable 10km/h – 20km/h speed range. A contributing factor to Council's waste trucks travelling within this range of speed is due to Le Clos Lane providing an approximate road width of 5m, facilitating one lane of travel and one lane of unrestricted parking. While the speed restriction is not enforceable in the laneway, to reduce the degradation of the bridge structure, Council proposes the installation of the 30T Gross Limit signage at the following locations of Le Clos Lane, Marrickville.



Figure 1. Signage plan of 30T Gross Limit Sign at the Le Clos Lane entrance.



Figure 2. Signage plan of 30T Gross Limit Sign at the Le Clos Lane bridge.

FINANCIAL IMPLICATIONS

The cost of installing a 'Bridge Load Limit 30T Gross' signage as recommended will be funded within Council's operational budget for signs and line marking.

ATTACHMENTS

1. [Download](#) BG&E Bridge Assessment Report

Le Clos Lane Bridge, Marrickville, NSW

Prepared for Inner West Council

May 2024
Project Number S23198
Version B



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Contents

1. Executive Summary	1
2. Introduction	2
2.1 Bridge Description	2
2.2 Scope	3
3. Inspection	4
4. Material Testing and Results	7
4.1 Concrete Scanning and Testing	7
4.2 Carbonation Testing	7
5. Load Rating Assessment	9
5.1 Rating Factor	9
5.2 Assessment Loads	9
5.3 Structural Analysis	10
5.4 Assessment Results	11
5.5 Load Limits	11
6. Conclusions	13
7. Recommendations	14
7.1 Recommended Work	14
7.2 Concrete Spall Repair	14
7.3 Clean Steel Work	15
Appendices	
Appendix A - Condition Assessment	17
Appendix B - Rating Vehicles	18
Appendix C - Component Plan	19

Document Control				
Revision	Date	Prepared	Reviewed	Approved
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1. Executive Summary

Le Close Lane Bridge is a single-span road bridge over a local drainage channel in Marrickville.

The superstructure consists of a single reinforced concrete deck spanning between masonry abutments on a skew of 12 degrees.

The bridge is in fair condition with no substantial change in the severity of the previous defects found in "Pitt & Sherry" report dated 15 July 2022.

The structure was modelled under six (6) vehicle load configurations and a load rating factor calculated for each member in the 'As-Is' state and in the 'As-New' conditions. The load rating factors of deficient members in the 'As-Is' condition are highlighted below.

The 'As-Is' condition of the slab was equivalent to the 'As-New' condition of the deck slab.

Table 1: 'As-New' / 'As-Is' Rating Factor for Deck Slab in Span 1

Span	No. of Lanes	Action	Vehicle					
			T44	Semi GML	Semi HML	BD GML	BD HML	Garbage Vehicle
1	1	Bending	0.63	0.75	0.75	0.75	0.75	0.92
1	1	Shear	1.17	1.48	1.43	1.48	1.43	2.03
1	2	Bending	0.59	0.69	0.66	0.69	0.66	-
1	2	Shear	1.07	1.41	1.33	1.41	1.33	-

From the results of the load rating assessment, it is recommended that a signed load limit is placed on the bridge, as outlined in Section 5.5 of this report.

The rating factor for garbage trucks of 0.92 is based on a ULS Factor of 1.5 and full dynamic load allowance. Due to the geometry of the road, any large vehicles would be travelling at low speeds. If the assumed maximum speed is limited to 20 km/h, the reduction in Dynamic Load Allowance from 0.4 to 0.25 is sufficient to increase the rating factor above 1.

The bridge is adequate to support General Fire appliances.

It is recommended that a signed load limit of 30t is applied to the structure.

2. Introduction

BG&E was commissioned by Inner West Council to conduct a structural inspection and load rating assessment of 5 rail overbridges located in Marrickville, NSW. This assessment includes a materials investigation to determine the extent of carbonation of the concrete.

2.1 Bridge Description

Le Close Lane Bridge is a single-span road bridge over a local drainage channel in Marrickville, NSW at the following coordinates: -33.908500, 151.158800.



Figure 1: Le Close Lane Bridge, Marrickville NSW (Six Maps Adapted, 2024)

A summary of the bridge measurements is provided below. As no as-built drawings were available for this bridge, all dimensions used in this assessment were based on the measurements recorded on site:

Table 2: Bridge Information

Attribute	Defect Type
Clear Span	2.22 m
Total Length	3.5 m
Width	6.48 m (total width) 6.2 m (between kerbs)
Deck Depth	275 mm

As Le Clos Lane is a no through road, heavy traffic does not travel down the road.

2.2 Scope

BG&E conducted a site inspection on the 10th of January 2024 with a follow up inspection on the 23rd of January to understand the structural form and condition of the bridge. The inspection also provided BG&E with an opportunity to take measurements of the bridge members of the theoretical load rating assessment. The site photos and defects have been compiled and are appended to this report.

The assessment is limited to the bridge structure under the specified rating vehicles (T44, B-Doubles and Semi-trailers), applied in accordance with AS5100.

2.2.1 Input Documents and Standards

The following documents were used in the undertaking of the load rating assessment.

- TfNSW Bridge Inspection Procedure Manual
- AS5100 - Bridge Design (2017)

3. Inspection

The bridge was inspected by BG&E on the 10th of January 2024 with a follow up inspection on the 23rd of January. Access was provided to the underside via the locked gate at Garners Avenue.

General photos of the bridge are provided below.



Figure 2: General View from Elevation 1 (Northern)



Figure 3: General View of Deck Over



Figure 4: General View from Approach 2 (Western Approach)



Figure 5: General View from Approach 2 (Western Approach)

The bridge was found to be in a fair condition with no substantial change in the severity of the previous defects found in "Pitt & Sherry" report dated 15 July 2022.

All observed defects are listed below. The full condition rating and required maintenance actions including photos are located in Appendix A.

Table 3: Observed Defects

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.
37405	1.1	CDSL	Concrete-Deck Slab / Cast In-situ Concrete	Weathering is throughout most area of concrete deck slab. No change	Monitor.	20	m ²	2 years
37406	1.2	CDSL	Concrete-Deck Slab / Cast In-situ Concrete	Two large longitudinal cracks to top of deck slab. No change One large longitudinal crack was observed along the elevation side 1 of deck slab. No change Cracks are approx 5mm in width. No change	Repair Cracks	10	m	1 year
37407	1.3	CDSL	Concrete-Deck Slab / Cast In-situ Concrete	Several spalls to soffit and side of deck slab with exposed reinforcement showing signs of corrosion. No change	Clean and recoat reinforcement. Patch repair concrete	15	m ²	1 year
37404	2.1	RMIS	Miscellaneous Railing including Guard fence / Other	Surface rust observed on steel fence. No change	Clean surface rust and repaint.	2	m ²	1 year

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.
37403	3.1	MMAS	Brick / Masonry / Reinforced Earth / Other	Weathering throughout most of the abutment brick walls. No change	Monitor.	16	m ²	2 years
37402	4.1	MGCL	General Cleaning	Graffiti observed on the channel brick wall. No change	Clean.	2	Each	2 years

4. Material Testing and Results

BG&E has undertaken the following testing methodology:

4.1 Concrete Scanning and Testing

4.1.1 Scanning

Contourscan was subcontracted by BG&E to undertake concrete scanning of the deck slab. They utilised a reinforcement electromagnetic scanning cover meter to identify the location of reinforcement in the deck slab, depth and spacing in the concrete.

A summary of the results taken on site and through the investigative works are provided below in Table 4:

Table 4: Recorded Measurements

Element	Dimension
Slab Depth	275 mm
Main Reinforcement	Cover = 30 mm Size = 15.875 mm (5/8") Spacing = 152.4 mm (6")
Transverse Reinforcement	Size = 15.875 mm (5/8") Spacing = 400 mm

4.2 Carbonation Testing

Carbonation is identified as one of the primary environment risks to durability of structural concrete elements. Carbonation testing is performed using a phenolphthalein indicator solution sprayed onto the recently drilled concrete region. The indicator will turn to a bright pink colour when contacting high pH (> 9.0) and remains clear when in contact with low pH (< 9.0) which allows the measurement of carbon depth. The carbonation depth is then used to calculate the remaining life before carbonation ingress reaches the reinforcement in the presence of moisture. Based on the results of the site investigation, predictive carbonation modelling has been undertaken to assess if the concrete is at risk of carbonation induced corrosion when exposed to moisture over its required additional life. Carbonation modelling has been undertaken using the formula:

$$X = k\sqrt{t}$$

Where:

X = Measured depth of carbonation

k = A constant for each particular material in a particular environment

t = time

Internal exposure refers to the internal environment of the deck slab, specifically the sections of the elements that are not exposed to the external factors such as wind and rain. On the other hand, the external environment refers to the external exposure of the deck slab, where areas are exposed to the external factors such as rain and wind. Figure 6 shows the internal and external environment of the deck slab.



Figure 6 Location of Internal and External Element of the Deck Slab

Carbonation corrosion can be defined as when the carbonation depth reaches the embedded steel, depassivation of the steel occurs and in the presence of moisture corrosion initiates. Carbonation testing was completed at the nominated testing locations. The measured carbonation depths were used in predictive modelling to obtain time to depassivation. The results of the modelling are provided in Table 5. Carbonation testing was only undertaken on the bridge's soffit.

Table 5: Carbonation Modelling Results

Structural Element	Cover (mm)	Depth of Carbonation (mm)	Time to Corrosion (Years)
Soffit – External	30	20	15
Soffit – Internal		20	15

The predictive modelling has indicated that carbonation front will reach the reinforcement depth within the next 15 years.

5. Load Rating Assessment

5.1 Rating Factor

BG&E has conducted a desktop load rating assessment on the 'As-New' and 'As-Is' condition of the structure using information obtained during the site inspection. This assessment determines the rating factor which is a measure of the available capacity of the element to support live loads. The Rating Factor (RF) is defined as:

$$RF = \frac{\text{Available Bridge Capacity for Live Load}}{\text{Live load effects of nominated rating vehicle}} = \frac{\phi R_u - \gamma_g S_{DL}^* - \gamma_{gs} S_{SDL}^*}{\gamma_L S_{LL+\alpha}^*}$$

Table 6: Rating Factor Parameters

Parameters	Description
γ_g, γ_{gs} and γ_L	Ultimate Load factors for dead load, super imposed dead load and Live load from AS5100.7-2017
$S_{DL}^*, S_{SDL}^*, S_{LL+\alpha}^*$	Nominal Loads for Dead Load, Superimposed dead load and live load plus DLA
ϕR_u	Ultimate capacity of member considering material properties and geometry

A rating factor equal to or greater than unity (1.0) indicates the element has capacity to resist the live load of the nominated vehicle.

5.2 Assessment Loads

5.2.1 Load Factors

The ULS factors for the permanent effects and rating vehicles have been applied according to AS5100.7-2017, Table 12.2(A) and Table 12.2(B) respectively and are summarised below:

Table 7: Load Factors

Type of Load	Load Factor (γ)
Dead load (concrete)	1.2
Superimposed dead load (removable load)	2.0
Vehicle traffic live load (T44, GML, HML)	2.0
Garbage truck live load	1.5

Two design lanes were positioned on the bridge in accordance with AS5100.7-2017 as well as positioned to produce the worst effects onto the members. A summary of the lane factors for each lane are provided below based on Table 8.

Table 8: Lane Factors

Standard Design Lane Number (n)	Accompanying Lane Factor (ALFi)
1 lane loaded	1.0
2 lanes loaded	1.0 for first lane; and 0.8 for second lane
3 or more lanes loaded	1.0 for first lane; 0.8 for second lane 0.4 for third and subsequent lanes

A DLA of 0.4 was adopted, as per clause 11.3.6 of AS5100.7-2017.

5.2.2 Rating Vehicles

As per the scope of works, the rating vehicles were as follows:

- T44
- B-Double 62.5t (GML)
- Semi-Trailer 42.5t (GML)
- B-Double 68t (HML)
- Semi-Trailer 45.5t (HML)
- Garbage Truck 23t

Refer to Appendix C for load configuration details.

5.3 Structural Analysis

5.3.1 Modelling

The superstructure and substructure were modelled as a 3D spaceframe in Space Gass, the deck was modelled as a grillage structure to simulate the reinforcement in the respective directions.

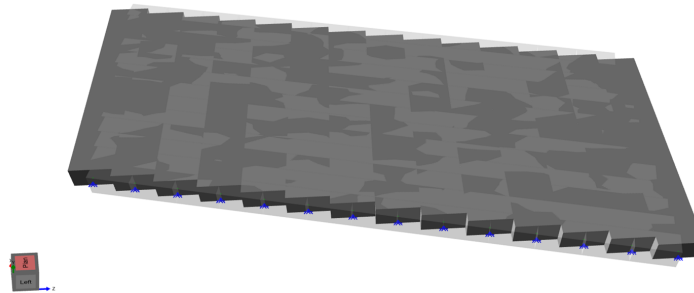


Figure 7: Perspective View of Space Gass Model

Moving vehicle loads were applied in accordance with AS5100.2, and the maximum effects were taken directly from the model for each member for each load case.

5.3.2 Material Properties

The following properties used for this assessment is based on the results from the materials testing and based on the age of the structure in line with AS5100.7. These values are provided in the table below.

Table 9: Material Properties

Material	Type	Material Property or (characteristic compressive strength)
Concrete	Structural	20 MPa

Table 10: Reinforcement Yield Strength

Element	Yield Strength
Reinforcement – Plain Round Bar	230 MPa

5.3.3 'As-Is' Sections

Due to the limited amounts of localized spalling as outlined in the condition assessment, the 'As-Is' condition is equivalent to the 'As-New' condition.

5.4 Assessment Results

5.4.1 Deck Slab

Based on the site observations, the deck slab was found to be in a fair condition, with some longitudinal cracking, possibly indicative of overloading. A summary of the 'As-New' / 'As-Is' results are provided below in Table 11.

Table 11: 'As-New' / 'As-Is' Rating Factor for Deck Slab in Span 1

Span	No. of Lanes	Action	Vehicle					
			T44	Semi GML	Semi HML	BD GML	BD HML	Garbage Truck
1	1	Bending	0.63	0.75	0.75	0.75	0.75	0.92
1	1	Shear	1.17	1.48	1.43	1.48	1.43	2.03
1	2	Bending	0.59	0.69	0.66	0.69	0.66	-
1	2	Shear	1.07	1.41	1.33	1.41	1.33	-

Noting that the bridge is used by garbage trucks on a regular basis, a reduced DLA (dynamic load allowance) factor of 0.25 was applied to provide a load rating factor greater than unity for the specified vehicle. This would result in a speed restriction of 20km/hr for the garbage trucks passing over the bridge structure. Given the geometry of the road in the vicinity of the bridge, it is unlikely that a speed of 20km/h could be safely reached by such a vehicle.

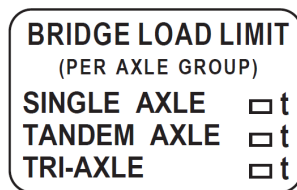
Table 12: 'As-New' / 'As-Is' Rating Factor for Deck Slab in Span 1 with reduced DLA

Span	No. of Lanes	Action	Vehicle
			Garbage Truck
1	1	Bending	1.02
1	1	Shear	2.03

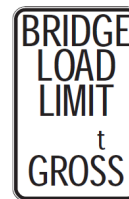
5.5 Load Limits

The following load limits have been calculated in line with RMS standard signages R6-17 and R6-3.

Restricted loads are rounded down to the nearest tonne.



RMS standard sign R6-17



RMS standard sign R6-3

Table 13: Load Limits

Controlling Load Case	Controlling Rating Factor	Axle Group	Unrestricted Load (t)	Restricted Load (t)
Semi-Trailer GML	0.75	Gross	42.5	31
	1.00	Single axle	6	6
	0.75	Tandem axle	16.5	12
	1.00	Tri-axle	20	20

It is also recommended as outlined in Section 5.4.1, that a speed restriction of 20km/hr is imposed for garbage trucks passing over the structure.

6. Conclusions

Based on the observations from the inspection, the bridge is generally in a fair condition. There are a number of localized spalls as well as longitudinal cracking.

The structure shows signs of heavy loading from the longitudinal cracking over the deck which is reflective in the load rating results.

Measured carbonation depths were recorded to be 20mm. Predictive modelling was undertaken to assess the time for the carbonation front to reach the reinforcement. Modelling has indicated that carbonation induced corrosion is a durability risk to the bridge in the presence of moisture in the next 15 years.

The analysis suggested that the structure is inadequate to support General Mass Limit vehicles. Given Le Clos Lane is not a throughfare, it is not suitable for large vehicles and the bridge provides access only to residents and council vehicles.

Council garbage trucks can continue to use the bridge, with a speed limit of 20km/h.

General fire appliances¹ can use the bridge without restrictions.

Recommended actions are provided in Section 7.

¹ https://www.fire.nsw.gov.au/gallery/files/pdf/guidelines/vehicle_access.pdf

7. Recommendations

7.1 Recommended Work

According to observations from the field inspection, investigative works and subsequent load rating analysis, the following maintenance works are recommended.

- Install load limit signage
- Impose a speed restriction on council garbage trucks vehicles travelling over the structure
- Rectify the following:
 - Deck Slab
 - o Spalling to edge of deck slab with exposed reinforcement.
 - o Crack repairs to longitudinal deck cracks
 - Gate
 - o Clean and recoat steelwork where surface rust present
- Consider replacement in the medium term

7.2 Concrete Spall Repair

7.2.1 Methodology

The following repair methodology is recommended based on that carbonation has not reached the reinforcement.

- Remove all damaged, unsound and contaminated concrete. Square cut edges (min depth 10mm for HB40). Ensure a minimum of 15mm is exposed around the bar to ensure any repair mortar can fully surround the reinforcement.
- Clean all exposed reinforcement. If significant section loss is identified (>50% in any one section), undertake the following: -
 - Break back further concrete along the affected reinforcement until the exposed reinforcement is not corroded.
 - Remove the corroded reinforcement.
 - Splice replacement reinforcement of the same diameter onto the remaining uncorroded reinforcement, either using Ancon MBT ET couplers, or providing a longitudinal splice of 400mm for N12 bars or 600mm for N16 bars
- Prime reinforcement to manufacturer's specification (e.g. Nitroprime Zincrich).
- Apply repair mortar to manufacturers specifications. Ensure that recommended application thickness is not exceeded. Where couplers have been used, ensure a minimum cover of 40mm is achieved.
- Finish patch repair to the texture of the surrounding concrete.

7.2.2 Cost Estimation

Estimated repair costs - \$15000

7.3 Concrete Crack Repair

7.3.1 Methodology

The following repair methodology is recommended.

- Clean the cracks to remove any contamination (such as oil, grease, dirt, fine particles of concrete) from the cracks with water under high pressure, or a special solvent. Blow out the residual water or solvent in the crack with filtered (dust and oil free) compressed air or allow adequate time for air drying.
- Drill holes into the crack for fitting pipe nipple entry ports for epoxy injection. A vacuum chuck and bit are useful in preventing the cracks from being choked with drill dust. Spacing of ports varies between 150mm to 500mm generally depending on the width and depth of the cracks. Establish the first and last entry ports at or near the bottom and top, respectively, of any vertical crack, or at the ends of any horizontal crack in a vertical or horizontal member.
- Mix the epoxy
- Inject the epoxy. Epoxy can be injected with hydraulic pumps, paint pressure pots or airactivated caulking guns. Select the injection pressure carefully. Increased pressure does not necessarily accelerate the rate of injection. In fact, excessive pressure can propagate the existing crack further. Carry out the injection from one end of the crack to the other end in the same manner. The crack is full only if the pressure can be maintained. If the pressure cannot be maintained, the epoxy is still flowing into the crack or leaking out from some point.
- After the crack has been sealed, remove the projecting entry ports and fill holes with an epoxy patching compound. If required, remove or even out the surface seals by grinding in order to restore the appearance of the structure

7.3.2 Cost Estimation

Estimated repair costs - \$2000

7.4 Clean Steel Work

7.4.1 Methodology

The following methodology is recommended:

- Remove all welding scale, slag and corrosion using power tools
- Degrease and remove all surface contaminants.
- Apply two coats of a zinc-rich primer (Jotun Galvanite, Dulux Zinc Rich or similar)

7.4.2 Cost Estimation

Estimated repair costs - \$700

Appendices

Item 3



Report for Inner West Council | Project Number S23198 | Page 16
S23198-REP-S-001-Le CLOS - B.docx | Version A

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Attachment 1

Appendix A - Condition Assessment

Item 3



Report for Inner West Council | Project Number S23198 | Page 17
S23198-REP-S-001-Le CLOS - B.docx | Version A

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Attachment 1

Le Clos Lane


Appendix A

Bridge Inspection Report – Condition Rating of Elements


Element Code	Description	Environment	QTY	Unit	Change	Percentage in Condition Rating				Comments
						1 – Good	2 – Fair	3 – Poor	4 – Very Poor	
CDSL	Concrete-Deck Slab / Cast In-situ Concrete	Moderate	42	m ²	None	0	20	0	22	<ul style="list-style-type: none"> - Weathering of deck slab - 2 large longitudinal cracks - 1 large longitudinal crack on side (5mm width)
RMIS	Miscellaneous Railing including Guard fence / Other	Moderate	7	m	None	0	6	1	0	<ul style="list-style-type: none"> - Surface rust on steel fence
MMAS	Brick / Masonry / Reinforced Earth / Other	Moderate	16	m ²	None	0	16	0	0	<ul style="list-style-type: none"> - Weathering throughout most of abutment brick walls
MAPP	Approach Carriageway / Other	Moderate	2	Each	None	0	2	0	0	
MWWY	Waterway / Other	Moderate	2	Each	None	0	2	0	0	
MGCL	General Cleaning / Other	Moderate	1	Each	None	0	0	1	0	<ul style="list-style-type: none"> - Graffiti observed on the channel brick wall

Le Clos Lane


Bridge Inspection Report – Required Maintenance Actions

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
37405	1.1	CDSL	Concrete-Deck Slab / Cast In-situ Concrete	Weathering is throughout most area of concrete deck slab. No change	Monitor.	20	m ²	2 years	


Le Clos Lane

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
37406	1.2	CDSL	Concrete-Deck Slab / Cast In-situ Concrete	<p>Two large longitudinal cracks to top of deck slab. No change</p> <p>One large longitudinal crack was observed along the elevation side 1 of deck slab. No change</p> <p>Cracks are approx 5mm in width. No change</p>	Undertake Crack Repairs	10	m	1 year	


Le Clos Lane

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
									


Le Clos Lane

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
37407	1.3	CDSL	Concrete-Deck Slab / Cast In-situ Concrete	Several spalls to soffit and side of deck slab with exposed reinforcement showing signs of corrosion. No change	Clean and recoat reinforcement. Patch repair concrete	15	m ²	1 year	



Le Clos Lane

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
									

Le Clos Lane

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
37404	2.1	RMIS	Miscellaneous Railing including Guard fence / Other	Surface rust observed on steel fence. No change	Clean surface rust and repaint.	2	m ²	1 year	

Le Clos Lane

ID Number	Defect Number	Element Code	Description	Defect	Required Actions	Est. Qty	Units	Date Req.	Photo
37403	3.1	MMAS	Brick / Masonry / Reinforced Earth / Other	Weathering throughout most of the abutment brick walls. No change	Monitor.	16	m ²	2 years	
37402	4.1	MGCL	General Cleaning	Graffiti observed on the channel brick wall. No change	Clean.	2	Each	2 years	

Appendix B - Rating Vehicles

Item 3

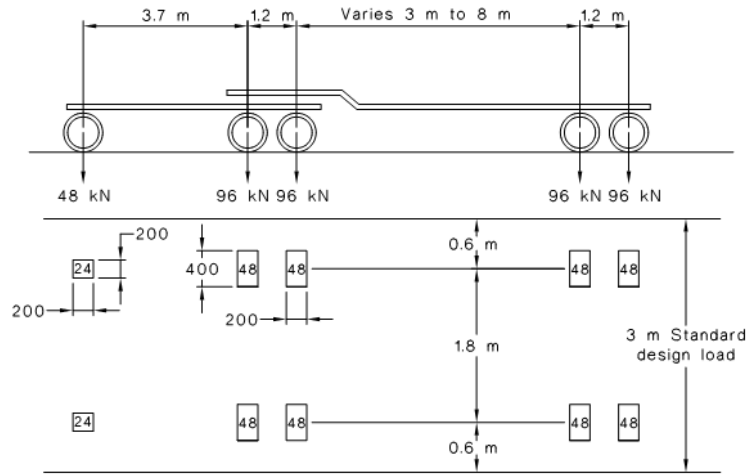


Report for Inner West Council | Project Number S23198 | Page 18
S23198-REP-S-001-Le CLOS - B.docx | Version A

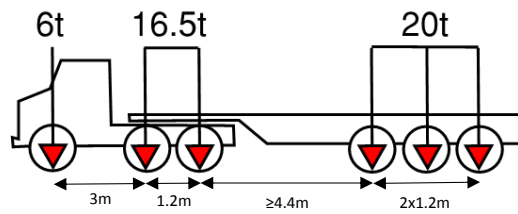
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Attachment 1

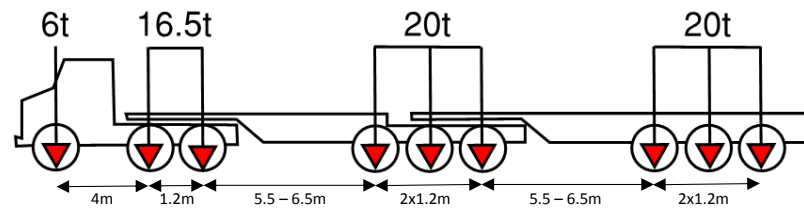
T44 Loading



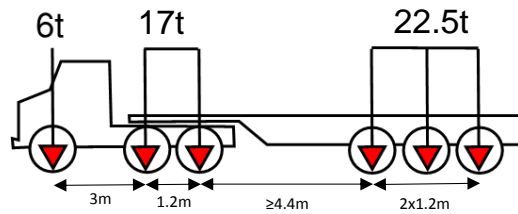
Semi Trailer GML (42.5t) Loading



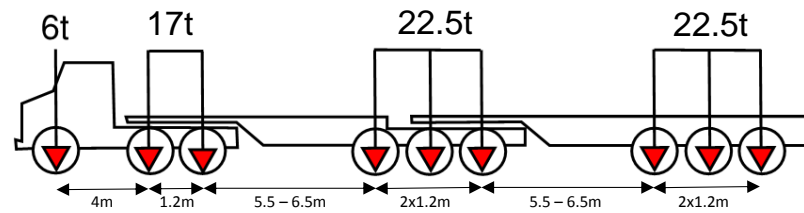
B-Double GML (62.5t) Loading



Semi Trailer HML (45.5t) Loading

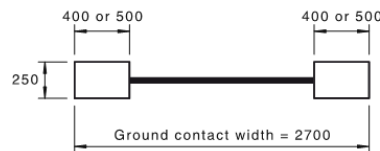


B-Double HML (68t) Loading

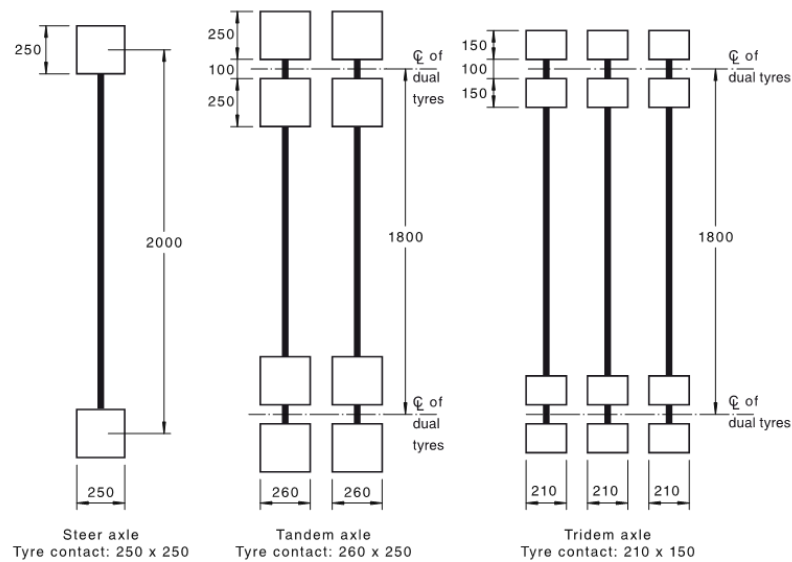


AS 5100.7:2017

22



(c) Typical crane axle configuration and tyre contact area

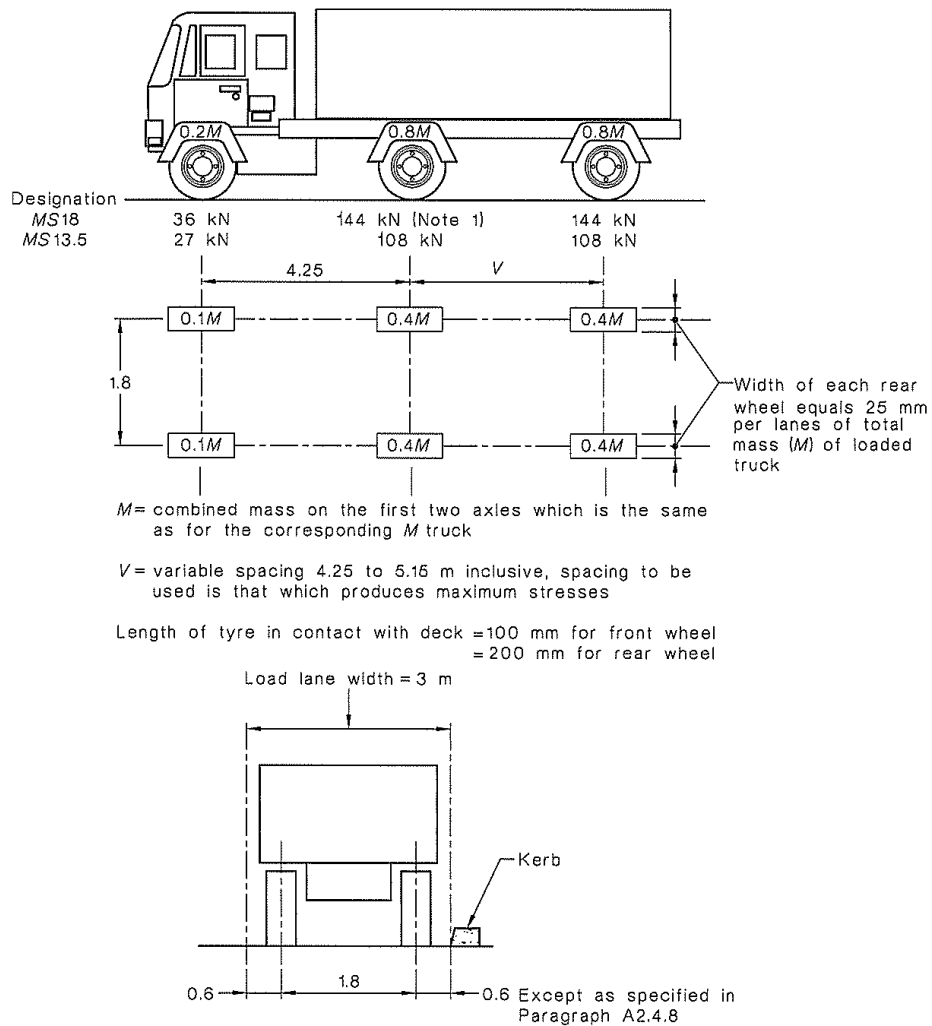


(d) Typical truck axle configuration and tyre contact area

DIMENSIONS IN MILLIMETRES

FIGURE 11.3.3 (in part) VEHICLE POSITIONING

Review of AS 5100.7 Rating of Existing Bridges and the Bridge Assessment Group Guidelines

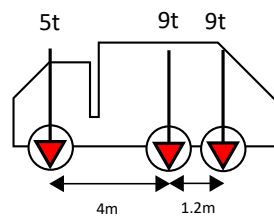


Note: In the design of steel grid and timber decks only, for MS18 loading, one axle load of 108 kN or two axle loads of 72 kN each spaced 1.2 m apart shall be used, whichever produces the greater stress, instead of the 144 kN axle shown. All other deck systems should be designed for the 144 kN axle. (A reason for the use of reduced loading on timber and steel grid decks is the ease and relative economy with which they can be replaced.)

DIMENSIONS IN METRES

FIGURE A9 STANDARD MS TRUCKS

Garbage Truck (23t) Loading



Appendix C - Component Plan

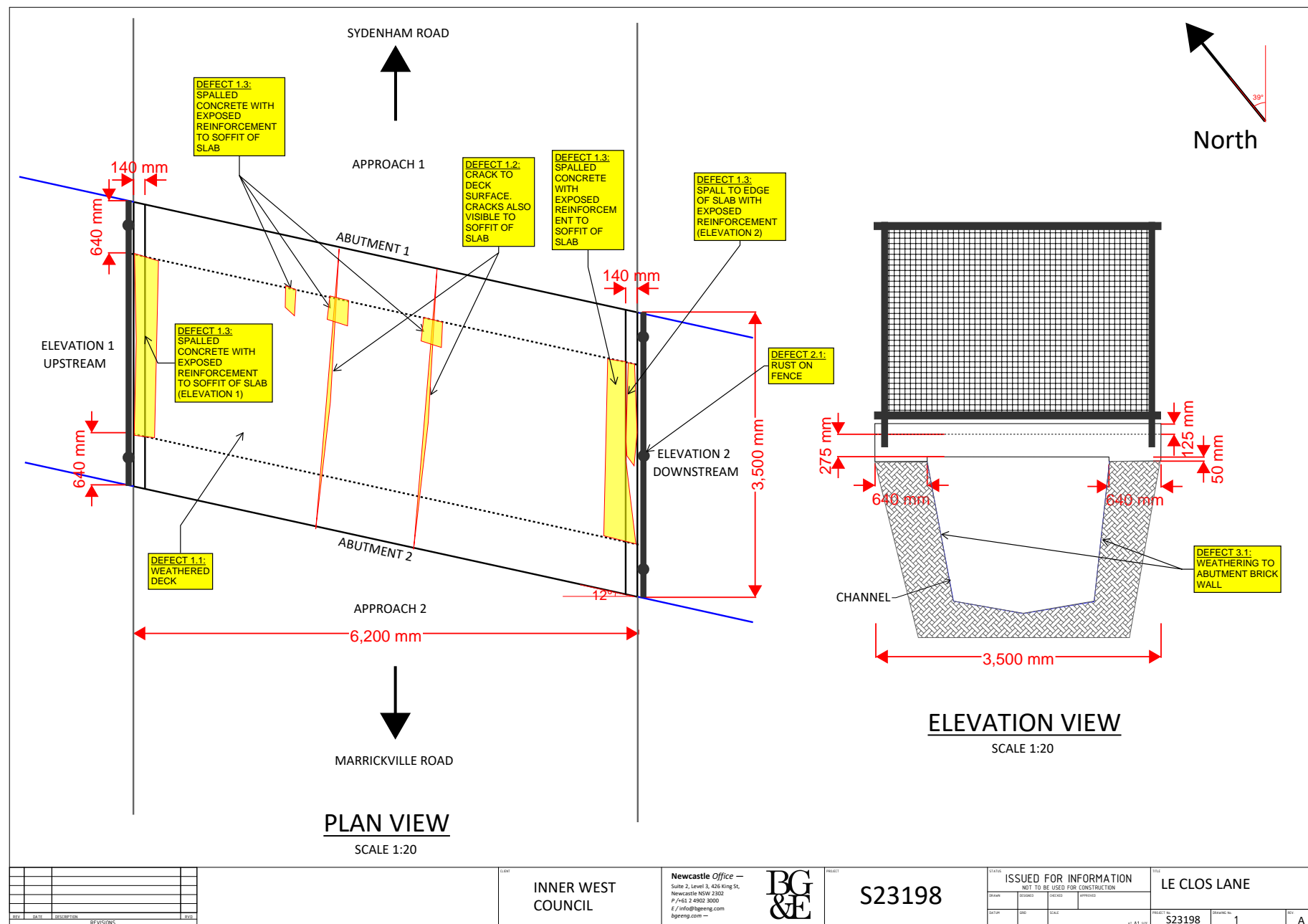
Item 3



Report for Inner West Council | Project Number S23198 | Page 19
S23198-REP-S-001-Le CLOS - B.docx | Version A

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Attachment 1





Item No: LTC1124(1) Item 4
Subject: ILLAWARRA ROAD AT THORNLEY STREET, MARRICKVILLE –
PROPOSED RAISED PEDESTRIAN CROSSING - DESIGN PLAN 10304
(MIDJUBURI-MARRICKVILLE WARD / SUMMER HILL ELECTORATE /
INNER WEST PAC)
Prepared By: Jennifer Adams - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the detailed design plan for the proposed new raised pedestrian crossing on Illawarra Road at Thornley Street, Marrickville and associated signs and line markings (as per Design Plan No.10304) be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Council has prepared a design plan to construct a raised concrete pedestrian crossing at the intersection of Illawarra Road and Thornley Street, Marrickville. The proposal aims to improve safety for pedestrian and motorist and will help address concerns with pedestrian and motorist behaviour at this location, particularly during busy times.

BACKGROUND

Council has in the past received concerns as to the safety of the pedestrian crossing on Illawarra Road near Thornley Street, Marrickville.

The Minutes of the Local Traffic Committee meeting held on 11 December 2023 were adopted at Council's meeting held on 05 March 2024 subject to the following amendments:

3. *The following amendments to LTC1223(1) Item 7 to include urgent action being taken to improve visibility of the pedestrian crossing at Illawarra Road at Yirran Gumal Early Learning Centre (refer to figure 1), including:*

- a) *a report including designs to improve visibility at the crossing be brought to the next possible Local Traffic Committee Meeting, considering options to raise the crossing, implement flashing lights, or any other measure to improve safety; and*
- b) *that upon approval by the Local Traffic Committee and Council, these works be immediately prioritised and undertaken, funded through the quarterly budget update.*

A report went to the April 2024 Local Traffic Committee recommending that "the upgrade of the existing pedestrian (zebra) crossing on Illawarra Road near Thornley Street to a raised

crossing facility be approved in principle and that the detail design be bought back to the Traffic Committee for consideration."



The existing pedestrian crossing located on Illawarra Road, immediately west of Thornley Street (and adjacent to the Yirran Gumal Early Learning Centre and Debbie and Abbey Borgia Community Recreation Centre on Illawarra Road) is an at grade crossing facility with kerb blisters and a centre refuge island. The facility is well used by the adjacent facilities and by bus patrons as bus stops are present nearby on either side of this crossing facility.

Illawarra Road is a Regional Road carrying around 14,500 vehicles per day. It is a bus route. In order to improve safety of pedestrians at this crossing and to address community concerns which have been raised, it was proposed to raise this crossing.

This report details the design plan for those improvement works and its related consultation results. It is noted that the original consultation plan sent out for consultation differs from the plan presented in this report in that the location of the proposed new tactiles is now relocated within the footpath ramps and no new bollards are included.

FINANCIAL IMPLICATIONS

The project is listed on Council's 2024/2025 PAMP Capital Program for construction and estimated cost is \$207,800. Project number is 303459.

OTHER STAFF COMMENTS

The following works are proposed and are illustrated on the attached Consultation Plan (Plan No. 10304). The proposed works aim to improve pedestrian safety by better defining the safe pedestrian crossing point and addresses concerns for pedestrian safety and driver behaviour at this location.

Specifically, the proposed scope of works includes the following:

- Construct a new raised concrete pedestrian crossing to replace existing at grade (flat) pedestrian crossing. The new pedestrian crossing will be slightly relocated from the existing location by approx. 1.3m south (refer to plan);
- Construct "gutter bridges" with heel safe grating to provide safe access over existing kerb and guttering to the new raised pedestrian crossing (where required);

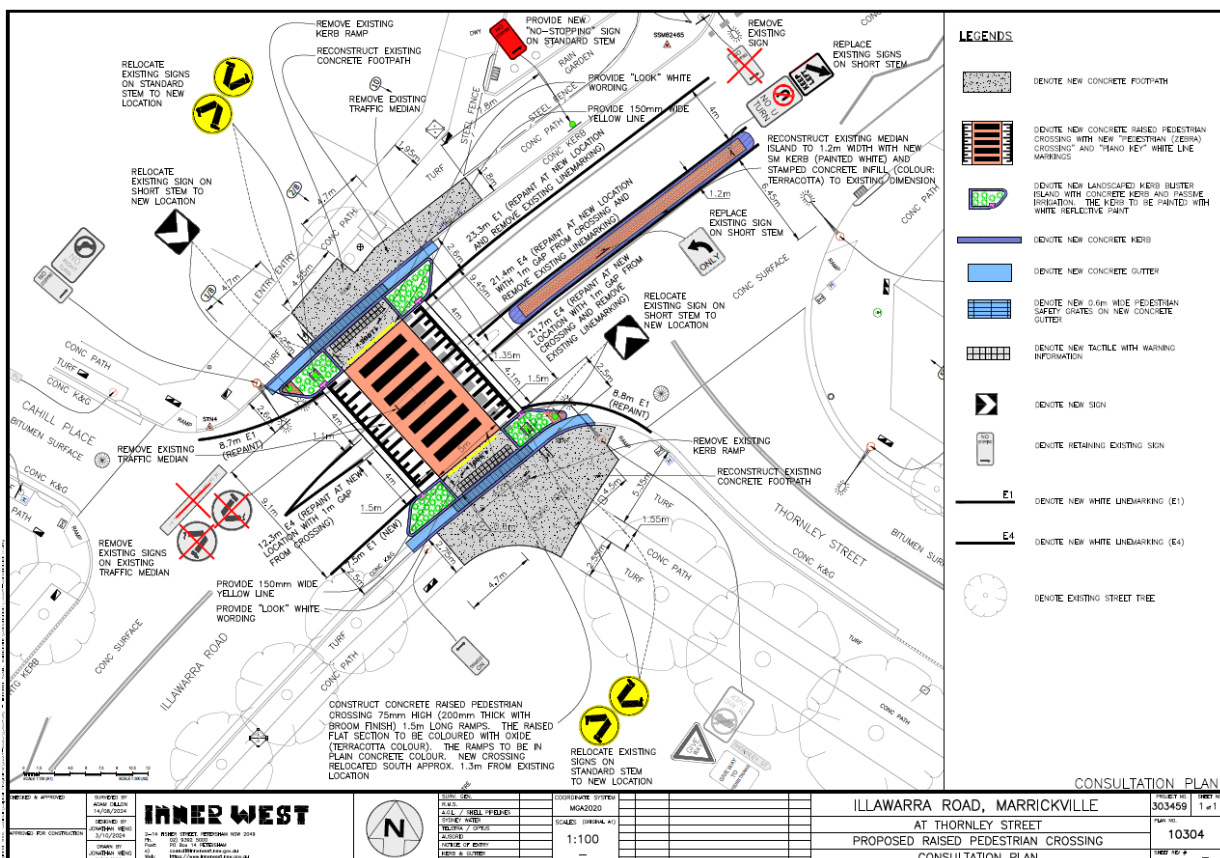
- Construct landscaped kerb blister islands in the road adjacent to the new raised pedestrian crossing. Landscaping to be suitable species of native grasses (subject to final design);
- Reconstruct existing central median island north of the new raised pedestrian crossing;
- Reconstruct some sections of damaged concrete footpath with new concrete footpath (as shown on the attached plan);
- Reconstruct sections of kerb & gutter with new concrete kerb & gutter (where shown on plan);
- Install new tactiles with bollards on either side of the new raised pedestrian crossing (where shown on plan);
- Install associated pavement line marking and signage as required;

Parking Changes

This proposal will result in no loss of on-street parking space. Some street parking will be affected temporarily to facilitate the construction activities during construction.

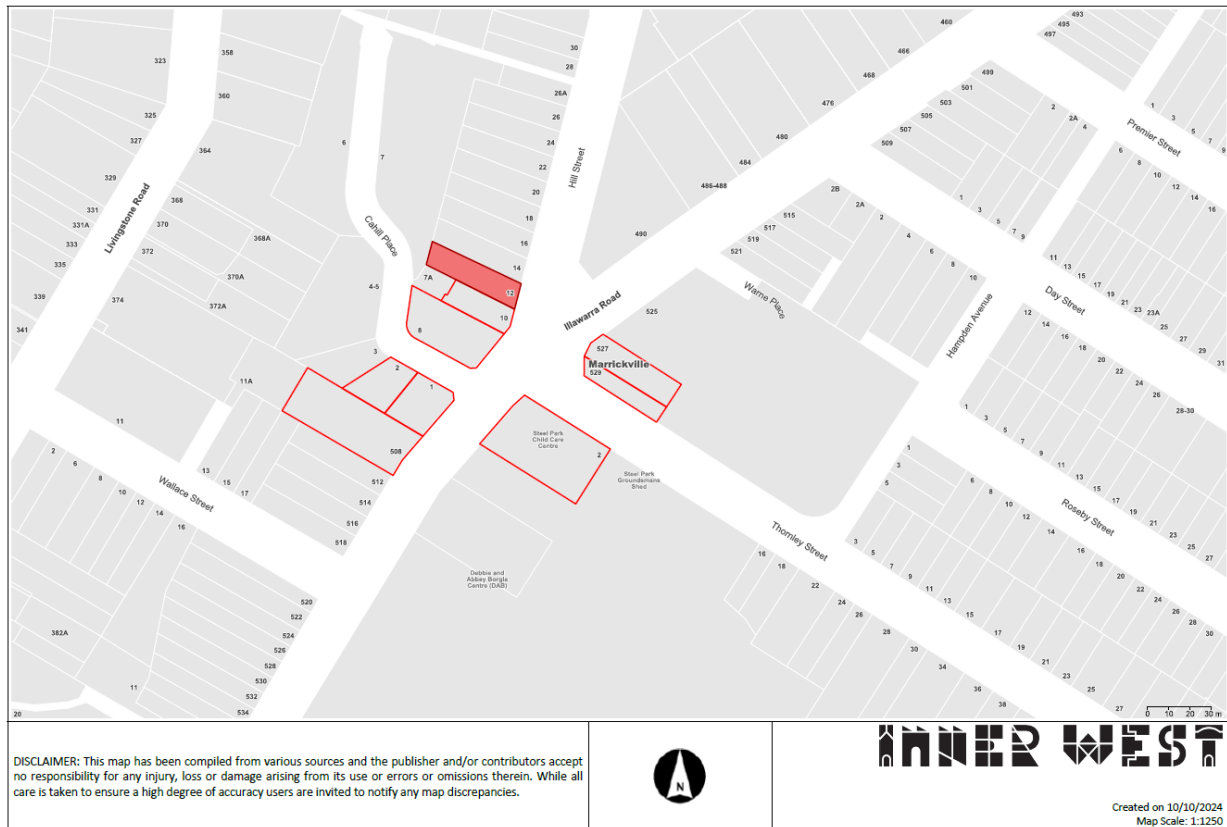
Streetlighting

The existing street lighting at the location for the proposed new raised pedestrian crossing is deemed adequate. Therefore, no new floodlights are proposed to be included as part of the works.



PUBLIC CONSULTATION

Consultation was conducted between 10 October and 30 October 2024. A letter along with a copy of the design plan was sent to residents / businesses in the immediate locality. A total of 8 letters were distributed. There were no responses.

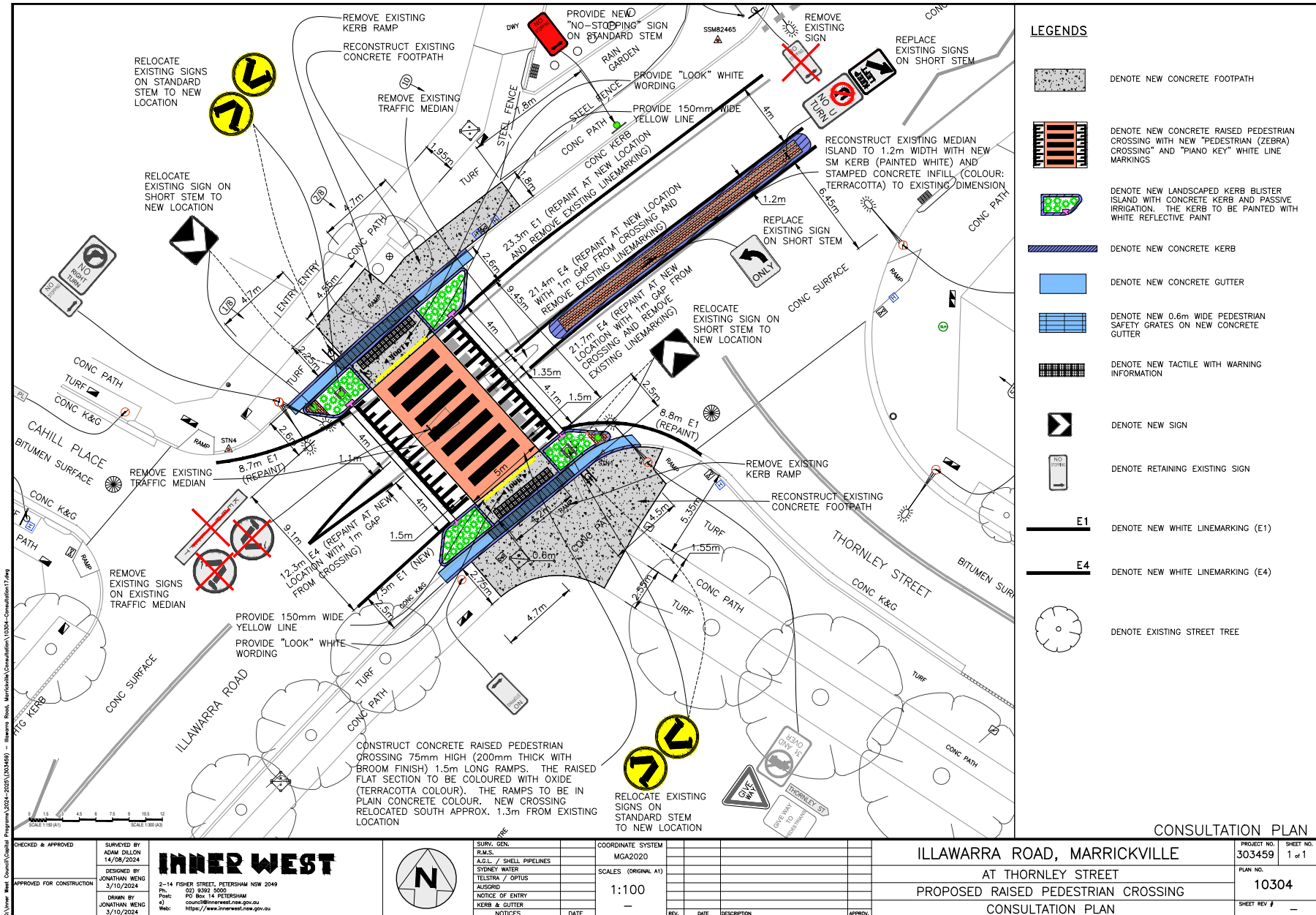


CONCLUSION

It is recommended that the detailed design plan of the proposed new raised pedestrian crossing and associated signs and line markings be supported to improve road safety at this location.

ATTACHMENTS

1. [Consultation Plan\(Amended\)](#)



Item No: LTC1124(1) Item 5
Subject: ILLAWARRA ROAD AT Warburton Street, Marrickville - PROPOSED 'ZIG ZAG' LINE MARKING FOR THE PEDESTRIAN CROSSING (MIDJUBURI-MARRICKVILLE WARD / SUMMER HILL ELECTORATE / INNER WEST PAC)
Prepared By: Daniel Li - Student/Graduate Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the installation of 'Zig Zag' line markings on both approaches to the pedestrian crossing of Illawarra Road near the intersection with Warburton Street, Marrickville be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

This report outlines current sight distance issues at the Illawarra Road pedestrian crossing at its intersection with Warburton Street, Marrickville and proposes the appropriate traffic treatments to improve the safety of the community.

BACKGROUND

Council has received several concerns from residents and pedestrians regarding vehicles not slowing down as they approach the pedestrian crossing at Illawarra Road near the intersection with Warburton Street, Marrickville.

In the past, Council Officers have investigated the raising of this pedestrian crossing. However, raising of this crossing is likely to create flooding issues and interrupt the over land flow into Station Street making such a solution complicated and expensive to implement.

Following an investigation Council Officers are proposing to install 'Zig Zag' line markings on both approaches to the crossing to indicate to motorists that a pedestrian crossing is ahead.

DISCUSSION

Council has received several concerns from residents and pedestrians regarding vehicles not slowing down as they approach the pedestrian crossing in Illawarra Road, Marrickville near Warburton Street.

A review of this site indicates that Illawarra Road has a change in horizontal and vertical alignment on approach to the pedestrian crossing, resulting in obstructed sight lines which raises safety concerns.

By providing 'zig zag' line markings, motorists will be advised of the incoming pedestrians and cyclists and will be aware of the crossing ahead, thereby improving pedestrian safety.

As such, Council is proposing the 'zig zag' line marking treatment in accordance with the below concept plan in *Figure 1*.

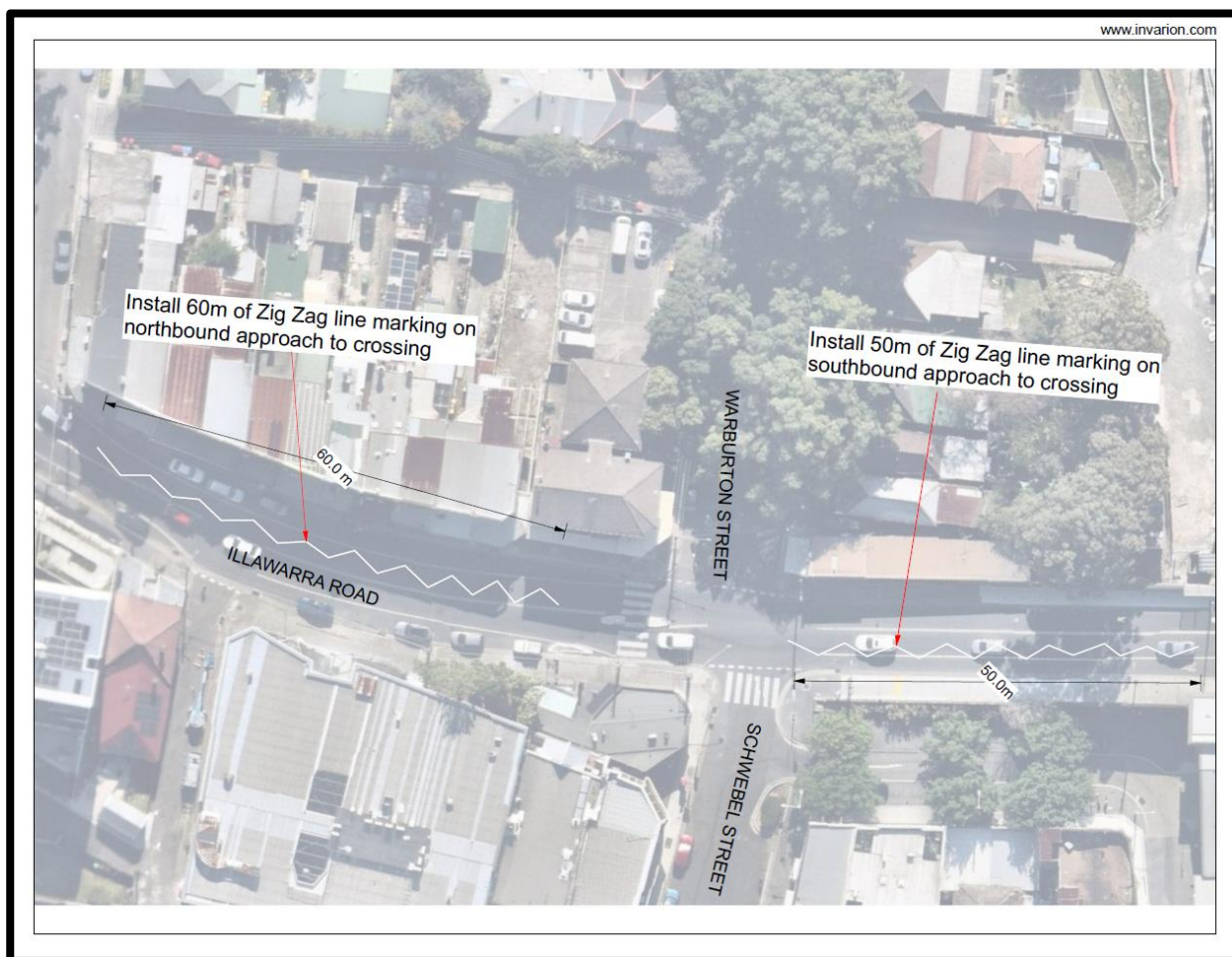


Figure 1. Proposed concept plan of Zig Zag line marking.

FINANCIAL IMPLICATIONS

The cost of the installation the of 'Zig Zag' line marking is to be funded from Council's operating budget for signs and line marking.

ATTACHMENTS

Nil.

Item No: LTC1124(1) Item 6
Subject: ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS - PART 1
(MIDJUBURI-MARRICKVILLE WARD/HEFFRON ELECTORATE/INNER WEST PAC)
Prepared By: James Nguyen - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the detailed design (WCX M5 – St Peters Interchange: Active Transport Works Community Consultation Plan, Sheets 1-8) prepared by COMPLETE URBAN for the St Peters Interchange Active Transport Works be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

This report outlines the design development of the St Peters Interchange Active Transport Works – Part 1 and notes key changes from the concept design. It also provides a summary of the community consultation that was completed. Accordingly, the design is to progress to construction.

BACKGROUND

The St Peters Interchange Active Transport Works – Part 1 project was previously referred to the Local Traffic Committee on 2 December 2019 and 6 July 2020. The concept design for this project was approved at the Local Traffic Committee meeting of 6 July 2020.

This report outlines subsequent detail design and community consultation developments from the concept design.

DISCUSSION

Table 1 below notes the changes or deviations made from the originally approved concept plan in the latest design development. The design plans are shown in *Attachment 2*:

Table 1 - Design changes/amendments

Street	Amendments/changes	Reason for change
Burrows Avenue	Remove the existing bicycle crossing on the proposed conversion of the at-grade crossing to a raised pedestrian crossing, east of George Street	New furniture/bollards installed at the Sydenham Station entrance would obstruct cyclists

Street	Amendments/changes	Reason for change
	Removal of kerb extension and reduction of the 'KISS AND RIDE' to provide four (4) new timed parking (1-hour) spaces on the northern side	Provide additional timed parking to address concerns parking loss and also provide an area for longer stay parking adjacent to the Station.
	Three (3) timed parking (2-hour) restrictions on the southern side of Burrows Avenue, east of George Street	Provide timed parking for adjacent users.
George Street	Retain the length of the existing kerb extension on the eastern side of George Street, south of Burrows Avenue (concept plan approved had a 25-metre-long kerb extension up to house no. 24 George Street)	Extended length is not required
	Retain existing speed hump on George Street, south of Unwins Bridge Road	Reconstruction to speed humps is not required due to existing humps providing speed reduction
	Retain existing speed hump on George Street, north of Henry Street	Reconstruction to speed humps is not required due to existing humps providing speed reduction
	Remark line marking and replace rumble bars at the intersection of George and Henry Streets	Improve safety at intersection
	Install wayfinding signs at the intersection of George and Henry Streets	Additional signs to improve wayfinding for cyclists
Yelverton Street	Upgrade/replace existing warning signs (W6-7 and W8-23) on Yelverton Street	Improve safety for cyclists
	Remark line marking and replace rumble bars at the intersection of Yelverton and Henry Streets	Improve safety at intersection
Frederick Street	Upgrade/replace existing warning signs (W6-7 and W8-23) on Frederick Street	Improve safety for cyclists
	Remark line marking and replace rumble bars at the intersection of Frederick and Henry Streets	Improve safety at intersection

Street	Amendments/changes	Reason for change
Sutherland Street	Upgrade/replace existing warning signs (W6-7 and W8-23) on Frederick Street	Improve safety for cyclists
	Remark line marking and replace rumble bars at the intersection of Henry and Sutherland Streets	Improve safety at intersection
Henry Street	Install wayfinding signs at the intersection of Grove and Henry Streets	Additional signs to improve wayfinding for cyclists
Grove Street	Install wayfinding signs at the intersection of Grove Street and Bakers Lane	Additional signs to improve wayfinding for cyclists
Alfred Street	Upgrade/replace existing warning signs (W6-7 and W8-23) on Alfred Street	Improve safety for cyclists
Bakers Lane	Install 'GIVE WAY' sign and line on Bakers Lane at the intersection of Mary Street (included following feedback from community consultation)	Improve safety at intersection

In addition to the above changes, a request has been made to consider changing the 'STOP' priority controls on Henry Street at the intersections of Sutherland, Frederick and Yelverton Streets to give road users along the minor leg, particularly cyclists priority.

Council engineers have assessed the viability of changing the 'STOP' priority controls on Henry Street at Sutherland, Frederick and Yelverton Streets. 'STOP' controls are installed at four-way intersections on the minor road. Based on the existing traffic volumes as shown in Table 2 below, both Sutherland and Frederick Street have higher levels of traffic volume, compared to Henry Street, and changing the 'STOP' priority control is not recommended.

The traffic volume on Yelverton Street is similar to Henry Street, and Council engineers are investigating further whether changing the 'STOP' priority control at this intersection can be considered.

Table 2 - Traffic volumes

Street	Volume (vpd)
Henry Street	~536
Sutherland Street	1086
Frederick Street	1250
Yelverton Street	415

PUBLIC CONSULTATION

The proposal was publicly exhibited on Your Say Inner West between 7 October 2024 and 5 November 2024. There were 50 responses received with 39 responses (78%) supporting and six (6) responses (12%) opposing the proposal. Five (5) responses received were unsure about the proposal.

The responses have been summarised into themes and shown in Table 3 below:

Table 3 - Community consultation summary

Theme	Community feedback	Council response
Connection to Inner West Bike Network	Many respondents desire expanded connections to key areas, including Wolli Creek, Mascot, Marrickville, and the SW Metro corridor, while ensuring safe and well-connected endpoints to avoid detours and unfinished segments.	Connection to Wolli Creek will be provided via new proposed Marrickville Road East (Sydenham Station to Meeks Road Marrickville along Marrickville Road) and EWPC (between Edgeware Road, Marrickville, and Garnet Street Dulwich Hill) projects. On the other end, the proposed cycleway will link with the existing cycleway at St Peters interchange.
Location of Bi-directional Cycleway on Burrows Road	Respondents suggest relocating the cycle path to the western side of Burrows Avenue for better access to the station, reduced pedestrian conflict, minimized impact on parking, and enhanced opportunities for local dining, while avoiding the need for cyclists to cross the road in high-traffic areas.	The bi-directional cycleway on Burrows Avenue was strategically placed on the side opposite the station due to multiple uses and parking restrictions on the station side. These restrictions include 'No Parking' zones, car share spaces, taxi stands, and a mailbox, which made this placement unfeasible. TfNSW, who is funding the project, required that the cycleway not be placed on the northern (station) side of Burrows Avenue due to potential conflicts with various users, including those accessing the train station, car share parking, train and taxi drop-offs and pick-ups, and the mailbox. During rail line shutdowns, when replacement buses are in operation, this side of Burrows Avenue is also

Theme	Community feedback	Council response
		<p>used for passenger drop-offs, pick-ups, and temporary bus stops.</p> <p>For these reasons, TfNSW did not endorse locating the cycleway on the northern side of the road.</p>
Parking	<p>Opinions on parking removal are mixed, with some advocating for reduced parking to promote sustainable transport, while others express concern over the impact on local businesses, residents, and overall livability, suggesting alternatives like shared paths, timed parking, and better enforcement of existing parking limits.</p>	<p>Adequate "No Parking" spaces have been designated to accommodate pick-up and drop-off needs during peak times, resulting in minimal impact on parking availability. Only parking on Burrows Avenue has been affected; however, Council has minimised disruption by rearranging the existing spaces. Instances of parking overstay on Swaine Street will be referred to our rangers for enforcement. The proposed time restrictions for parking areas have been carefully selected to support the operation of the train station, nearby businesses, and peak demand for pick-up and drop-off.</p> <p>Overall, the proposal will result in a net increase in available parking in the area.</p> <p>The proposed parking restrictions will be actively monitored, and adjustments can be made in the future if deemed necessary.</p>
Intersection of Burrows Avenue and George Street	<p>Respondents suggest moving the stop line on George Street to prevent blocking cyclists, upgrading the Burrows Avenue crossing to a raised cycle and zebra crossing, and redesigning the intersection with George Street as a continuous footpath to provide a smoother and more attractive route for cyclists.</p>	<p>The bicycle route has been designed in compliance with all relevant standards and guidelines.</p> <p>A raised treatment at the intersection was deemed unnecessary, as the main bicycle path continues along George Street, not directly to the station entrance.</p> <p>Continuous footpath treatment across George Street is not feasible due to stormwater flow considerations and is currently not warranted based on pedestrian traffic counts.</p> <p>This will continue to be monitored in the future for any necessary adjustments.</p>
Henry Street Priority	<p>Respondents advocate for prioritizing cyclists along Henry Street by reorienting stop signs to cross streets,</p>	<p>The possibility of adjusting priorities at Henry Street intersections was thoroughly considered, with traffic counts collected for pedestrians,</p>

Theme	Community feedback	Council response
	implementing "No Stopping" zones for clearer sightlines, and making Henry Street the through route to improve cyclist safety, reduce stop-starting, and enhance traffic calming for local residents.	vehicles, and cyclists. Findings indicate that only the intersection between Yelverton Street and Henry Street meets criteria warranting a priority change, which Council is currently evaluating. All other intersections will retain their existing arrangements. Cyclist safety was a key factor in this decision-making process, and Council will continue to monitor the effectiveness of these arrangements.
Signalised Intersection of Unwins Bridge Road and George Street	Respondents suggest improving pedestrian and cyclist safety at the Unwins Bridge Road and George Street intersection by adjusting signal phasing, adding a pedestrian crossing, creating advanced stopping areas and green bike zones, and shifting cycle lanes to the center to reduce conflicts with left-turning vehicles and improve lane legibility.	The proposal includes the upgrade of an existing signalised intersection at George Street and Unwins Bridge Road. In collaboration with Transport for NSW (TfNSW), the Council has ensured that the upgraded signals provide protected bicycle access concurrent with pedestrian access, allowing safe passage for both cyclists and pedestrians without motor vehicle conflict. The new configuration incorporates cycle lanterns, enabling cyclists to move during the pedestrian phase, signalled by a green cycle symbol. This enhancement maintains existing traffic priorities and overall intersection phasing, with the only change being the integration of cyclists into the pedestrian phase. The positioning of turning vehicles has been strategically designed to accommodate turning movements and ensure clear sight lines.
Traffic Matters	Respondents appreciate the upgrades near Sydenham Station but emphasise the need for safer cycle paths, lower speed limits (suggesting 30km/h), and improved intersections, while expressing concerns over the removal of the right-turn lane from Burrows to Gleeson, which impacts Swain Street residents, and suggesting a zebra crossing on Roberts Lane for shared path users.	Improvements to the intersection of Burrows, Bolton, and Hogan Streets were undertaken a few years ago as part of TfNSW's enhancements to Sydenham Train Station. It is important to note that options for intersection improvements are somewhat limited due to the regular presence of heavy vehicles, such as semi-trailers. The Council has strategically chosen routes that avoid streets with high traffic volumes. In other areas, efforts have been made to provide clear separation between vehicles and cyclists. Additionally, the Council is working on reducing speeds in the

Theme	Community feedback	Council response
		<p>vicinity by implementing a 40 km/h speed limit. Further reductions in speed are not proposed currently. Wherever feasible, amendments will be made to the existing traffic infrastructure to align with current standards. Council has collaborated closely with TfNSW on signalized intersections, and adjustments were necessary to accommodate turning movements of larger vehicles. TfNSW has reviewed and approved the proposed intersection designs.</p> <p>The Council remains committed to enhancing cycleway connections within our LGA and will closely monitor the new arrangements post-construction to ensure they function as intended.</p>
Bakers Lane Ends Priorities	<p>Respondents express concerns about safety at the Mary Street and Bakers Lane intersection, suggesting a dedicated cycle path, improved signage, traffic calming measures, and lane modifications to reduce hazards from heavy traffic, poor sight lines, and shared paths, while proposing one-way traffic on Bakers Lane to better accommodate pedestrians and cyclists.</p>	<p>Initially, warning signs will be installed, followed by Give Way signs at Bakers Lane. A speed hump is in place to slow traffic, given the limited sight lines.</p> <p>The position of the bicycle symbols has been adjusted, shifting them closer to the kerb at the intersection of Bakers Lane and Mary Street to enhance visibility.</p> <p>The current line markings shown on the plan are deemed sufficient. Additionally, a bicycle warning sign will be placed on Mary Street near its intersection with Bakers Lane.</p> <p>The cycleway design utilises local streets due to their low traffic volumes and speeds, providing a safer environment for cyclists. Mary Street includes a shared path, ensuring cyclists remain separate from vehicular traffic.</p> <p>Due to its high traffic volume, making any changes to the priority or perceived priority between Mary Street and the side streets is considered unsuitable, as it could significantly impact safety. Grove Street, with its low volume and 40 km/h speed limit, offers a safe route for cyclists.</p> <p>This proposal was developed by a</p>

Theme	Community feedback	Council response
		team of designers as the optimal solution, and TfNSW has reviewed and approved the design as acceptable.
Rumble Bars	Respondents question the necessity and benefits of rumble bars, expressing concerns about their impact on road usability for both cars and bicycles, and suggest exploring a more secure installation method to prevent dislodgement.	Rumble bars are used at the intersection to control vehicle speeds and manage turning movements, necessitated by the narrow roadways and limited alternative measures. It is important to note that no new rumble bars are being introduced; the proposal only includes the renewal of the existing ones.
Stormwater Upgrade	There is a major stormwater management issue at the northern end of Hogan Ave.	Stormwater infrastructure upgrades at the intersection of Burrows, Bolton, and Hogan will be incorporated in this project.

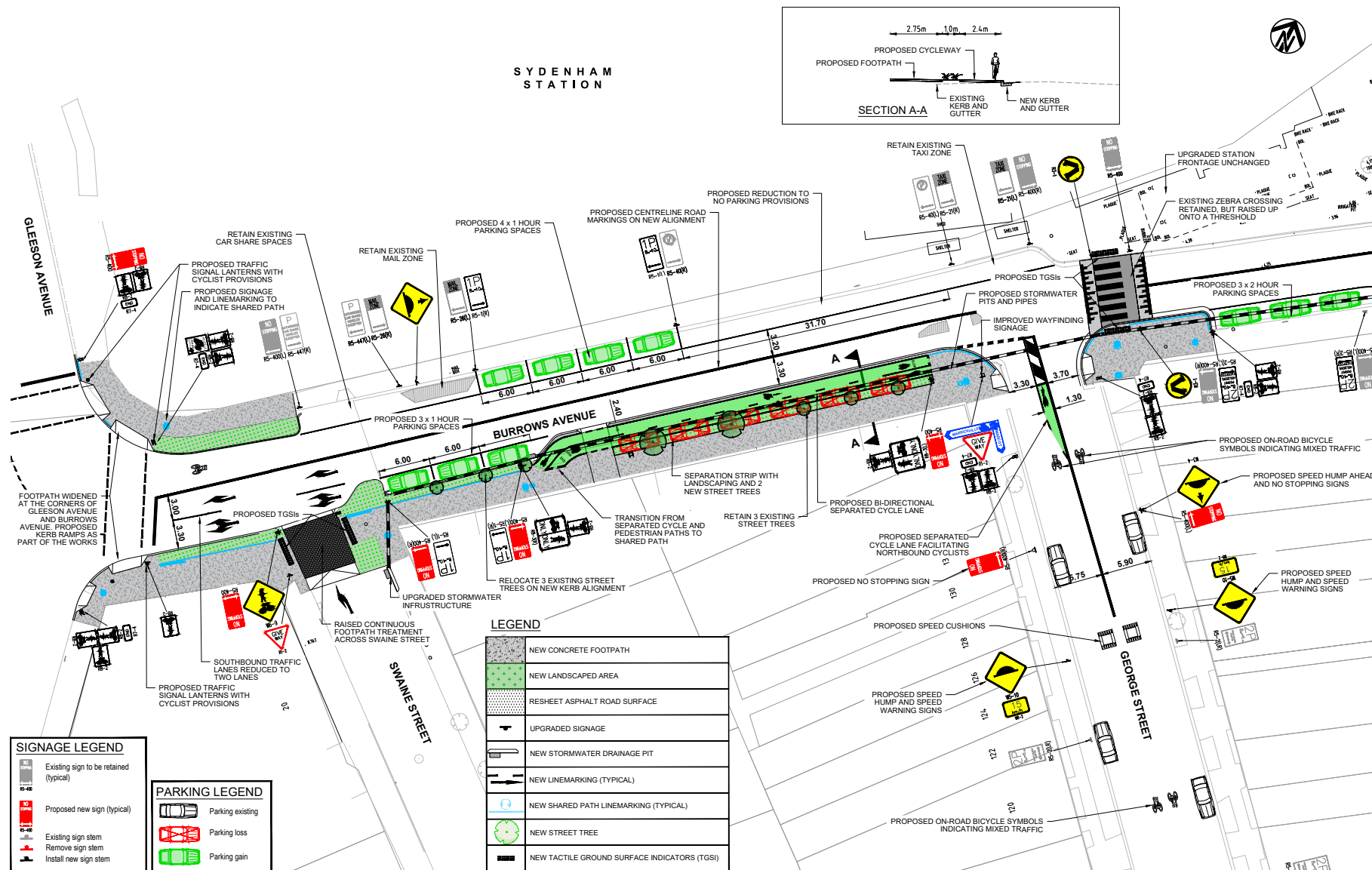
FINANCIAL IMPLICATIONS

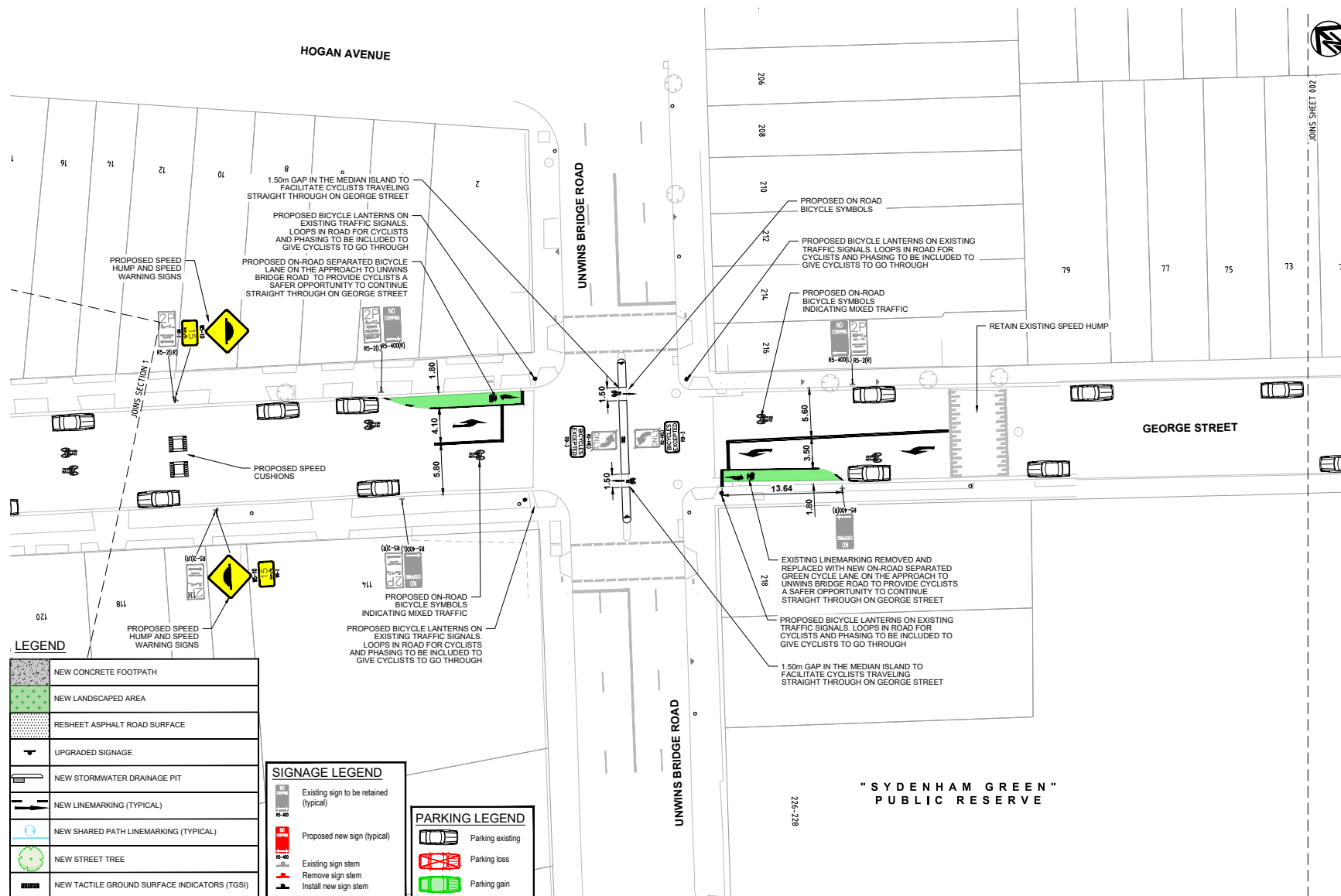
The cost of the project is to be funded by Transport for NSW.

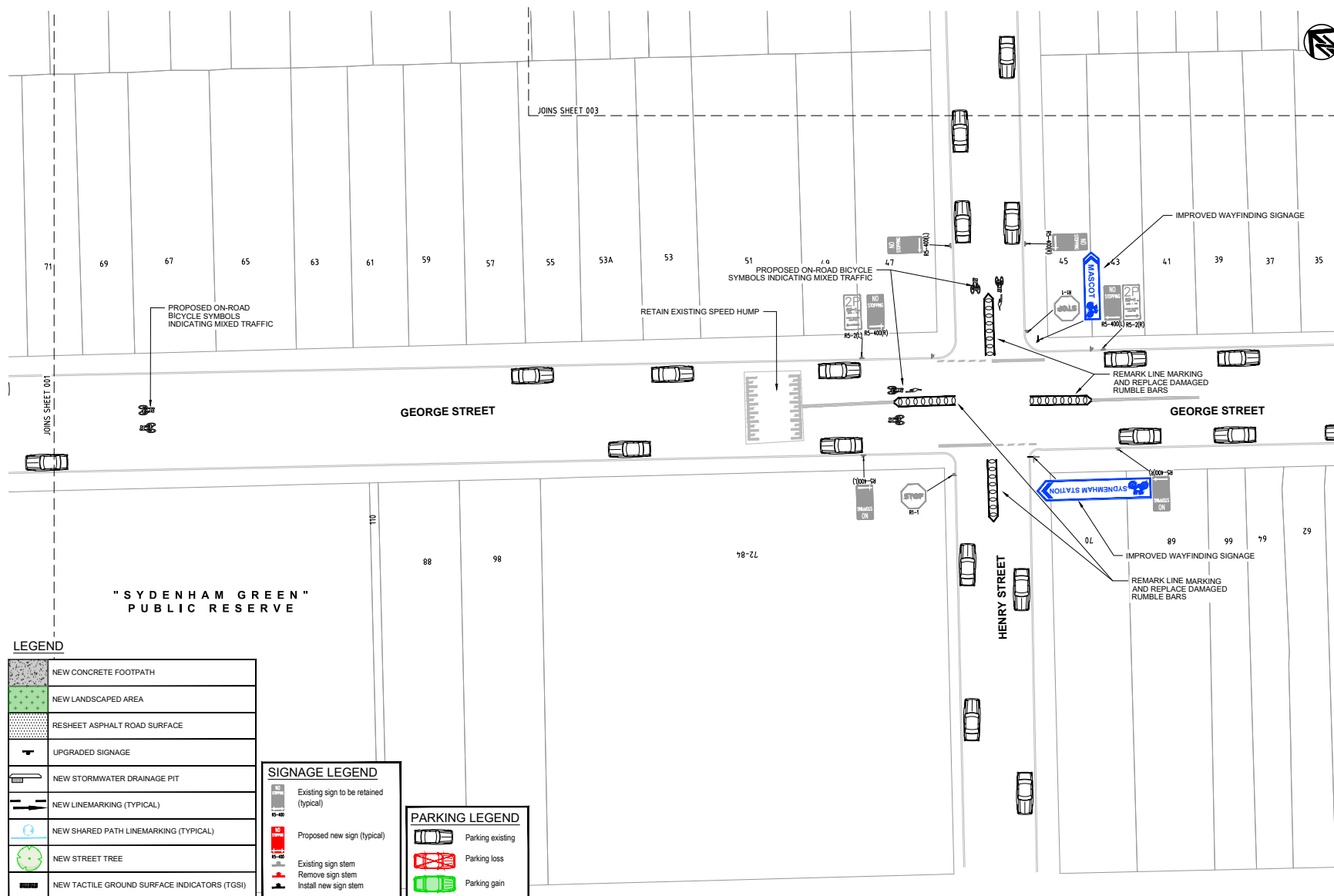
ATTACHMENTS

1. [Download](#) Final design plan

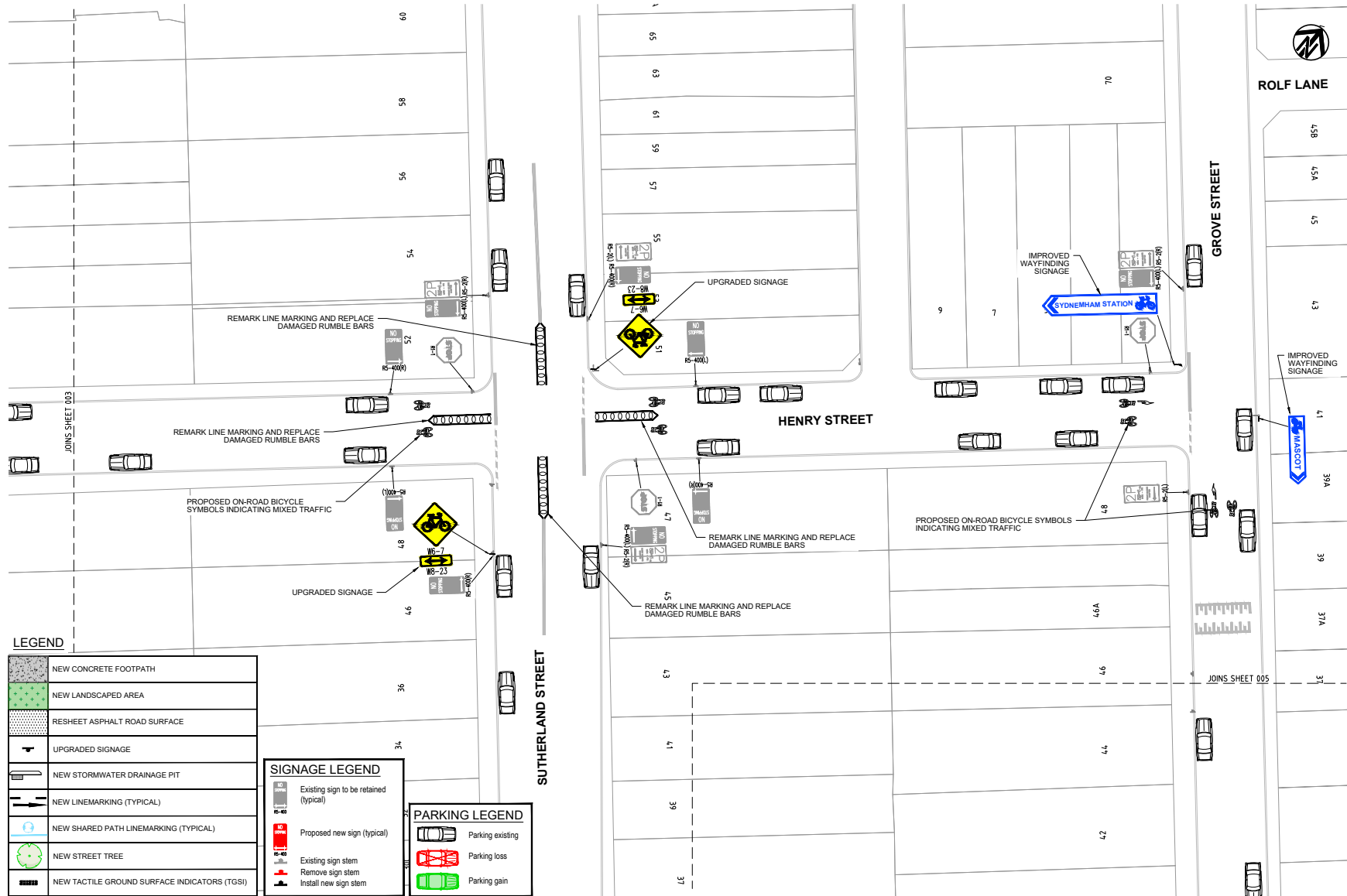


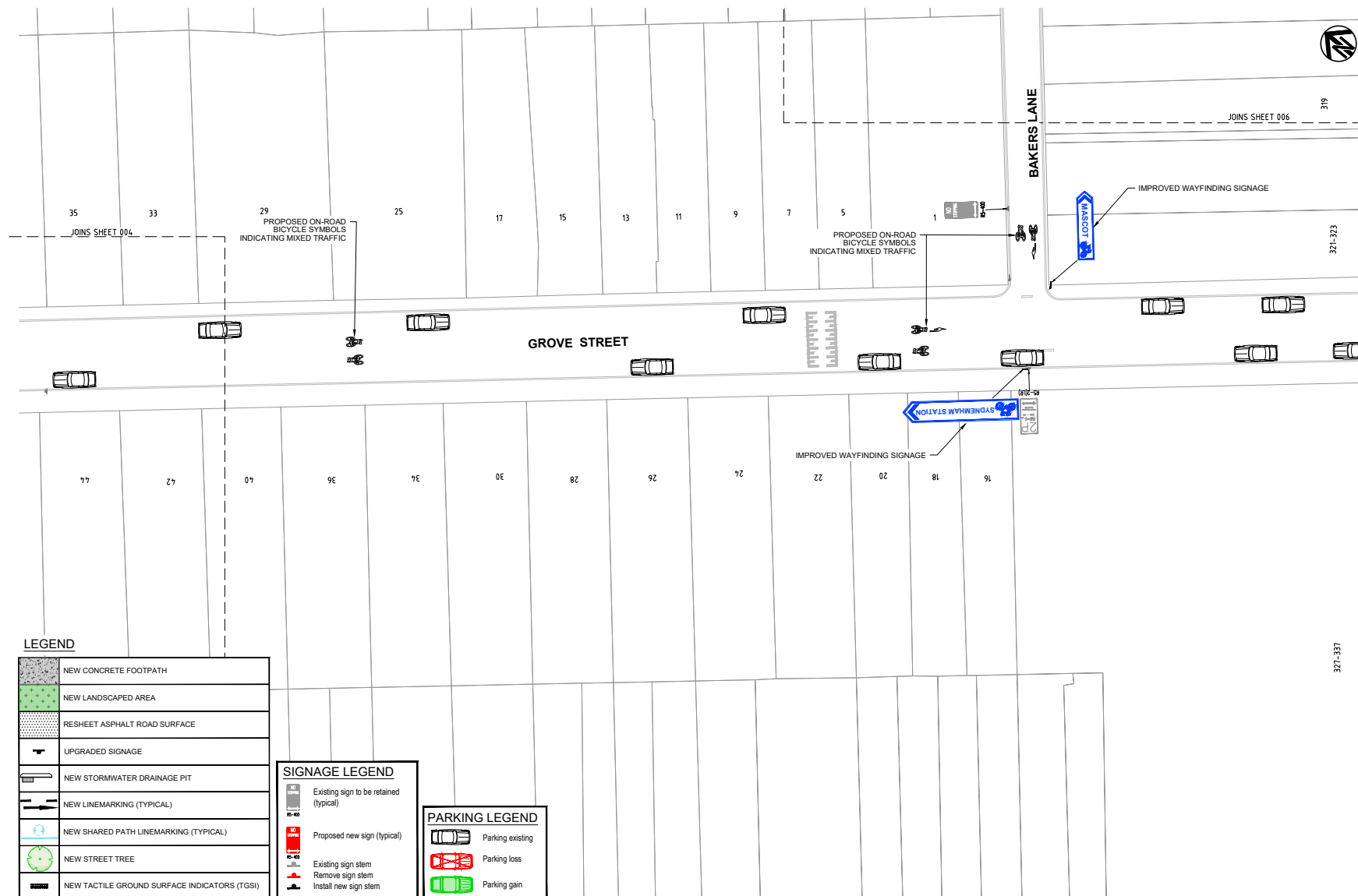


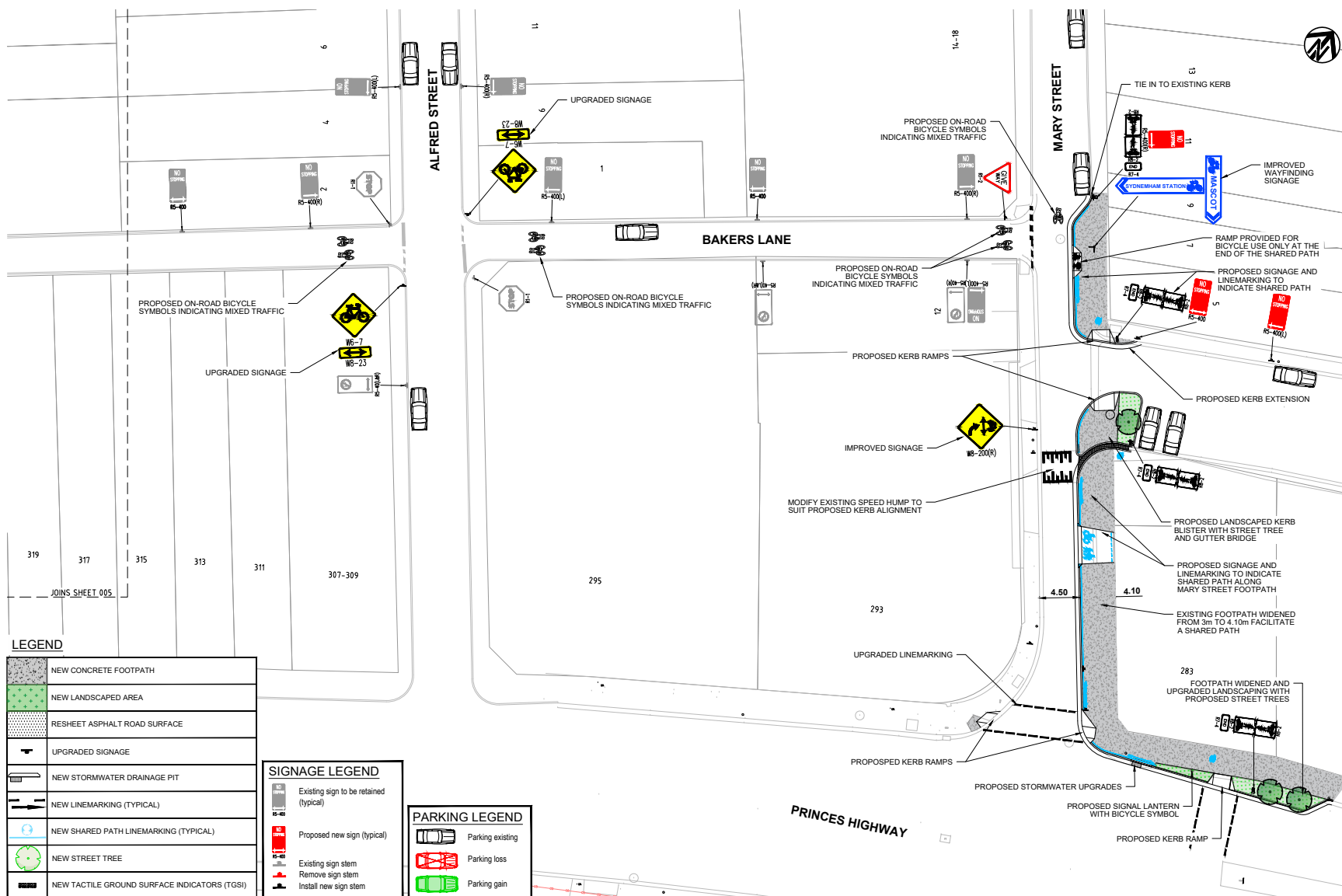












Item No: LTC1124(1) Item 7

Subject: ABERGELDIE STREET AT ARLINGTON STREET, DULWICH HILL – PROPOSED INSTALLATION OF STATUTORY ‘NO STOPPING’ CONTINUOUS YELLOW LINE MARKING RESTRICTIONS AT THE INTERSECTION (DJARRAWUNANG - ASHFIELD WARD/ SUMMER HILL ELECTORATE/ INNER WEST PAC)

Prepared By: Jennifer Adams - Traffic Engineer

Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

1. That unbroken yellow lines (statutory 10 metre ‘No Stopping’ lines) be approved for installation on both sides of all approaches to the intersection of Abergeldie Street at Arlington Street, Dulwich Hill as follows in order to deter illegal parking, increase safety and improve motorist visibility and access for turning motorists:
 - a) Install solid yellow line marking on Abergeldie Street (northern side) for a distance of 10 metres west of Arlington Street,
 - b) Install solid yellow line marking on Abergeldie Street (northern side) for a distance of 10 metres east of Arlington Street,
 - c) Install solid yellow line marking on Abergeldie Street (southern side) for a distance of 10 metres west of Arlington Street,
 - d) Install solid yellow line marking on Abergeldie Street (southern side) for a distance of 10 metres east of Arlington Street,
 - e) Install solid yellow line marking on Arlington Street (western side) for a distance of 10 metres north of Abergeldie Street,
 - f) Install solid yellow line marking on Arlington Street (eastern side) for a distance of 10 metres north of Abergeldie Street,
 - g) Install solid yellow line marking on Arlington Street (western side) for a distance of 10 metres south of Abergeldie Street,
 - h) Install solid yellow line marking on Arlington Street (eastern side) for a distance of 10 metres south of Abergeldie Street.
2. That Council Rangers be advised in terms of this report.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council’s Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Council has received concerns regarding vehicles obstructing sightlines and traffic movement by parking within the statutory ‘No Stopping’ zones at the intersection of Abergeldie Street and Arlington Street, Dulwich Hill.

In order to alleviate this issue, it is proposed to install 10-metre statutory “No Stopping” yellow line zones on all legs of the intersection at Abergeldie Street and Arlington Street, Dulwich Hill.

BACKGROUND

In accordance with the Australian Road Rules (ARR 170-3), a 'No Stopping' zone is mandatory from an intersecting road without traffic signals. Generally, a statutory distance of 10 metres is signposted and/or marked on the road by a solid yellow unbroken line.

RMS Technical Directions state that signposting and/or marking with a solid yellow line at an unsignalised intersection (without pedestrian crossing) "should only be required where there is a compliance problem or there is adjoining signposting". In this case, there is evidence that of a compliance problem which is causing problems for motorists wanting to turn through the intersection.

In order to avoid signage clutter in the area, continuous yellow edge linemarking is proposed. An unbroken yellow kerb line is a NO STOPPING line. Under the Road Rules, a driver must not stop at the side of a road marked with a continuous yellow edge line unless there is a medical or other emergency. The yellow lines are enforceable.

A consultation letter went out to the surrounding residents detailing the proposal to install statutory 'No Stopping' restrictions at the intersection to improve access through the intersection. (Refer to map below). This report gives a summary of the results of that consultation.



PUBLIC CONSULTATION

A notification letter was posted out to 8 properties in the immediate locality in both Abergeldie Street and Arlington Street, Dulwich Hill. on 14 October 2024.

The closing date for submissions ended on Friday 8 November 2024. There were no responses.

FINANCIAL IMPLICATIONS

The cost of the installation of the painted road markings associated with the proposed 'No Stopping' is to be funded from Council's operating budget for signs and line marking.

CONCLUSION

It is recommended that the statutory 'No Stopping' restrictions, in the form of unbroken yellow lines, be installed on both sides of all approaches to the intersection of Abergeldie Street and Arlington Street, Dulwich Hill for the distances as indicated in order to deter illegal parking, improve visibility and access and increase safety for all road users.

ATTACHMENTS

Nil.

Item No: LTC1124(1) Item 8
Subject: CARRINGTON STREET, LILYFIELD - CONSTRUCTION TRAFFIC MANAGEMENT PLAN (BALUDARRI-BALMAIN WARD/BALMAIN ELECTORATE/LEICHHARDT PAC)
Prepared By: Amir Falamarzi - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the temporary full road closure of Carrington Street (between Joseph Street and Balmain Road), The Boulevarde (between Joseph Street and Balmain Road) and partial temporary on-street parking removal along Balmain Road (between Helena Street and Grove Street) be approved for the purpose of the installation of a modular building at No.12 Carrington Street, Lilyfield for a 2 day period in the week commencing 24 March 2025, with a four week contingency, subject to the following conditions and all standard Council conditions for temporary full road closures:

- a) A Road Occupancy License application be obtained from the Transport Management Centre for road closure on Balmain Road.
- b) Road, footpath and car park occupancy permit to be obtained from the Council for road closure on The Boulevardee.
- c) Temporary parking arrangements must be provided for the existing disabled parking bays on Balmain Road, Carrington Street and The Boulevardee which will be affected during the work and should be located as close as possible to their original locations.
- d) NHVR permit to be obtained prior to the work.
- e) Notice of the proposed event be forwarded to the NSW Police Local Area Commander, Transit Systems, the NSW Fire Brigades and the NSW Ambulance Services.
- f) All residents and businesses in and around the affected area are to be notified of the temporary road closure in writing by the applicant in advance (at least 7 days prior to the event) with the applicant making reasonable provision for stakeholders.
- g) On-street parking to be maintained for local residents along the east side of The Boulevarde (between Joseph Street and the laneway south of Balmain Road).
- h) Vehicular access to the laneway south of Balmain Road to be maintained within the road closure zone.
- i) Authorised pilot vehicles to accompany and manage the trucks at major intersections including City West Link Road at Balmain Road, Balmain Road at Perry Street, Balmain Road at The Boulevarde and Balmain Road at Carrington Road.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Inner West City Council has received a Temporary Road Closure (TMP) application from Modscape (*Attachment 1*) seeking an in-principal approval for delivering a modular building at No.12 Carrington Street, Lilyfield in the week commencing 24 March 2025. As part of this road closure, there will be full road closure on Carrington Street (between Joseph Street and Balmain Road), The Boulevarde (between Joseph Street and Balmain Road) and partial temporary on-street parking removal along Balmain Road (between Helena Street and Grove Street).

BACKGROUND

Modscape and its client are in the final stages of planning for a proposed new dwelling at 12 Carrington Street, Lilyfield NSW. The proposed building is to be a pre-fabricated building constructed off-site. The proposed building methodology is a full welded steel structure which comprises 6 modules in total. Each module will be independent when fabricated off-site and transported individually, but upon installation on-site, the dwelling will be welded and bolted together to form one complete diaphragm.

DISCUSSION

Site and the proposed development

The Site is Lot 12 of DP1063821 which occupies a rectangular shaped block of approximate 271.9m² with a street frontage to 12 Carrington Street, Lilyfield NSW. It is proposed to demolish the existing house and construct a new single dwelling on the property. The Site is adjoined by residential dwellings, while there are several parks, school, playgrounds, early centers and other types of accommodation in the surrounding area.

Construction vehicle route

All trucks involving the delivery of the work will follow the proposed route to Carrington Street including City West Link, Balmain Road, The Boulevarde (temporary truck standby zone) and Carrington Street. As part of this route:

- Authorised pilot vehicles will accompany and manage the trucks at major intersections including City West Link Road at Balmain Road, Balmain Road at Perry Street, Balmain Road at The Boulevardee and Balmain Road at Carrington Road.
- It is proposed that the laydown area for the articulated trucks is along The Boulevarde which has been selected to minimize disruption to traffic on Balmain Road and its width to enable module de-hoarding.
- Modscape has confirmed that a swept path assessment has been undertaken and was deemed achievable.

Traffic and Parking management plans

The proposed traffic and parking management plans include:

1. Work area and where the crane will be located.
2. Full road closure on Balmain Road between May Street and Grove Street.
3. Full road closure on Carrington Street between Balmain Road and Joseph Street.
4. Full road closure on The Boulevarde between Balmain Road and Joseph Street.
5. Detour plan
6. Parking removal along north side of Balmain Road between Helena Street and May Street.
7. Parking removal along south side of Balmain Road between May Street and The Boulevarde.
8. Partial parking removal on both sides of Balmain Road between The Boulevarde and Grove Street.
9. Partial parking removal along west side of The Boulevarde between Joseph Street and Balmain Road.
10. Parking removal along east side of The Boulevarde between Balmain Road and the laneway south of Balmain Road.
11. Parking removal on both sides of Carrington Street between Balmain Road and No.22 Carrington Street.

Pedestrian management

Pedestrian access will be provided with the assistance of licensed traffic controllers.

Impact on public transport and other modes of transport

Consultation with residents within the works zones will be undertaken to ensure plans, times and level of impact is clearly articulated to affected residents.

Initially will undertake a door knock for the more severely affected residents and neighbours to allow questions and introduce a point of contact for the works. This is normally undertaken once site has been established.

Modscape typically will undertake a minimum of 2x letterbox drops with in a drop zone determined by council. These being 7x day prior to road closures and another the day prior to road closure.

Work schedule

The above-mentioned works are confirmed to be conducted over Tuesday through to Wednesday. The running sheet for the work is as follows:

Tuesday 7:00AM

- Traffic Management commence on-site to notify residents of the impending works and restricted parking as per Parking Prevention Plan 'TLTGS219453'

Tuesday 11:00AM to 12:00PM

- Traffic Management commence set-up of the impending works as per Crane Works 'TLTGS219451'

Tuesday 11:30AM

- Traffic control to close off access to Carrington Street in anticipation of crane arrival.

- Tuesday 12:00PM to 6:00PM 250T Crane mobilization / arrival to site and commences set-up.
- 20T Franna Crane to be used to lay out crane pads.
- Approx. 4x counterweight trucks to be unloaded in Carrington Street and leave the area.
- 250T crane to be left on site overnight.
- Crane crew to leave site once crane is set-up.
- Counterweight trucks to arrive in scheduled intervals of approx. 45mins.
- Communication to be in place to ensure the area doesn't become congested with unscheduled arrivals.
- Sufficient room within Carrington Street to cater for up to 4x counterweight trucks if there are any delays with crane set-up.
- No counterweight trucks are to be left within the local area affected by these works.

Tuesday 6:00PM to 9:30PM

- Traffic Management enforcing Parking Prevention Plan 'TLTGS219453'
- Traffic Management enforcing works as per Crane Works 'TLTGS219451'

Wednesday 12:00AM to 3:30AM

- Traffic Management enforcing Parking Prevention Plan 'TLTGS219453'
- Traffic Management enforcing works as per Crane Works 'TLTGS219451'
- Trucks to commence arriving to site in spaced intervals under escort in accordance with NHVR permit approvals.
- Trucks to reverse into The Boulevard per Sweep Path 03
- Total of 5x oversize loads with associated pilot / escorts
- Trucks to turn off amber beacon lights once entered into closed road The Boulevard and park up per Sweep Path 04
- Truck drivers and pilot vehicles to park up and remain vehicles until 7:00am

Wednesday 3:00AM to 7:00AM

- Traffic Management enforcing Parking Prevention Plan 'TLTGS219453'
- Traffic Management enforcing works as per Crane Works 'TLTGS219451'

Wednesday 6:30AM to 7:15AM

- Worker's arrival, unload tools & equipment and morning prestart meeting

Wednesday 7:00AM to Approx. 8:00AM

- Commence module de-hoarding in The Boulevard
- Once first module has been de-hoarded, truck to move from The Boulevard and reverse into Carrington Street under traffic management guidance per Sweep Path 06
- Refer to attached truck movements and sequencing on TMP-01 to TMP-16
- The process is repeated until all 5 trucks have been de-hoarded.
- Traffic Management enforcing Parking Prevention Plan 'TLTGS219453'
- Traffic Management enforcing works as per Crane Works 'TLTGS219451'

Wednesday Approx. 8:00AM to approx. 3:00PM

- Commence module installation.
- Anticipated 1hr required per module install.
- Truck 1, once unloaded to close trailer and leave Carrington Street in forwards direction per Sweep Path 07
- The process is repeated until all 5 trucks have been unloaded and installed into position.
- Traffic Management enforcing Parking Prevention Plan 'TLTGS219453'
- Traffic Management enforcing works as per Crane Works 'TLTGS219451'

Wednesday approx. 3:00PM to approx. 7:30PM

- 250T Crane pack down and de-mobilization / removal from site
- Approx. 4x counterweight trucks to be loaded in Carrington Street and leave the area.
- Counterweight trucks to arrive in scheduled intervals of approx. 45mins.
- Communication to be in place to ensure the area doesn't become congested with unscheduled arrivals.
- 20T Franna Crane to be used to pack-up crane pads.
- Sufficient room within Carrington Street to cater for up to 4x counterweight trucks if there are any delays with crane pack down.
- Traffic Management enforcing Parking Prevention Plan 'TLTGS219453'
- Traffic Management enforcing works as per Crane Works 'TLTGS219451'

Wednesday 3:00PM to 5:00PM

- Clean and tidy all street work areas.
- Construction crews to leave site.

Wednesday 7:30PM to 9:30PM

- Traffic control to pack up and demobilize from site.

COMMUNITY CONSULTATION

Consultation with residents within the works zones will be undertaken to ensure plans, times and level of impact is clearly articulated to affected residents.

Initially will undertake a door knock for the more severely affected residents and neighbors to allow questions and introduce a point of contact for the works. This is normally undertaken once site has been established.

Modscape typically will undertake a minimum of 2x letterbox drops within a drop zone determined by council. These being seven days prior to road closures and another the day prior to road closure.

It is requested that a complaints/feedback register be created for post event review.

Notice of the temporary full road closure has been advertised in accordance with the Roads Act.

FINANCIAL IMPLICATIONS

There are no financial implications associated with the implementation of the proposed recommendations outlined in the report.

ATTACHMENTS

1. [Carrington Street, Lilyfield - Traffic Management Plan](#)

**PROPOSED RESIDENTIAL
DEVELOPMENT**

12 CARRINGTON STREET, LILYFIELD NSW

**Construction Vehicle and Pedestrian Plan of
Management**

27th September 2024

MODSCAPE™

112 Global Ave, Essendon Fields VIC 3041
Tel: (03) 9316 6000
Email: brendan@modscape.com.au

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TABLE OF CONTENTS

1.0	INTRODUCTION.....	4
2.0	PROPOSED DEVELOPMENT.....	5
1.	Site, Context and Existing Use.....	5
2.	Proposed Development.....	5
3.	Construction Program.....	6
4.	Construction Process	6
3.0	TRAFFIC CONDITIONS.....	7
1.	Traffic Conditions.....	7
2.	Transport Services.....	7
4.0	PROPOSED CONSTRUCTION TRAFFIC MANAGEMENT PLAN.....	8
1.	Construction Vehicle Route.....	8
2.	Truck Movements.....	9
3.	Construction Hours.....	9
4.	Crane and Materials Handling.....	9
5.	Site Induction.....	9
6.	Traffic Control Plans.....	10
7.	Pedestrian Management.....	10
8.	Hoardings.....	10
9.	Community Consultation.....	10
5.0	CRANAGE WORKS.....	12
1.	Crane Lifting Plan.....	12
2.	Risk Assessment.....	12
APPENDIX A	ARCHITECTURAL PLANS	
APPENDIX B	TRANSPORT SERVICES (NHVR & AUSGRID APPROVAL)	
APPENDIX C	MODULES/TRUCK SIZES	
APPENDIX D	TURNING PATH ASSESSMENT	
APPENDIX E	TRAFFIC CONTROL PLAN	
APPENDIX F	CRANE LIFT STUDY/RISK ASSESSMENT (INC. AUSGRID)	
APPENDIX G	RESIDENT NOTIFICATION	
APPENDIX H	PROPOSED RUNNING SHEET FOR INSTALLATION	

LIST OF ILLUSTRATIONS

FIGURE 1	SITE PLAN
FIGURE 2	VEHICLE ROUTE

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1.0 INTRODUCTION

Modscape and our client are in the final stages of planning for a proposed new dwelling at 12 Carrington Street, Lilyfield NSW. Pre-approval has been received for Complying Development Certificate (CDC) from the certifying authority for this development.

This Construction Vehicle and Pedestrian Plan of Management has been prepared to assist council and or others understand the building process involved in the new dwelling and any impacts that may arise with the installation of a modular dwelling.

The proposed building is to be a pre-fabricated building constructed off-site. The proposed building methodology is a full welded steel structure which comprises 6 modules in total. Each module will be independent when fabricated off-site and transported individually, but upon installation on-site, the dwelling will be welded and bolted together to form one complete diaphragm. The modules are designed, engineered, built and installed using fully qualified contractors and consultants.

Modscape has been in business for over 16 years and have successfully delivered hundreds of new projects across Australia.

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2.0 PROPOSED DEVELOPMENT

2.1 SITE, CONTEXT AND EXISTING USE

The Site (Figure 1) is Lot 12 of DP1063821 which occupies a rectangular shaped block of approximate 271.9m² with a street frontage to 12 Carrington Street, Lilyfield NSW.

The Site is adjoined by residential dwellings, while there are several parks, schools and other types of accommodation in the surrounding area.

The Site is currently occupied with an existing dwelling which is owner-occupied.

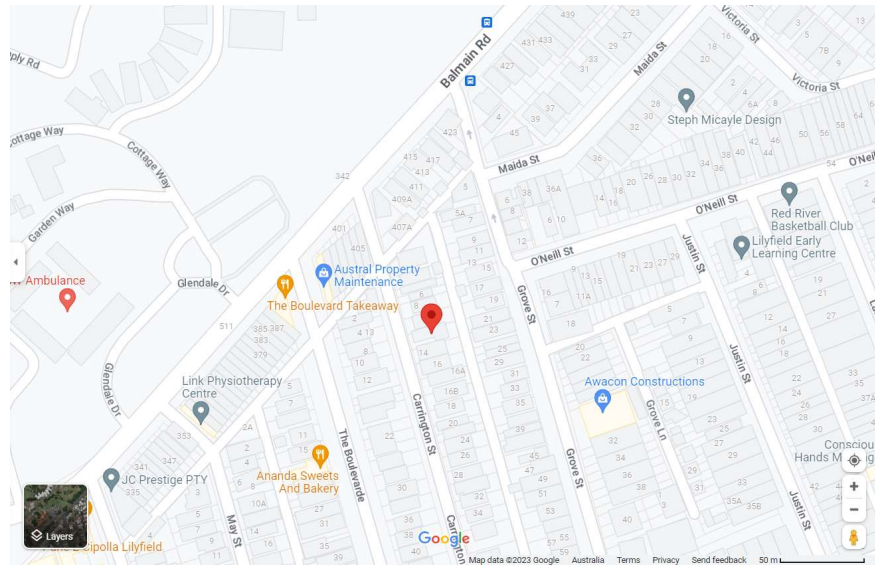


FIGURE 1 SITE PLAN

2.2 PROPOSED DEVELOPMENT

It is proposed to demolish the existing house and construct a new dwelling on the property.

The proposed dwelling consists of;

- 3 Bedrooms with 2.5 bathrooms
- Living, Dining and Kitchen
- Rumpus and Study
- Laundry
- Garage
- External landscaping

A copy of the proposed Architectural Plans are provided in Appendix A.

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Pre-approval has been received for Complying Development Certificate (CDC) from the certifying authority for this development.

2.3 CONSTRUCTION PROGRAM

The proposed timelines for various stages of the works are as follows;

Demolition	2 weeks
Earthworks/In-Ground Services	4 weeks
Construction (off-site fabrication)	12 weeks
Construction (on site completion)	4 weeks
Total on site works	10 weeks
Total program	16 weeks

The total construction program of 16 weeks is considerably less than an average conventional building.

2.4 CONSTRUCTION PROCESS

The proposed development will be a new modular dwelling, which will comprise of on-site works and off-site works.

The works are best separated into 3 stages;

- Stage 1 – Pre-Installation Works
- Stage 2 – Off-Site Manufacturing and Installation Works
- Stage 3 – Post Installation Works

Stage 1 works will consist of Demolition, Earthworks and in-ground services and considered the enabling works prior to the installation of the modules.

Stage 2 works will consist of the off-site manufacture of the dwelling and the process of transportation and installation of the modules. The works are built off-site in a controlled environment in Brooklyn, Victoria. The dwelling is a fully welded structural steel frame which is then manufactured completely off-site. Each module is non-typical in size and site dependant. The installation of the dwelling is achieved by a crane lift on-site undertaken by a suitably qualified crane contractor.

Examples of Modscape residential dwellings can be found on our website www.modscape.com.au

Stage 3 works will consist of the completion of works post the installation of the modules (i.e installation of floor coverings and final commissioning of services)

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3.0 TRAFFIC CONDITIONS

3.1 TRAFFIC CONDITIONS

Observations of traffic activity via the proposed vehicle route on the main arterial as well as the ancillary roads to enter site for the modular installation was completed during typical morning, business and afternoon peak periods.

The observations revealed relatively free flowing conditions except for minor stoppages due to signal operations and any on-street parking/stoppages.

A Traffic Management Plan and VMS Board will be required for the assistance of the installation of the modular buildings. Applications will be submitted to council for approval upon final CDC approval.

On final application in the lead-up to works being undertaken notice will be issued Transport Management Centre (TMC) NSW outlining the proposed works inclusive of dates and times.

3.2 TRANSPORT SERVICES

Our Traffic Management Plan and Traffic Control Plan will take public transport routes into consideration and alternative routes will be provided at all times to maintain transport services.

On final application in the lead-up to works being undertaken notice will be issued to Transport for NSW (TfNSW) outlining the proposed works inclusive of dates and times.

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4.0 PROPOSED CONSTRUCTION TRAFFIC MANAGEMENT PLAN

As described previously, there are 3no. stages throughout the project lifecycle. They are as follows;

- Stage 1 – Pre-installation Works
- Stage 2 – Installation Works
- Stage 3 – Post Installation Works

4.1 CONSTRUCTION VEHICLE ROUTE

All trucks involved during Stage 1 and Stage 3, including but not limited to demolition and earthworks machinery, will access the site normally.

The approach and departure route for the crane and articulated trailer for Stage 2 module delivery and installation will be applied for to the National Heavy Vehicle Regulator (NHVR) and will go through the typical application process.

It is confirmed that Modscape transport subcontractor Nunn Bros Haulage has already been to site to assess the vehicle route. Their proposed entry to Carrington Street is shown in Figure 2.

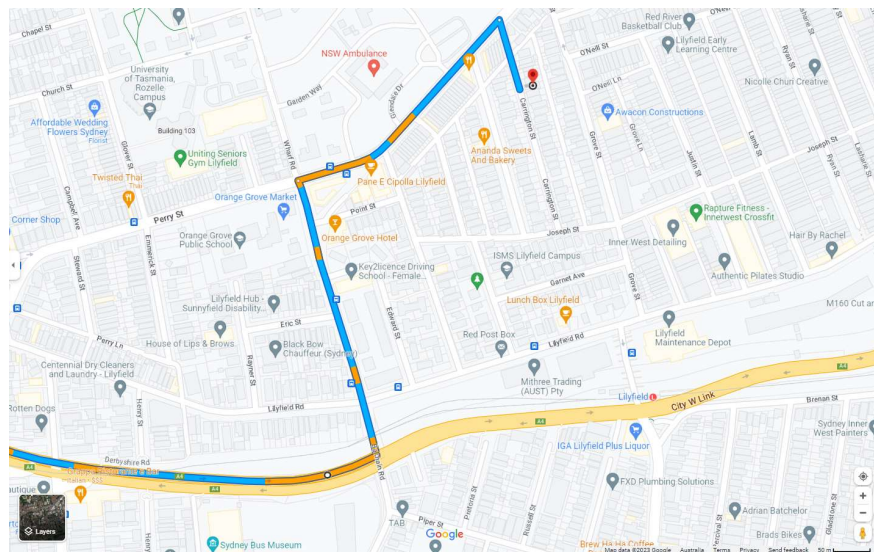


FIGURE 2 VEHICLE ROUTE

It is proposed that the laydown area for the articulated trucks is along The Boulevard which has been selected to minimize disruption to traffic on Balmain Road and its width to enable module de-hoarding.

To enable the installation to occur we confirm that we will require a road closure of Carrington Street and and partial road closure on The Boulevard.

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The confirmed times/dates will be based on the approval given to Modscape by council.

Modscape can confirm that a swept path assessment has been undertaken and was deemed achievable. These details are provided in Appendix D

4.2 VEHICLE MOVEMENTS

The proposed truck and vehicle movements are as follows;

- Stage 1 – Pre-Installation Works
 - Demolition – 2 to 3 per day
 - Earthworks/In-Ground Services – 2 to 3 per day
- Stage 2 – Installation Works
 - Vehicle movement will be confirmed upon application, however the proposed running sheet during the installation works is described in Appendix H
- Stage 3 – Post Installation Works
 - Services Trades – 2 to 3 per day
 - Finishing Trades – 3 to 4 per day

4.3 CONSTRUCTION HOURS

The approved hours of on-site construction during Stage 1 and Stage 3 works will be determined as part of the DA / CDC process.

Typical hours of construction activity are:

Monday to Friday	7:00am – 5:00pm
Saturday	7:00am – 3:00pm
Sunday and public holidays	No works

The approved hours of installation works will be determined upon application to council with approval of road closures and the NVHR permits.

4.4 MATERIALS HANDLING

All deliveries during Stage 1 will loaded/unloaded within the work zone (i.e the site).

There will be minimal to no deliveries once the modules have been installed as most works will have been completed prior to arrival. Any major materials that are required post installation will have already been loaded into the building to enable for completion of works.

4.5 SITE INDUCTION

All workers and visitors on the site will be subject to a formal site induction process and all the inductions will be performed specific to each trades according to Modscape OH&S requirements.

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4.6 TRAFFIC CONTROL PLANS

Traffic Control Plans have been prepared by a suitably qualified Traffic Control contractor and are included in Appendix E for Council review and approval.

4.7 PEDESTRIAN MANAGEMENT

Site Fencing will be installed at all times on the site boundary. Footpaths will be accessible at all times, apart from any deliveries, during Stage 1 and Stage 3 of the works.

Full Traffic Management will be in place during Stage 2 installation works. RMS accredited Traffic Controllers will be on-site at all times, as well as Senior Modscape management.

4.8 HOARDINGS

Site Fencing will be installed at all times. Any other conditions will be adhered to as required.

4.9 COMMUNITY CONSULTATION

Consultation with residents within the works zones will be undertaken to ensure plans, times and level of impact is clearly articulated to affected residents.

Initially will undertake a door knock for the more severely affected residents and neighbours to allow questions and introduce a point of contact for the works. This is normally undertaken once site has been established.

Modscape typically will undertake a minimum of 2x letterbox drops within a dropzone determined by council. These being 7x day prior to road closures and another the day prior to road closure.

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5.0 CRANAGE WORKS

We note that we will require a mobile crane lift for the modular building, which are works pertaining to Stage 2 as noted above.

It is confirmed that Modscape crane subcontractor Melrose cranes has already been to site to assess the access and lift capacities and any restrictions.

5.1 CRANE LIFTING PLAN

A proposed crane lifting plan has been developed yet, however we can confirmed that a lift study has been completed for another project in Scott Street.

This assessment confirmed that a 300T crane would be used.

All relevant permit applications will be made once DA is received.

5.2 RISK ASSESSMENT

A site inspection has been completed and the key item identified was the required for Tiger Tales to certain locations.

These will be identified within the crane lifting plan which forms part of the permit applications.

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APPENDIX C

MODULE/TRUCK SIZES

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12 Carrington Street, Lilyfield NSW

Module and Truck Sizes

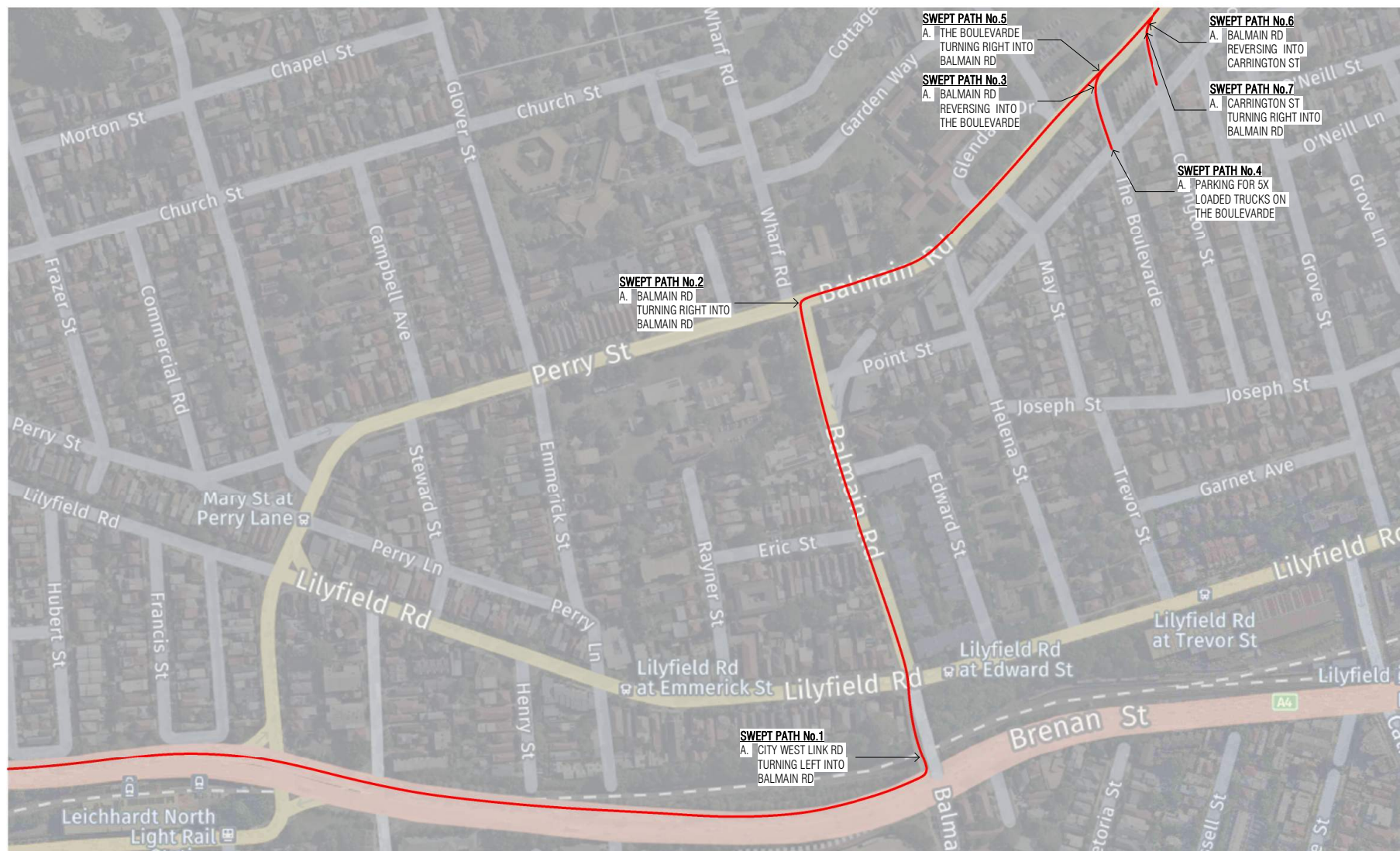
Actual As Built Module Size				
Module	Width (mm)	Length (mm)	Height (mm)	Weight (t)
Module A	4,550	15,480	3,600	16
Module B	3,670	6,500	3,600	6.5
Module C	4,750	13,330	3,600	14
Module D	1,900	9,100	3,550	10
Module E	4,439	15000	3,550	16

Truck size including Module Load					
Truck	Module	Width (mm)	Length (mm)	Height (mm)	Weight (t)
Truck 1	Module A	4,550	23,480	4,640	36
Truck 2	Module C	4,750	21,330	4,640	34
Truck 3	Module B	3,670	19,000	4,640	26.5
Truck 4	Module E	4,439	23,000	4,590	36
Truck 5	Module D	1,900	19,000	4,590	30

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APPENDIX D

TURNING PATH ASSESSMENT



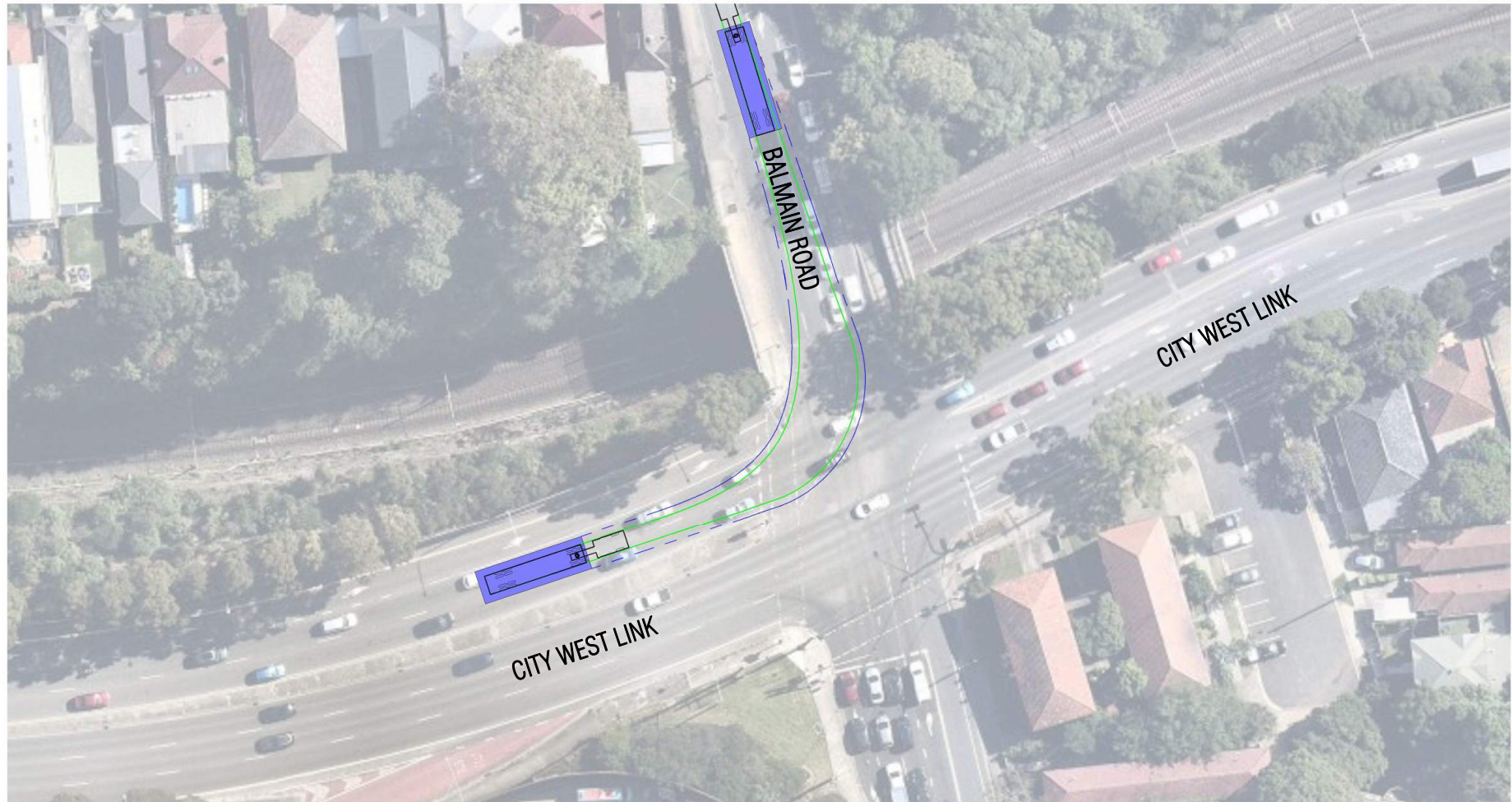
Swept Path Locality Plan

Lilyfield

SCALE: NTS

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City West Link Turning Left into Balmain Rd Swept Path

SCALE 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

Swept Path 01

Lilyfield

SCALE: 1 : 500

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Balmain Rd Turning Right into Balmain Rd Swept Path

SCALE 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

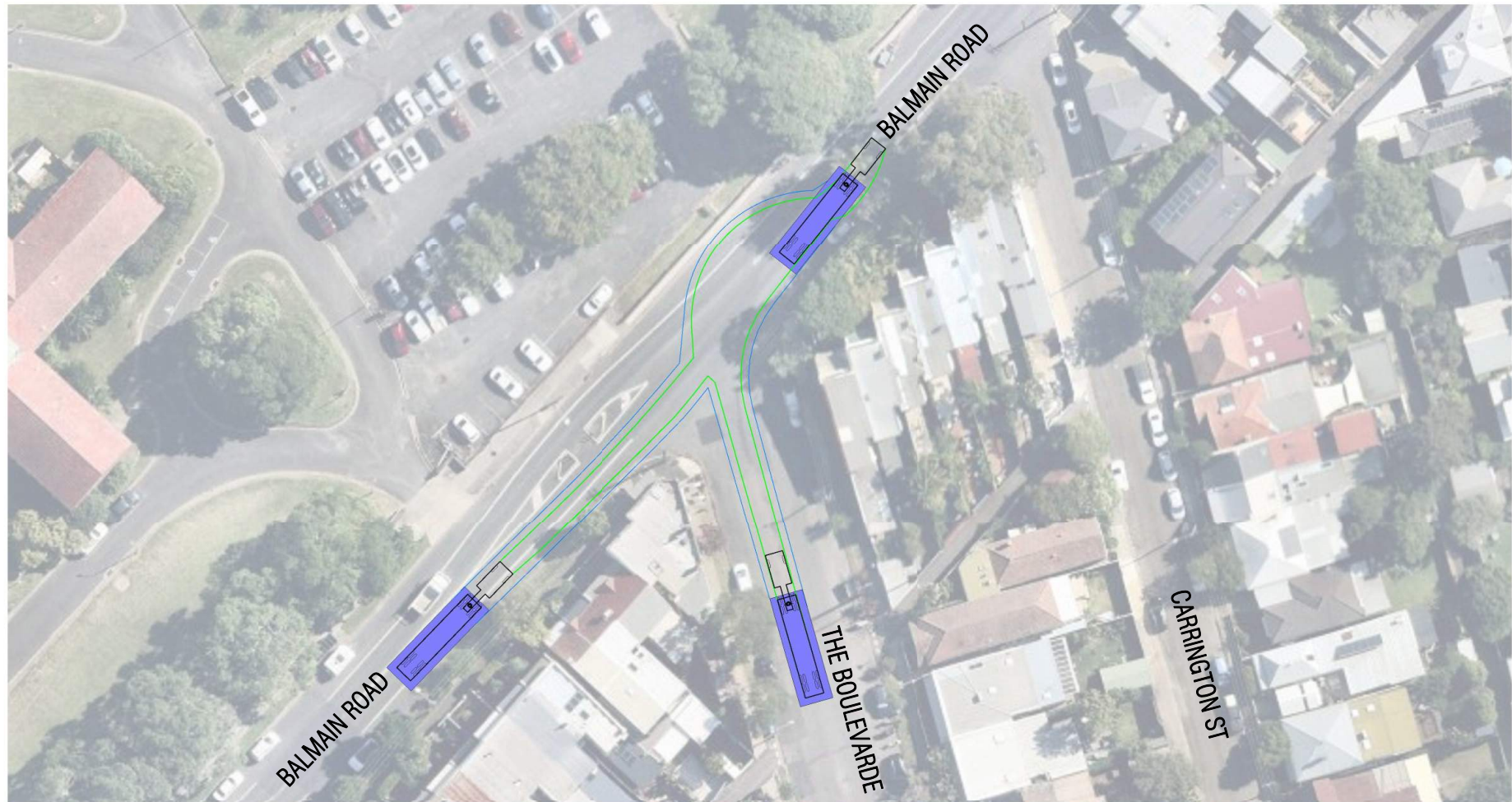
Swept Path 02

Lilyfield

SCALE: 1 : 500

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Balmain Rd Reversing into The Boulevard Swept Path

SCALE 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

Swept Path 03

Lilyfield

SCALE: 1 : 500

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Parking of 5x Loaded Trucks on The Boulevard

SCALE: 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

Swept Path 04

Lilyfield

SCALE: 1 : 500

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The Boulevard Turning Right into Balmain Rd Swept Path

SCALE 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

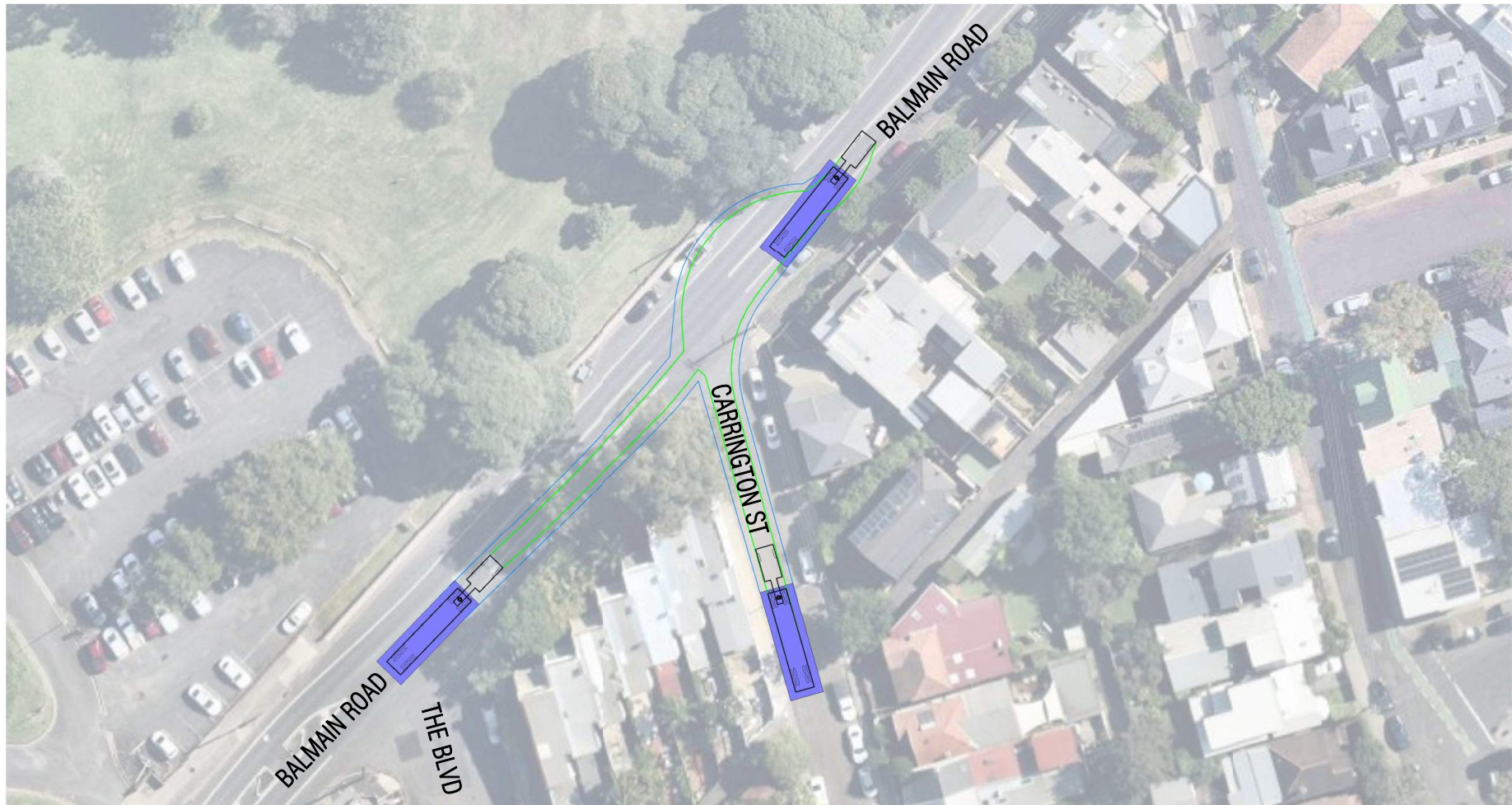
Swept Path 05

Lilyfield

SCALE: 1 : 500

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MODSCAPE.



Balmain Rd Reversing into Carrington St Swept Path

SCALE: 1 : 500

Swept Path 06

Lilyfield

SCALE: 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

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Carrington St Turning Right into Balmain Rd Swept Path

SCALE 1 : 500

Swept Path Legend

- MODULAR BUILDING
- MODULAR ENVELOPE
- WHEEL BASE ENVELOPE

Swept Path 07

Lilyfield

SCALE: 1 : 500

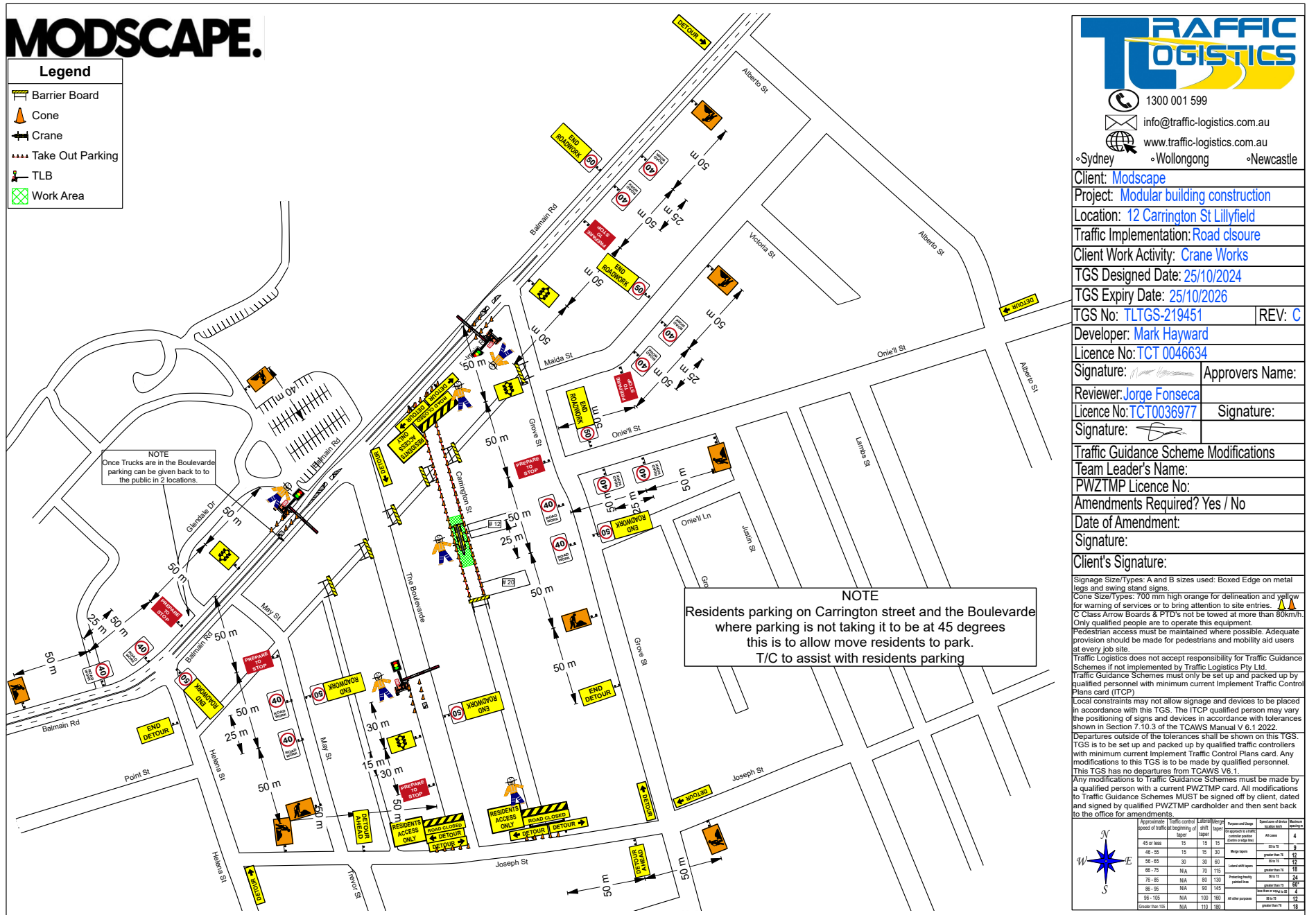
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APPENDIX E

TRAFFIC CONTROL PLAN





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APPENDIX F

CRANE LIFT STUDY/RISK ASSESSMENT

Includes Ausgrid's Tiger Tail Plan & Safety Advice Response Form



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APPENDIX G

RESIDENT NOTIFICATION

MODSCAPE.

430 FRANCIS STREET BROOKLYN VIC 3012
187-197 MILITARY ROAD NEUTRAL BAY NSW 2089
03 9316 6000
MODSCAPE.COM.AU

[insert date] – issue once site works commence.
COMPLETE RESIDENT NOTIFICATION REGISTER

To the Resident

RE: Building Works to 12 Carrington Street, Lilyfield

Dear Resident,

Modscape is a modular home builder who specialize in designing and building beautiful and functional modular homes. Modular building is a modern building process, where homes are built off-site (in our case, a factory in Melbourne) then transported and lifted into their permanent location. This makes for a faster on-site build process and far less disruption on-site when compared to a standard house build. This process is well-established in Australia, and we have been in business for over 16 years and have installed hundreds of modular buildings across Australia.

In the coming weeks Modscape will be delivering & installing a new modular home at 12 Carrington Street, Lilyfield.

During this period, we will be setting up a crane and there will be semi-trailer trucks entering and exiting or passing through your street. The works take place under the supervision of experienced traffic management professionals, with representatives from Modscape on-site at all times.

The attached Parking Plan shows the proposed impact on your area during the installation period. We are kindly requesting that vehicles are not parked on the road during this time to enable our works to proceed. As you live in the parking prevention area, you may experience minor delays entering or exiting your property during the times noted on the attached plan.

Modscape's normal process would be to doorknock the local area, however we understand that under the current circumstances this may not be appropriate. If you feel more comfortable with a phone or email discussion please text 0401 118 096 or email brendan@modscape.com.au with your street number, and I will ensure you are contacted.

If you have any questions on the process, or on modular building more broadly, please contact us on (03) 9316 6000 or through our website, www.modscape.com.au
We appreciate your understanding.

Warm regards,



Brendan Farquhar
Operations Manager
0401 118 096
MODSCAPE.



MODSCAPE.

430 FRANCIS STREET BROOKLYN VIC 3012
187-197 MILITARY ROAD NEUTRAL BAY NSW 2089
03 9316 6000
MODSCAPE.COM.AU

Item 8

ATTACH RELIVANT TCP FOR PARKING PREVENTION PLANS

SAMPLE 1



Attachment 1

MODSCAPE.

12 Carrington St, Lilyfield NSW 2040 – NOTICE OF UPCOMING CHANGE IN TRAFFIC CONDITIONS

We have read, understood and accept the upcoming change in traffic conditions detailed in Modscape's letter provided [\[sample letter 1 or 2\]](#)

PROPERTY ADDRESS	RESIDENT NAME	SIGNATURE	DATE	REGO DETAILS	COMMENTS

MODSCAPE.

430 FRANCIS STREET BROOKLYN VIC 3012
187-197 MILITARY ROAD NEUTRAL BAY NSW 2089
03 9316 6000
MODSCAPE.COM.AU

[insert date] – issue minimum 7x days out from install.

To the Resident

RE: Building Works to 12 Carrington Street, Lilyfield

Dear Resident,

As you may be aware, Modscape are the modular home builder who are undertaking the project of constructing a new modular home on Carrington Street, Lilyfield.

We are writing you this letter to inform you that we are 7 days out from the confirmed delivery and installation dates of the home. As a reminder, installation will be taking place from [insert date & time] through until [insert date & time].

During this period, we will be setting up a crane and there will be semi-trailer trucks entering and exiting or passing through your street. The works take place under the supervision of experienced traffic management professionals, with representatives from Modscape on-site at all times.

The attached Parking Plan shows the proposed impact on your area during the installation period. As you live in the area, you may experience minor delays entering or exiting your property during the times noted on the attached plan.

If you'd like more information about this job, please text or email myself on 0401 118 096 or email brendan@modscape.com.au

If you have any questions on the process, or on modular building more broadly, please contact us on (03) 9316 6000 or through our website, www.modscape.com.au

We appreciate your understanding.

Warm regards,



Brendan Farquhar
Operations Manager
0401 118 096
MODSCAPE.



MODSCAPE.

430 FRANCIS STREET BROOKLYN VIC 3012
187-197 MILITARY ROAD NEUTRAL BAY NSW 2089
03 9316 6000
MODSCAPE.COM.AU

Item 8

ATTACH RELIVANT TCP FOR PARKING PREVENTION PLANS

SAMPLE 2



Attachment 1

MODSCAPE.

430 FRANCIS STREET BROOKLYN VIC 3012
187-197 MILITARY ROAD NEUTRAL BAY NSW 2089
03 9316 6000
MODSCAPE.COM.AU

[insert date] – issue 1x day prior to install works commencing.

To the Resident

RE: Building Works to 12 Carrington Street, Lilyfield

Dear Resident,

As you may be aware, Modscape are the modular home builder who are undertaking the project of constructing a new modular home on Scott Street, Bronte.

We are writing you this letter to confirm our delivery and installation dates of the home. As a reminder, installation will be taking place from [insert date & time] through until [insert date & time].

During this period, we will be setting up a crane and there will be semi-trailer trucks entering and exiting or passing through your street. The works take place under the supervision of experienced traffic management professionals, with representatives from Modscape on-site at all times.

The attached Parking Plan shows the proposed impact on your area during the installation period. As you live in the area, you may experience minor delays entering or exiting your property during the times noted on the attached plan.

If you'd like more information about this job, please text or email myself on 0401 118 096 or email brendan@modscape.com.au

If you have any questions on the process, or on modular building more broadly, please contact us on (03) 9316 6000 or through our website, www.modscape.com.au

We appreciate your understanding.

Warm regards,



Brendan Farquhar
Operations Manager
0401 118 096
MODSCAPE.



MODSCAPE.

430 FRANCIS STREET BROOKLYN VIC 3012
187-197 MILITARY ROAD NEUTRAL BAY NSW 2089
03 9316 6000
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Item 8

ATTACH RELIVANT TCP FOR PARKING PREVENTION PLANS

SAMPLE 3



Attachment 1

Item No: LTC1124(1) Item 9

Subject: BALMAIN ROAD, LILYFIELD - ORANGE GROVE RAISED PEDESTRIAN CROSSING (BALUDARRI-BALMAIN WARD/BALMAIN ELECTORATE/LEICHHARDT PAC)

Prepared By: Jackie Ng - Graduate Traffic Engineer

Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

1. That the proposed Raised Pedestrian Crossing at Balmain Road between Point Street and Edward Street, Lilyfield including the closure of the slip lane at the intersection of Point Street and Balmain Road from Point Street to the crossing, and introduction of angle parking on the east side of Balmain Road be supported in principle and included for consideration in Council's Capital Works Program and that Grant funding opportunities be sought for delivery of the crossing.
2. That the detailed design for the proposed works be brought back to the Traffic Committee for consideration.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

This report outlines the proposed public domain works to support the upgrade of Orange Grove Public School by NSW Department of Education – School Infrastructure. With the school upgrade, the Department has requested a pedestrian crossing at Balmain Road between Point Street and Edward Street, to cover walking catchment areas predominantly to the east of the school and to improve road safety for school children and pedestrians. The proposal also includes closure of the slip lane in front of Orange Grove Hotel, public domain improvements, and new angle parking arrangement.

BACKGROUND

The NSW Department of Education – School Infrastructure has proposed an upgrade to Orange Grove Public School, to meet the enrolment needs for the community. To support the school upgrade and improve safety for children and pedestrians, the following public domain works are proposed:

- A raised pedestrian crossing across Balmain Road between Point Street and Edward Street, Lilyfield.
- Closure of the slip lane outside the Orange Grove Hotel, public domain improvements, and angled parking arrangements.

The proposed raised pedestrian crossing location has been carefully selected to minimise the loss of on-street parking, improve accessibility to the school's entrance and in consideration of its distance and queuing with existing traffic signals. Additionally, the public domain improvements will also provide opportunities for landscaping and outdoor dining.

DISCUSSION

A Transport Working Group comprising of representatives and stakeholders for the Orange Grove Public School upgrade developed a case to improve accessibility and road safety in Balmain Road. The crossing was also required to cover the walking catchments for the Orange Grove Public School, predominantly the areas to the east for school children and their carers. Several concept designs were developed and considered, taking into consideration many factors such as loss of parking, Bus operations, pedestrian walking paths and queuing from the existing traffic signals at the Perry Street/Balmain Road intersection, loading requirements for the Orange Grove Hotel, as well as queuing from Lilyfield Road/Balmain Road intersection. The latest concept is attached for consideration and attempts to balance stakeholder concerns.

The existing school gate at Balmain Road located approximately 30m south of Point Street is expected to remain unchanged when the upgrade works are completed at the School. An assessment undertaken by consultants on behalf of NSW Depart of Education – School Infrastructure indicate that 38% of the School's catchment is expected to use the proposed crossing mainly to the residential areas to the east of the school between Balmain Road and Lilyfield Road. This would indicate that the forecast pedestrian movements at the crossing will meet the minimum pedestrian volumes outlined in Council's Crossing warrants.

Council's current revised pedestrian crossing warrants comprise of:

In each of two separate one-hour periods in a typical day, the pedestrian flow per hour crossing the road is or is expected to be equal to or greater than 20, and vehicle volumes be equal to or greater than 200 where children and elderly or mobility impaired pedestrians count as two pedestrians.

Previous traffic count in Balmain Road near the proposed location indicate that Balmain Road carries in the order of 6,000 vehicles/day, this equates to approximately 600 vehicles in the peak hours, which would meet the vehicle warrants.

An examination of the crash database provided from Transport for NSW, indicate one reported crash in Balmain Road between Point Street and Lilyfield Road for the 5 year period ending in 2022, Rum code 73- Light truck westbound at intersection, off-road right into object at Point Street, two injuries.

To support the school upgrade and improve safety for children and pedestrians, the following public domain works are proposed:

- A raised pedestrian crossing across Balmain Road between Point Street and Edward Street, Lilyfield.
- Closure of the slip lane outside the Orange Grove Hotel, public domain improvements, and angled parking arrangements.

A letter outlining the proposal was distributed to 105 properties and a total of six (6) responses were received.

Feedback and comments have been summarized in the table below.

Resident Comments	Council Comments
<p>Loss of Parking</p> <p>The proposed location will wipe out many parking spaces as well as the long-standing parent drop-off and pick-up location in front of the Childcare Centre.</p>	<p>The current design aims to prioritize pedestrian safety while minimizing the impact the project will have on the surrounding public domain. The proposal will relocate the same number of parent short-term parking for the childcare centre, and minimise the loss of parking capacity by</p>

	<p>providing angle parking at the closure of the Point Street slip lane.</p> <p>The raised pedestrian crossing will also provide opportunities for parents and pedestrians to park in nearby streets and safely cross Balmain Road through the new crossing.</p>
<p>Location of the Crossing</p> <p>There is an existing signalized crossing at the Balmain Road and Perry Street intersection that appears adequate and much safer.</p> <p>The crossing will impact driveways in the surrounding area. We suggest moving the crossing further north or south.</p>	<p>The location of the raised pedestrian crossing was chosen as it was the optimum location, taking into consideration of issues such as loss of parking, Bus operations, pedestrian walking paths and queuing from the existing traffic signals at the Perry Street/Balmain Road intersection, loading requirements for the Orange Grove Hotel, as well as queuing from Lilyfield Road/Balmain Road intersection.</p> <p>Balmain Road currently only has two crossing points that allow pedestrians to cross safely in a stretch of approximately 300 metres (at Perry Street and Lilyfield Road). The crossing is required to provide a suitable crossing for school students and their carers, in particular the school catchment areas to the east of Orange Grove School.</p>
<p>Traffic Congestion and Safety</p> <p>The proposed changes will create considerable disruption in the area due to the angled parking and the closure of the slip lane increasing risk of injury to pedestrians and damage to vehicles.</p> <p>The additional extension of the bicycle track will reduce the lanes available for vehicular traffic again adding to the congestion.</p>	<p>See comment about loss of parking for angled parking concerns.</p> <p>The closure of the slip lane will reduce the number of conflict points at the intersection and reduce vehicular speeds. A reduction in conflict points will reduce the possibility of collisions and will also provide opportunity for outdoor dining, street beautification, landscaping, improving walkability.</p> <p>Balmain Road predominantly operates as a single lane in each direction between Point Street and Lilyfield Road. The current proposal is not expected to change the existing road configuration, although the crossing will be expected to help lower vehicle speeds in a sensitive land use area.</p>
<p>Delivery Vehicles</p> <p>The inability of the large delivery vehicles being able to manoeuvre properly to the loading areas for the Orange Grove Hotel will cause additional congestion.</p> <p>The pedestrian fence will most likely be a problem for larger rear loading delivery trucks that point into and across the centre</p>	<p>The revised concept does not propose a pedestrian fence along the central median. Additionally an analysis on vehicle swept path has been conducted and it is found that the proposal will provide adequate space to allow for a medium rigid vehicle movement into the Orange Grove Hotel driveway without impacting the existing central median or the proposed kerb islands.</p>

of the road to reverse into the hotel driveway.	
Parking Restrictions It is requested that the new angle parking spaces be either untimed or reduced to 2P 8am-4pm Mon-Fri.	Council will consider changes to the restrictions during the development of the detailed design. If necessary, additional consultation will be undertaken for the parking changes.

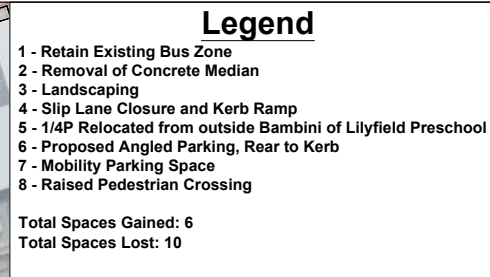
FINANCIAL IMPLICATIONS

Initial estimate of the proposed raised pedestrian crossing, including public domain works to be approximately \$400,000. It is expected that this project would be eligible for funding streams from the NSW Government.

Works are to be considered as part of Council's Capital Works program for Traffic Facilities with final costing to be finalized following completion of detailed design.

ATTACHMENTS

1. [Proposed Orange Grove Raised Pedestrian Crossing - Concept Plan](#)



Item No: LTC1124(1) Item 10
Subject: CURTIS ROAD, BALMAIN - PROPOSED PEDESTRIAN IMPROVEMENT WORKS
CURTIS ROAD, BALMAIN - PROPOSED PEDESTRIAN IMPROVEMENT WORKS (BALUDARRI-BALMAIN WARD/BALMAIN ELECTORATE/LEICHHARDT PAC)
Prepared By: Zara Helal - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the following treatments be approved in principle for inclusion in Council's Traffic Facility Capital Works Program and that a detailed design including consultation be brought back to the Committee for consideration:

- a) Raised pedestrian crossing on Curtis Road, north of Darling Street as shown in *Attachment 1*.
- b) Raised pedestrian crossing on Curtis Road between McDonald Street and Thames Street, continuous footpath treatment on Thames Street at Curtis Road, and conversion of McDonald Street from Curtis Road to Hoffmans Lane to one-way northbound (subject to approval of a TMP by TfNSW) as shown in *Attachment 2*.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Council has received requests for consideration of pedestrian crossing points at Curtis Road, Balmain between Thames Street and Darling Street, to give pedestrians priority when crossing the road. To improve pedestrian safety and deter speeding in this area, it is proposed to install the following treatments:

1. Raised pedestrian crossing on Curtis Road, north of Darling Street as shown in *Attachment 1*
2. Raised pedestrian crossing on Curtis Road between McDonald Street and Thames Street, as per *Attachment 2*
3. Continuous footpath treatment on Thames Street at Curtis Road, as per *Attachment 2*.

Furthermore, it is proposed that McDonald Street from Curtis Road to Hoffmans Lane, be converted to one-way northbound as shown in *Attachment 2* which is required to accommodate the pedestrian crossing. A tree will need to be removed in front of No. 31 Curtis Road as detailed on the plan to accommodate the crossing.

BACKGROUND

Table 1 outlines the features of Curtis Road between Thames Street and Darling Street.

Street Name	Curtis Road – between Thames Street and Darling Street
Kerb to Kerb Width	9.7m approx.
Carriageway Type	Two-way, one travel lane each direction, mixed traffic with parking lanes on both sides of the road.
Classification	Local
Speed Limit	40 km/h
Available TfNSW recorded crash history last 5 years (2019-2023)	One (1) crash occurred between 'unknown' type vehicle turning left from McDonald Street to Curtis Road and pedal cycle travelling east on Curtis Road. RUM code: 16 Injury type: Serious injury Date: 16/02/2021
Parking Arrangements	Parking permitted on both sides

DISCUSSION

Road Network

The section of Curtis Road between Thames Street and Darling Street serves as a pedestrian connection between the Montrose Aged Care Centre and Balmain Care for Kids Preschool on Thames Street, to key land use attractors such as the Balmain shopping strip and Gladstone Park located west of the Curtis Road / Darling Street intersection. There are other pedestrian movements associated with local schools and residents walking to bus stops in Darling Street. As such, there is a high number of pedestrians, namely children and elderly pedestrians, that cross Curtis Road to travel to these locations. Darling Street and Curtis Road intersect to form a three-leg intersection, and traffic movements are regulated by an existing roundabout.

Council completed pedestrian and vehicle counts to determine if the volumes satisfy the warrants for a pedestrian crossing as required in Council's Pedestrian Crossing Warrant Policy. Both the vehicle and pedestrian volumes exceed the minimum required volumes, at both proposed crossing locations with more than 20 pedestrian movements and 200 vehicular movements.

AM peak traffic counts in the form of tube counts and intersection counts at Curtis Road between Thames Street and Darling Street are shown in Figure 1.

PM peak traffic counts in the form of tube counts and intersection counts at Curtis Road between Thames Street and Darling Street are shown in Figure 2.

Figure 1: Tube count and intersection count traffic data – AM Peak 8:00AM-9:00AM

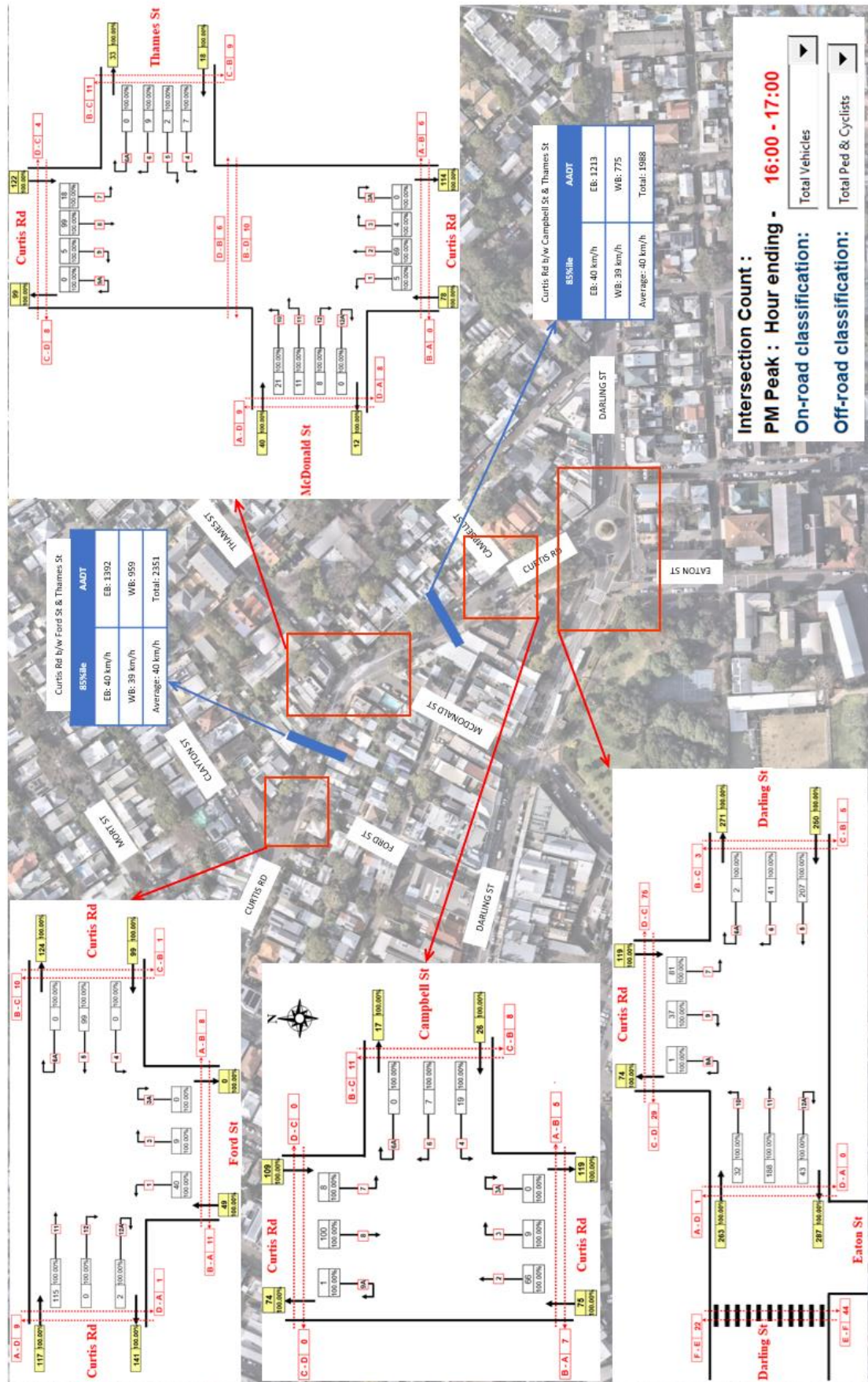


Figure 2: Tube count and intersection count traffic data – PM Peak 4:00PM-5:00PM

Council assessed the approach sight distance (ASD) with respect to Austroads Guide to Road Design Part 4A: Unsignalised and signalised intersections to assess the feasibility and safety of installing a pedestrian crossing at Curtis Road between McDonald Street and Thames Street, as there is an identified sight constraint for eastbound movements in Curtis Road due to the curvature and slope of the road.

The ASD for the proposed raised pedestrian crossing at Curtis Road between McDonald Street and Thames Street is shown in Table 2.

Street	85 th %ile speed (km/h)	ASD (required) (metres)	ASD (existing) (metres)	Acceptable (Yes/No)
Curtis Road (travelling eastbound from west of crossing)	40	34	43	Yes

The following assumptions were made in determining the ASD:

- A reaction time of 1.5 seconds (Absolute minimum value. Only used in very constrained situations where drivers will be alert)
- A coefficient of deceleration value of 0.36 (Desirable value for calculating minimum stopping sight distance for most urban and rural road types, and level crossings)
- A positive longitudinal uphill grade of 5% for Curtis Road when travelling eastbound



Based on the ASD assessment, the existing sight distance from the location the proposed pedestrian crossing is acceptable.

PUBLIC CONSULTATION

Initial community consultation for the Birchgrove / Balmain East Local Area Traffic Management (LATM) study was undertaken from Monday 27 May to Sunday 23 June 2024. This study area includes Curtis Road. A draft plan for the entire LATM area is currently being prepared.

Further to this consultation, an on-site meeting was held on Thursday 11 July 2024 to gather additional specific feedback relating to improving safety along Curtis Road between Campbell Street and Ford Street as raised by some members of the public. 14 residents attended the on-site meeting.

Feedback and comments from the on-site meeting have been summarized in the table below.

Resident Comments	Officer Comments
<p>Curtis Road at intersections with Thames Street & McDonald Street</p> <ul style="list-style-type: none"> - Sightline issues from both streets upon entering Curtis Road. - Both Thames & McDonald Streets have narrow road width. 	<p>Noted. A continuous footpath treatment is proposed at Thames Street at its intersection with Curtis Road. This treatment will require road users to give way to pedestrians, and lower speeds when approaching the intersection.</p> <p>Additionally, McDonald Street is proposed to be converted to one-way northbound between Hoffmans Lane and Curtis Road, which will reduce conflict points at the intersection.</p> <p>The proposed raised pedestrian crossing at Curtis Road near its intersection with McDonald Street and Thames Street will provide pedestrians priority when crossing the road.</p>
<p>Curtis Road is used as a rat run to avoid Darling Street.</p>	<p>Noted. The two (2) proposed raised pedestrian crossings will provide traffic calming and deter speeding behaviours along Curtis Road.</p>
<p>Speed issues mostly occur on the top slope northbound of Curtis Road between Darling Street and McDonald Street.</p> <ul style="list-style-type: none"> - VMS speed sign is facing the wrong direction to capture vehicle speed on this section of Curtis Road. - Most vehicles exceeding the 40km/h speed limit are high-performance SUV vehicles. - Excessive speeds occur mainly during am/pm peak periods and evening hours. 	<p>The proposed raised pedestrian crossing at Curtis Road near its intersection with McDonald Street and Thames Street will provide traffic calming and deter speeding behaviours at this location.</p>
<p>Residents suggest traffic calming is required on Curtis Road to slow speed.</p>	<p>Noted. Traffic calming in the form of two (2) raised pedestrian crossings at Curtis Road are proposed.</p>
<p>Drivers circulate from Curtis Road into surrounding streets to locate parking when visiting nearby cafes often performing dangerous U-turns at the McDonald Street</p>	<p>McDonald Street is proposed to be converted to one-way northbound between Hoffmans Lane to deter U-turns at its intersection with Curtis Road.</p>

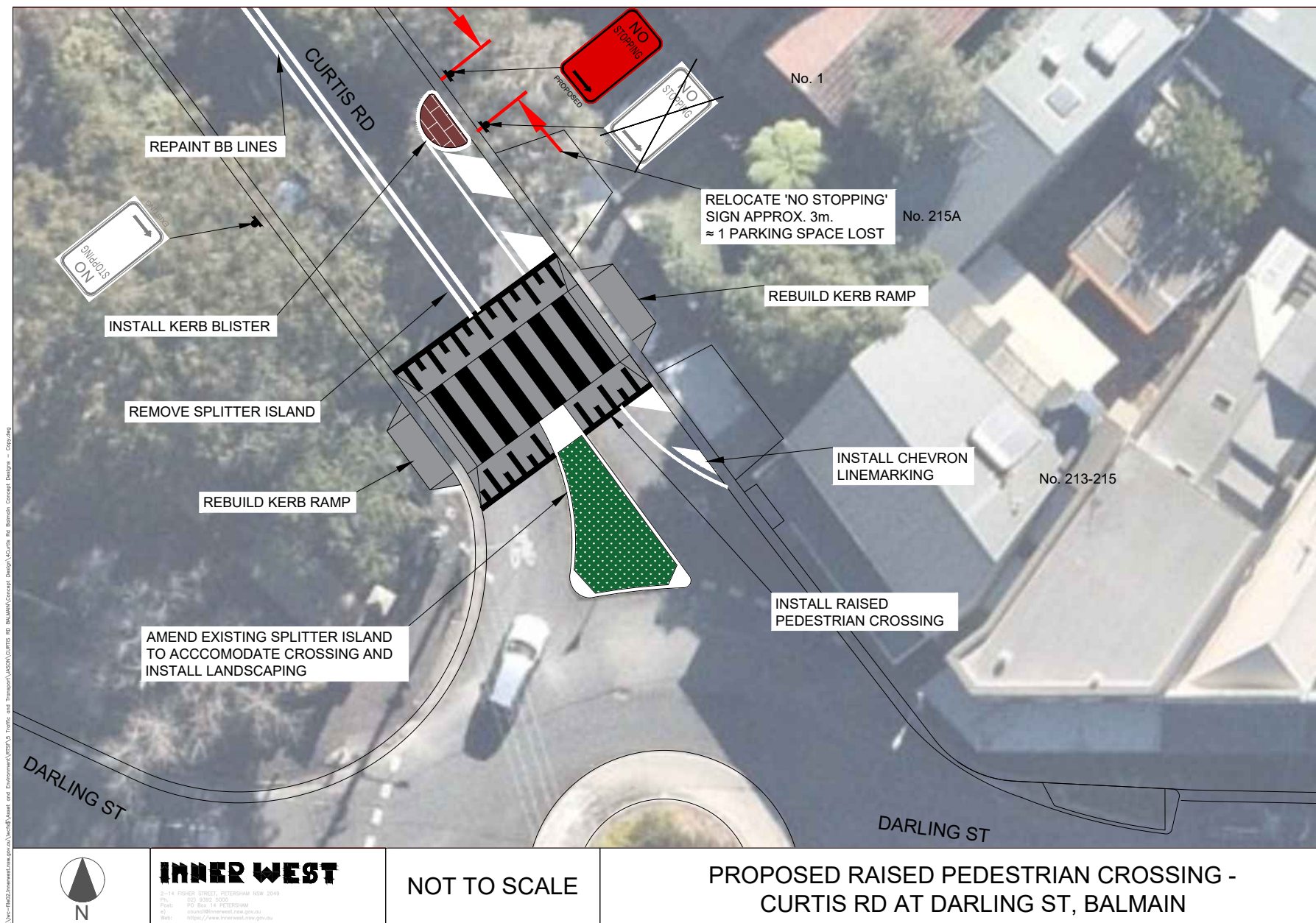
intersection to secure a parking space.	
Hotel/pub located on Curtis Road attracts additional traffic from patrons.	Noted. The two (2) proposed raised pedestrian crossings will provide pedestrians priority when crossing the road.
Drivers entering Curtis Road from Ford Street fail to stop before turning onto Curtis Road.	The proposed treatments will reduce the speed profile in Curtis Road heading westbound.
Residents have concerns regarding noise association with speed humps to provide traffic calming however acknowledge that pedestrian safety is the priority.	Noted. The location of the two (2) proposed raised pedestrian crossings have been selected as such that there is minimal disturbance on neighbouring properties whilst providing priority crossing opportunity for pedestrians at desire lines.
The existing pedestrian refuge at the intersection of McDonald Street/Thames Street is the preferred desire line for pedestrians and the most obvious location for a raised pedestrian (zebra) crossing. However, due to the complex geometry of the intersection along with vehicles parked nearby, visibility and sightlines are impacted. Relocation or loss of parking may be considered to ensure visibility of pedestrians when crossing.	<p>Noted. The proposed pedestrian crossing at Curtis Road near its intersection with McDonald Street and Thames Street follows the existing pedestrian desire line.</p> <p>Council assessed the approach sight distance (ASD) determine the feasibility and safety of installing a pedestrian crossing at Curtis Road between McDonald Street and Thames Street, as there is an identified sight constraint due to the curvature and slope of the road.</p> <p>Parking is proposed to be removed and/or relocated as required by NSW Road Rules for stopping on or near a pedestrian crossing, and to ensure adequate sight distance.</p> <p>Conversion to one way northbound in McDonald Street required to allow sufficient space for pedestrian crossing to be provided conflict points at intersection.</p>
Vehicles that reverse out of driveways at Curtis Road are causing safety issues.	Noted. It is the drivers' responsibility to reverse out when it is safe to do so.

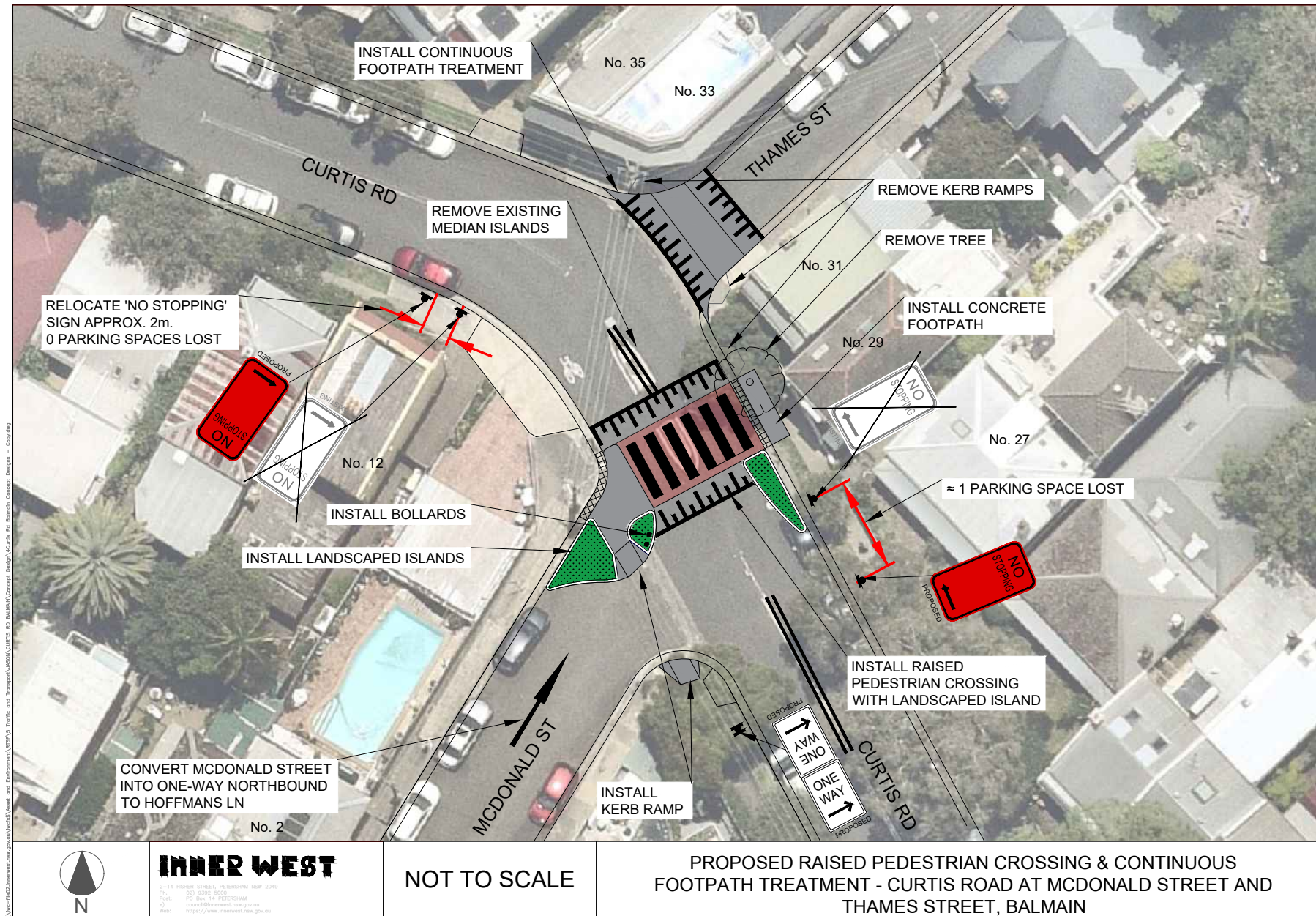
FINANCIAL IMPLICATIONS

Works are to be considered as part of Council's Capital Works program for Traffic Facilities with final costing to be finalised following completion of detailed design.

ATTACHMENTS

1. [Download](#) Curtis Road at Darling Street, Balmain - Concept Design
2. [Download](#) Curtis Road at McDonald Street and Thames Street, Balmain - Concept Design





Item No: LTC1124(1) Item 11

Subject: **EMMERICK STREET, LILYFIELD - PROPOSED NEW KERB BLISTER (BALUDARRI-BALMAIN WARD/BALMAIN ELECTORATE/LEICHHARDT PAC)**

Prepared By: Amir Falamarzi - Traffic Engineer

Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the attached detailed design plan (Design Plan No. 10306) for the proposed kerb blisters in Emmerick Street at the intersection with Perry Street, Lilyfield be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Council is planning to improve pedestrian accessibility and safety in Emmerick Street, Lilyfield at Perry Street by constructing kerb blister islands with integrated kerb ramps at this location. The works aim to improve road safety by better defining the pedestrian crossing points, reducing vehicle speeds and reducing the overall crossing distance for pedestrians crossing Emmerick Street.

BACKGROUND

The detailed design plan shown in *Attachment 1* outlines the proposed work on Emmerick Street, Lilyfield and includes the following:

- Construct two landscaped kerb blister islands with 'gutter bridge' crossings and integrated kerb ramps in Emmerick Street at its intersection with Perry Street.
- Resurface the road pavement with new asphalt as shown in the plan (final extent of any resurfacing will be subject to final funding allocations).
- Remove the existing kerb ramps and reconstruct damaged sections of concrete footpath with new concrete footpath.
- Construct two new stormwater drainage inlet pits in new locations in Emmerick Street and provide space for the new landscaped kerb blister islands, connect to the existing stormwater drainage system.
- Provide new 'Motor Bike' parking spaces between the existing in-road tree and the new landscaped kerb blister island on the west side of Emmerick Street.
- Slightly adjust the existing 'No Stopping' signage, and provide new 'No Stopping' signs.
- Adjust existing regulatory signage as indicated on the plan.
- Install associated new pavement line marking and signage as indicated on the plan.

The works are generally being proposed within the statutory 10m No Stopping zone in Emmerick Street, and therefore the proposal will not result any loss of any legal on-street parking spaces. A small section of 'Motor Bike' parking will be created within the residual

kerbspace created between the kerb blister island and an existing street tree on the west side of Emmerick Street.

PUBLIC CONSULTATION

A letter outlining the proposal was issued to the properties (39 properties) shown in the distribution map below. No response has been received from the consultation.

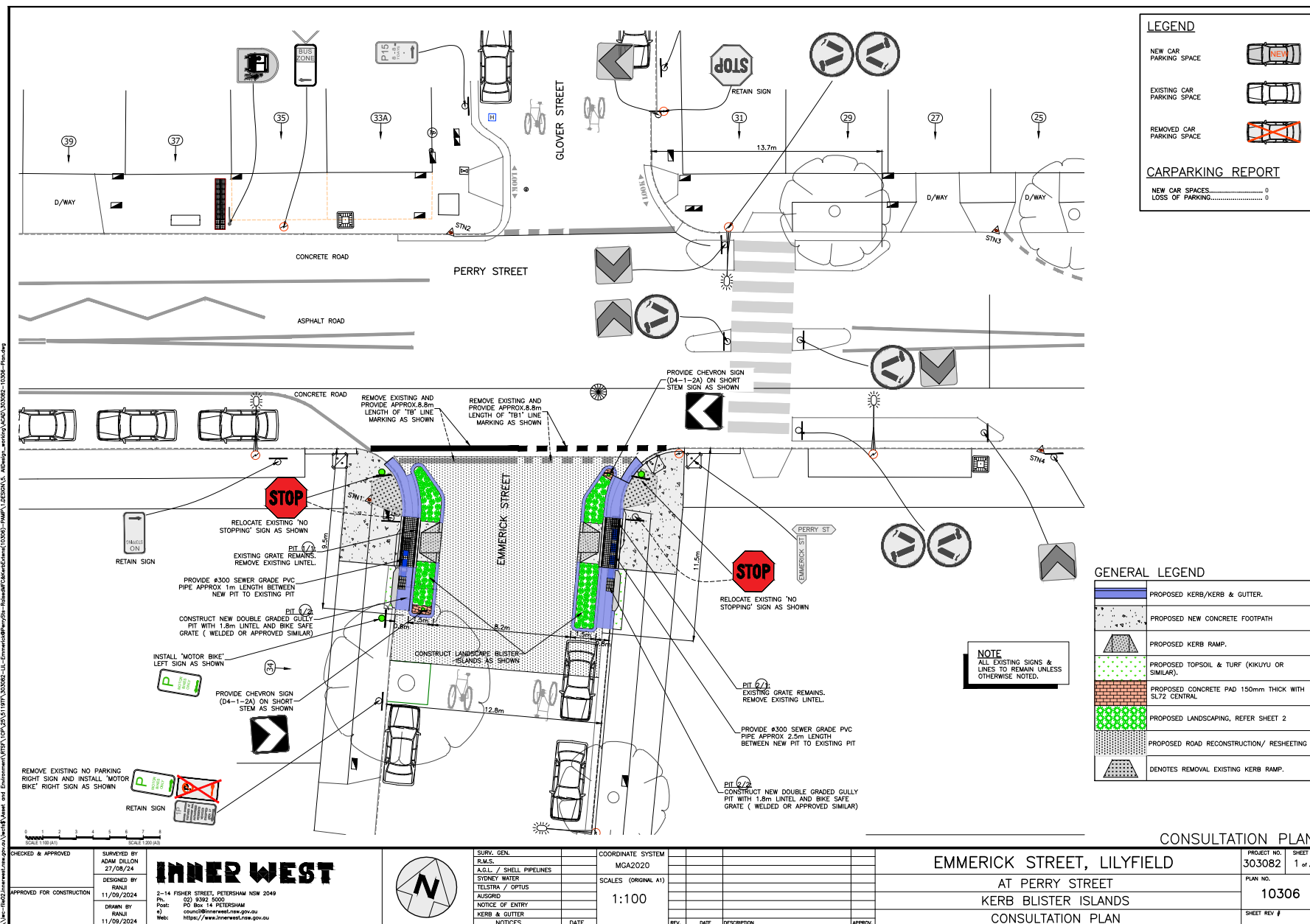


FINANCIAL IMPLICATIONS

Funding of \$106,500 will be allocated from the PAMP programs with construction scheduled to commence in 2024/2025.

ATTACHMENTS

1. [Emmerick Street - Kerb Blister Islands Detailed Design Plan](#)



Item No: LTC1124(1) Item 12
Subject: DARLING STREET BETWEEN MORT STREET AND BOOTH STREET, BALMAIN - ROAD OCCUPANCY - ANZAC DAY DAWN SERVICE (BALUDARRI - BALMAIN WARD/ BALMAIN ELECTORATE/ LEICHHARDT PAC)
Prepared By: Amir Falamarzi - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the temporary road closure of Darling Street (Mort Street to Booth Street), Balmain on Friday, 25 April 2025 between 2.30am – 9.30am be approved, subject to the following conditions be approved:

- a) all affected residents and businesses, including the NSW Police Local Area Commander, Fire & Rescue NSW and NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary road closure at least 14 days in advance of the closure with the applicant making reasonable provision for stakeholders;
- b) that an unencumbered passage minimum 3.0m wide be available for emergency vehicles through the closed section of Darling Street, Balmain; and
- c) the occupation of the road carriageway must not occur until the road has been physically closed.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

In preparation to mark the ANZAC Day Dawn Service 2025 on Friday, 25 April 2025, Inner West Council is organising the ANZAC Day dawn Service at the Loyalty Square War Memorial, Balmain. To facilitate the event, it is proposed to close Darling Street between Mort Street and Booth Street between 2:30am and 9:30am.

BACKGROUND

In preparation to mark the ANZAC Day Dawn Service 2025, Council is organising an event at the Loyalty Square memorial, Balmain.

The details of the event are as follows:

- Day: Friday, 25 April 2025
- Commemorative Service: 6.00am – 7.00am
- Venue: Loyalty Square, Balmain (outside Woolworths)
- Procession: There will be no march down Darling Street for 2025

To facilitate the event, it is proposed to close Darling Street between Mort Street and Booth Street (approximately 200m) between 2.30am and 9.30am. This road closure will prevent all

traffic travelling through this closed section and will prevent any additional on-street parking on both sides of the subject section of Darling Street.

To further support the event area and surrounding businesses and commercial operators, Ford Street and Hoffmans Lane, between Curtis Road and McDonald Street will have local access permitted via a soft closure.

It should be noted that although the land uses in this area are predominantly retail/commercial, there are a number of residential properties in this section of Darling Street.

The Traffic Management Plan prepared for the event is outlined in *Attachment 1*.

DISCUSSION

All businesses/residents in the affected area will be advised to avoid parking in the subject section of Darling Street.

Based on the discussions held with the Transit Systems representative, buses would need to terminate at Darling Street/Rowntree Street/Montague Street intersection as Curtis Road is not suitable for bus movements. To maintain transport services between Balmain East and Balmain, Council will organise two shuttle bus services running between Balmain East and Darling Street/Rowntree Street/Montague Street. The shuttle bus service routes are shown on the attached TGS (*Attachment 2*).

Transit Systems will post notifications at all bus stops on Darling Street between Beattie Street and Balmain East wharf to advise passengers to use Council's community buses on the morning of 25 April 2025. All other eastbound and westbound traffic will need to use Curtis Road or Palmer Street to access Balmain East.

The Transit Systems representative has also requested the temporary expansion of the bus turning area at Grove Street to accommodate additional buses. Therefore, it is proposed to temporarily remove three parking spaces on the northern side even numbered side (No. 22, 24, 26A) of Grove Street near Deloitte Avenue, Birchgrove. Affected residents will be notified.

PUBLIC CONSULTATION

Council's Community Events Coordinator will organise a notification letter outlining the closure of Darling Street (between Mort Street and Booth Street) to all affected properties and the emergency services, including Balmain Hospital of the temporary road closure.

The proposed road closure is currently advertised on Council's website in accordance with the *Roads Act 1993*.

FINANCIAL IMPLICATIONS

There are no financial implications associated with the implementation of the proposed recommendations outlined in the report.

ATTACHMENTS

1. [ANZAC 2025 Traffic Management Plan](#)
2. [ANZAC 2025 Traffic Guidance Scheme](#)



TRAFFIC MANAGEMENT PLAN

ANZAC DAY 2025
DARLING STREET, BALMAIN



Friday 25th April 2025

PREPARED FOR



by CATO Location Services

TABLE OF CONTENTS

TABLE OF CONTENTS	2
STATEMENT OF CONFIDENTIALITY & NON-DISCLOSURE	4
DOCUMENT CONTROL	5
DEFINITIONS	5
REFERENCE DOCUMENTS	6
1. GENERAL EVENT INFORMATION	7
1.1. EVENT SUMMARY	7
1.2. TRAFFIC IMPACT SUMMARY	8
1.3. EVENT DETAILS	8
1.4. KEY EVENT CONTACTS	9
2. EVENT LOCATION	10
2.1. EVENT SITE MAP	10
3. WORKPLACE HEALTH & SAFETY	11
3.1. RISK ASSESSMENT PLANS	11
3.2. PUBLIC LIABILITY INSURANCE	13
3.3. NSW POLICE FORCE	13
3.4. NSW FIRE & RESCUE AND NSW AMBULANCE	13
3.5. EVENT DELAYS, POSTPONEMENT OR CANCELLATION	13
4. TRAFFIC AND TRANSPORT MANAGEMENT	14
4.1. EVENT IMPACT ON ROAD NETWORK	14
4.2. DETOURS	14
4.3. IMPACTS ON PUBLIC TRANSPORT	14
4.5.1. BUSES	14
4.5.3. TAXIS AND RIDE-SHARE PROVIDERS	15
4.6. CHANGES TO CYCLE ROUTES	15
4.7. HOSTILE VEHICLE MITIGATION	15
4.8. RE-OPENING ROADS AFTER THE EVENT	15
4.9. TRAFFIC CONTROL	16
5. MINIMISING IMPACT ON THE NON-EVENT COMMUNITY	17
5.1. ACCESS FOR EMERGENCY VEHICLES	17
5.2. ADVERTISING TRAFFIC MANAGEMENT ARRANGEMENTS	17
5.3. EVENT PROMOTION	17



6 APPROVALS 18

6.1. EVENT ORGANISER APPROVAL 18

6.2. AUTHORISATION TO REGULATE TRAFFIC 18

6.3. PUBLIC LIABILITY INSURANCE 20

7 ATTACHMENTS 21

7.1. TRAFFIC GUIDANCE SCHEMES 21



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

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DOCUMENT CONTROL

This document is uncontrolled once printed – the final version with specifications and site diagrams will be locked for printing and restricted by password.

A copy of the final version will be supplied to the event organiser prior to the event.

Version	Prepared by	Date	Comments	Reviewed by
1.0	Craig Hunter	25/10/2024	First Draft	Anna Rosa

DEFINITIONS

Term	Definition
TMP	Transport Management Plan
TGS	Traffic Guidance Scheme
VMS	Variable Message Sign
HVM	Hostile Vehicle Mitigation
TMC	Transport Management Centre
SMP	Security Management Plan
TfNSW	Transport for NSW



REFERENCE DOCUMENTS

Title	Version
Guide to Traffic and Transport Management for Special Events	v3.5, July 2018
Traffic Control at Worksites Technical Manual	v6.1, February 2022
Workplace Health and Safety ACT NSW	2011
Workplace Health and Safety ACT Amendments NSW	2023
Workplace Health and Safety Regulations NSW	2017
Safe Work NSW website – www.safeworkaustralia.nsw.gov.au	Current website
Working near Sydney Light Rail – www.transdev.com.au/solutions/work-access-permits/	Current website.
Safe Work Code of Practice – First Aid in the workplace	January 2020
Safe Work Code of Practice – Hazardous Manual Tasks	August 2019
Safe Work Code of Practice – Managing the risks of plant in the workplace	December 2022
Safe Work Code of Practice – How to manage work health and safety risks	August 2019
Safe Work Guideline – Traffic Management: Guide for Events	April 2021



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

6

1. GENERAL EVENT INFORMATION

1.1. EVENT SUMMARY

ANZAC Day (25th April) marks the anniversary of the first major military action fought by Australian and New Zealand forces during the First World War.

In preparation to mark ANZAC Day Dawn Service 2025, Inner West Council is organising an event at the Loyalty Square War Memorial, Balmain.



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

7

1.2. TRAFFIC IMPACT SUMMARY

To facilitate the event, it is proposed to close Darling Street between Mort Street and Booth Street, Balmain. This proposal will prevent all traffic travelling through this closed section and will prevent any additional on-street parking on both sides of the subsection section of Darling Street.

To support the event area the following location is to be closed via a “hard road closure”:

- + Darling Street, between Mort Street and Booth Street

To further support the event area and surrounding businesses and commercial operators, the following locations will have local access permitted via a “soft closure”

- + Ford Street and Hoffmans Lane, between Curtis Rd and McDonald St

Pedestrian access maintained along all existing footpaths and crossing points along streets surrounding the event area. Pedestrians will have full access within the event area. To further manage any pedestrian impacts due to the event please refer to the Security Management Plan by contacting the Event Organiser as listed in Section 1.4 of this document.

1.3. EVENT DETAILS

Event Date:	Friday 25 th April 2025
Event Times:	0600 - 0700
Event Venue:	Darling Street, Balmain
Target Market:	All demographics
Traffic Closure Start:	0230 Friday 25 th April 2025
Bump-in Starts:	0130 Friday 25 th April 2025
Bump-out Finishes:	0930 Friday 25 th April 2025
Traffic Closure End:	0930 Friday 25 th April 2025



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

8

1.4. KEY EVENT CONTACTS

Event Organiser: Inner West Council
Event Manager: Helaina Gardiner
Event Manager Phone: 02 9392 5294
Event Manager Email: helaina.gardiner@innerwest.nsw.gov.au

Venue Owner: Inner West Council
Venue Manager: Inner West Council
Venue Owner Phone: 02 9392 5000
Venue Owner Email: council@innerwest.nsw.gov.au

Police Area Command: Leichhardt PAC
Police Contact: TBC
Police Phone: 02 9552 8099

TfNSW Contact: TBC
TfNSW Phone: TBC
TfNSW Email: TBC

Traffic Control Provider: CATO Location Services
Traffic Control Contact: Anna Rosa
Traffic Control Phone: 0466 388 421
Traffic Control Email: anna@catolocationsservices.com.au



2. EVENT LOCATION

ANZAC Day 2025 will be held at the Loyalty Square War Memorial, Balmain.



2.1. EVENT SITE MAP

A detailed event site plan is being prepared and will be updated closer to the event within the Event Management Plan. Refer to the Event Organiser in section 1.4 of this document for the latest site plan.



3. WORKPLACE HEALTH & SAFETY

3.1. RISK ASSESSMENT PLANS

A Risk Management approach is a fundamental part of the planning for any event. The safety risk identification, assessment and control processes are legal obligations (as per the WHS Act and Regulation 2011) and should be aligned with AS/NZS ISO 31000 Risk Management – Principles and Guidelines. Broader event risk management is best practice and a fundamental part of due diligence.

Inner West Council has compiled Risk Assessments and site-specific safety plans for the events that are not included in this Transport Management Plan.

This section of the Transport Management Plan describes the possible issues/risks that may interfere with the event and the action to be taken to minimise the disturbance of the event.

Issues / Risks	Applicable	Action Taken
All one-way streets are as described	YES	Road closures, barricade and signage installed. Point duty by NSW Police and or authorised Traffic Controllers.
Block access to local businesses	YES	Confirm list of letters to residents, businesses, and car parks. Advertisement of event to general public.
Block Police vehicle access	YES	Confirm access and consultation of routes to and within areas affected by closures with Emergency Services. HVM vehicles can be temporarily moved if required to allow for access. A driver will be available to facilitate this action
Block Ambulance access	YES	Confirm access and consultation of routes to and within areas affected by closures with Emergency Services. HVM vehicles can be temporarily moved if required to allow for access. A driver will be available to facilitate this action
Block fire station access	NO	Normal access to fire station facilities are maintained. Confirm access and consultation of routes to and within areas affected by closures with Emergency Services. HVM vehicles can be temporarily moved if required to allow for access. A driver will be available to facilitate this action
Block heavy vehicle access	YES	All heavy vehicles are diverted before the closure.
Restricted movements banned turns, heavy/high vehicles	YES	All vehicles are diverted before the closure.
Block Public facility (football oval, car park etc.)	NO	Confirm list of letters to residents, businesses, and car parks. Advertisement of event to general public.
Block public transport access	YES	Buses diverted around closure.



Can route use alternatives such as bicycle tracks, paths, parks, bush tracks etc.?	YES	None required
Construction – existing, proposed that may conflict	NO	None required
Numbers of lanes and their width are as described	YES	None required
Road signage existing/temporary	YES	None required Temporary signage Installed and removed by CATO. Special Event Clearway signage will be installed by the TfNSW.
Route impeded by traffic calming devices?	YES	None required
Signalised intersections (flashing yellow? Point duty?)	YES	As required by NSW Police
Tidal flows	NO	None required
Traffic generators shopping centres, schools, churches, industrial area, hospitals	YES	Advertisement of event to general public.
Traffic movement contrary to any Notice	YES	Under the direction of Police or traffic controllers
Traffic signals are as described	Y/N	Controlled by TMC
Turning lanes are as described	YES	None required
Letter Drop Zone Maps to indicate precincts mailed	YES	Notification to be arranged by Event Organiser
Heavy Weather	YES	Heavy weather may cause crowds to depart early or organiser consider delaying/cancelling the event
Flood hazard in event area	NO	Event organiser, TMC/TfNSW and Police provide diversions around flooded area.
Flood hazard at the parking area	NO	None required
Parking during Wet weather	YES	Local Car parks only.
Accident on surrounding roads	YES	If CCTV, monitored by TMC. Facilitate emergency response to area.
Breakdown on surrounding roads	YES	If CCTV, monitored by TMC. Facilitate emergency response to area.
Block public transport access	YES	Divert general public to next available transport, considering safety and circumstances. Relevant transport agency to employ appropriate steps to accommodate.
Delayed Event	N/A	At the discretion of the event organiser
Cancellation of Event	N/A	Cancellation of any aspect of the event will be communicated by the event organiser.



3.2. PUBLIC LIABILITY INSURANCE

Inner West Council has Public Liability Insurance to the value of \$20,000,000. This policy covers all activities taking place as part of ANZAC Day 2025, Balmain. A copy of the current policy is contained in this document.

All contractors completing activities as part of this event are also required to hold a valid Public Liability Insurance to the value of \$20,000,000.

3.3. NSW POLICE FORCE

The Leichhardt (PAC) will be involved in the planning of ANZAC Day 2025, Balmain, via Council's Traffic Committee including aspects relating to use of the roadway, closure of selected roads and hostile vehicle mitigation. They will be formally notified at least two weeks prior to the event taking place.

User Pays Police will be engaged by the event organisers based on guidance from PAC, in conjunction with the relevant Security Management Plan.

3.4. NSW FIRE & RESCUE AND NSW AMBULANCE

NSW Fire & Rescue and NSW Ambulance will be notified at least two weeks prior to the event taking place.

3.5. EVENT DELAYS, POSTPONEMENT OR CANCELLATION

Any decision to delay, postpone or cancel the event due to weather impacts or any other reason will be made by the Event Organiser and follow their protocols for emergency management. Once any decision is made in this regard it will be communicated to all relevant stakeholders as per the event's Emergency Management Plan.



4. TRAFFIC AND TRANSPORT MANAGEMENT

4.1. EVENT IMPACT ON ROAD NETWORK

It should be noted that the land uses along the subject section of Darling Street are predominantly retail/commercial with a few residential properties.

All eastbound and westbound traffic except buses will need to use Curtis Road or Palmer Street to access Balmain East.

Every effort has been made to minimise the disruption to road users, residents and businesses by implementing local access closure points. Access is maintained outside of the event area throughout the event.

4.2. DETOURS

Traffic Guidance Systems (TGSs) will be implemented for the closures and detours noted above. Refer to the attachments to this TMP for copies.

4.3. IMPACTS ON PUBLIC TRANSPORT

ANZAC Day 2025 will be held in Balmain on 25th April. Local buses will operate on a public holiday timetable and will therefore be reduced on the event day in comparison to a weekday. The impact on public transport will be as follows:

4.5.1. BUSES

As discussed with Transit Systems representatives, all buses need to terminate at Darling Street/Rowntree Street/Montague Street intersection as Curtis Road is not suitable for bus movements.

To maintain transport services between East Balmain and Balmain, Council will organise a shuttle bus services running between Balmain East and Darling Street/Rowntree Street.

The shuttle bus service route is shown on the TGS.

The Transit Systems representative will organise posting notifications at all bus stops on Darling Street between Beattie Street and Balmain East wharf to advise passengers to use Council's community buses on the morning of 25 April.



4.5.3. TAXIS AND RIDE-SHARE PROVIDERS

Taxis and ride-share providers will continue to have access to the road network around the closures as per other road users.

4.6. CHANGES TO CYCLE ROUTES

The closure of Darling Street will not significantly affect designated cycling routes through Balmain. Cyclists will still be able to dismount and walk their bikes through the event site. All existing cycle routes will remain in place and operational around the event site.

4.7. HOSTILE VEHICLE MITIGATION

The closure of the streets is designed to provide an extended pedestrian-friendly area for the event to operate in and for pedestrians to participate.

The Event Organiser will, in conjunction with the nominated Security Advisor, produce a Hostile Vehicle Mitigation, and Target Hardening Plan for the event.

When the closures are installed, applicable hostile vehicle mitigation (HVM) vehicles or barriers will be placed at each entry point as noted on the TGS and HVM Risk Assessment at the direction of the nominated HVM Security Advisor to prevent access to the site by unauthorised or errant vehicles. A driver for each vehicle will always be present in case the vehicle needs to be moved to allow access for emergency vehicles.

Once the HVM install is complete, the nominated Security Advisor will be on site to authorise the implementation, the positioning and suitability of all devices.

Authorisation is to be granted from the Event Manager for vehicles to be moved once in position for access when required.

4.8. RE-OPENING ROADS AFTER THE EVENT

The road closures in Balmain are planned to re-open at 0930, however this may occur earlier if the road is clear and it is safe to do so.



4.9. TRAFFIC CONTROL

The implementation of the traffic guidance schemes, including road closures, will be supervised by an accredited Traffic Manager from CATO Location Services.

Temporary traffic control equipment, barricades, and signage must be placed in accordance with the Traffic Guidance Schemes by qualified traffic controllers who possess a TfNSW execute traffic guidance schemes certification.

Other qualifications that are required by the authorised traffic controllers include (but not limited to):

- + General Construction Induction (also known as “White Card”)
- + Traffic Controller Licence (also known as “Blue Card”)
- + Implement Traffic Control Licence (also known as “Yellow Card”)
- + Prepare Work Zone TMP Licence (also known as “Orange Card”)



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

16

5. MINIMISING IMPACT ON THE NON-EVENT COMMUNITY

5.1. ACCESS FOR EMERGENCY VEHICLES

A minimum four metre emergency lane will be maintained along the entire closure, beyond the HVM vehicles. There will be no event infrastructure in the emergency lane. Traffic controllers will be onsite to assist emergency vehicle through the closure points. While HVM measures will be in place, a driver for all vehicles will always be present in case the vehicle needs to be moved to allow access for emergency vehicles.

5.2. ADVERTISING TRAFFIC MANAGEMENT ARRANGEMENTS

The Event Organiser will advertise the road closures via social media and on Council's website. A letterbox drop will be conducted to all resident and businesses in the immediately vicinity of the event sites and road closures as well as the surrounding streets in Balmain.

The road closures will also be advertised by way of trailer mounted variable message signs (VMS) to warn other road users of the upcoming event, and the road closures that will be implemented

5.3. EVENT PROMOTION

The Event Organiser will promote ANZAC Day 2025 and the road closures taking place using a variety of methods in the weeks preceding the event including:

- + Council's social media platforms
- + Council's website
- + Local signage where available



6 APPROVALS

6.1. EVENT ORGANISER APPROVAL

TMP Approved by:

.....
(Name)

.....
(Signature) (Date)

6.2. AUTHORISATION TO REGULATE TRAFFIC

Council's traffic management requirements have been met. Regulation of traffic is therefore authorised for all non-classified roads described in the risk management plans and this TMP.

Regulation of Traffic Authorised by:

.....
(Council)

.....
(Name)

.....
(Signature) (Date)



The Transport for New South Wales (TfNSW) traffic management requirements have been met. Regulation of traffic is therefore authorised for all classified roads described in the risk management plans and this TMP.

Regulation of Traffic Authorised by:

.....
(TfNSW)

.....
(Name)

.....
(Signature) (Date)



6.3 PUBLIC LIABILITY INSURANCE

UPDATED PLI TO BE PROVIDED



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

20

7 ATTACHMENTS

7.1 TRAFFIC GUIDANCE SCHEMES

TGSs are provided on the following pages showing:

- + The traffic management measures in place to facilitate the road closures for the event



ANZAC DAY 2025 – BALMAIN – TRANSPORT MANAGEMENT PLAN
V1.0 – 25th OCTOBER 2024 – CRAIG HUNTER – TCT0015830

21

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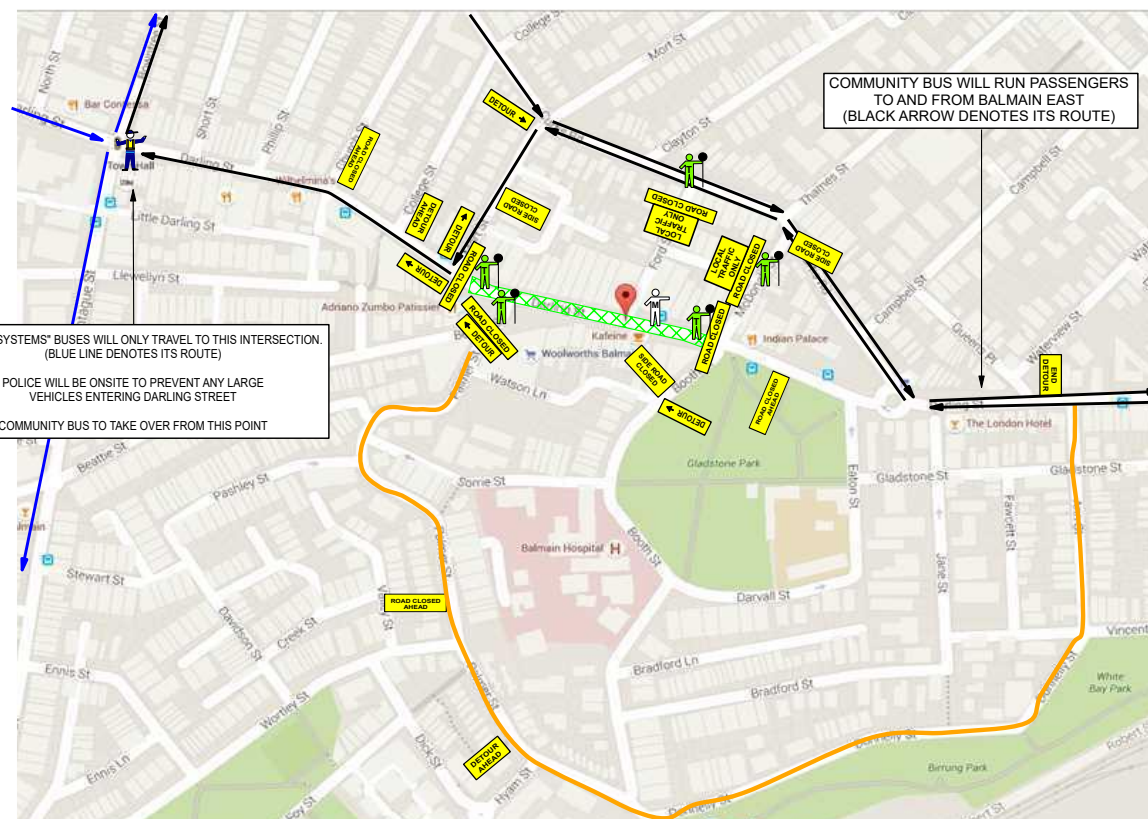


MANIFEST

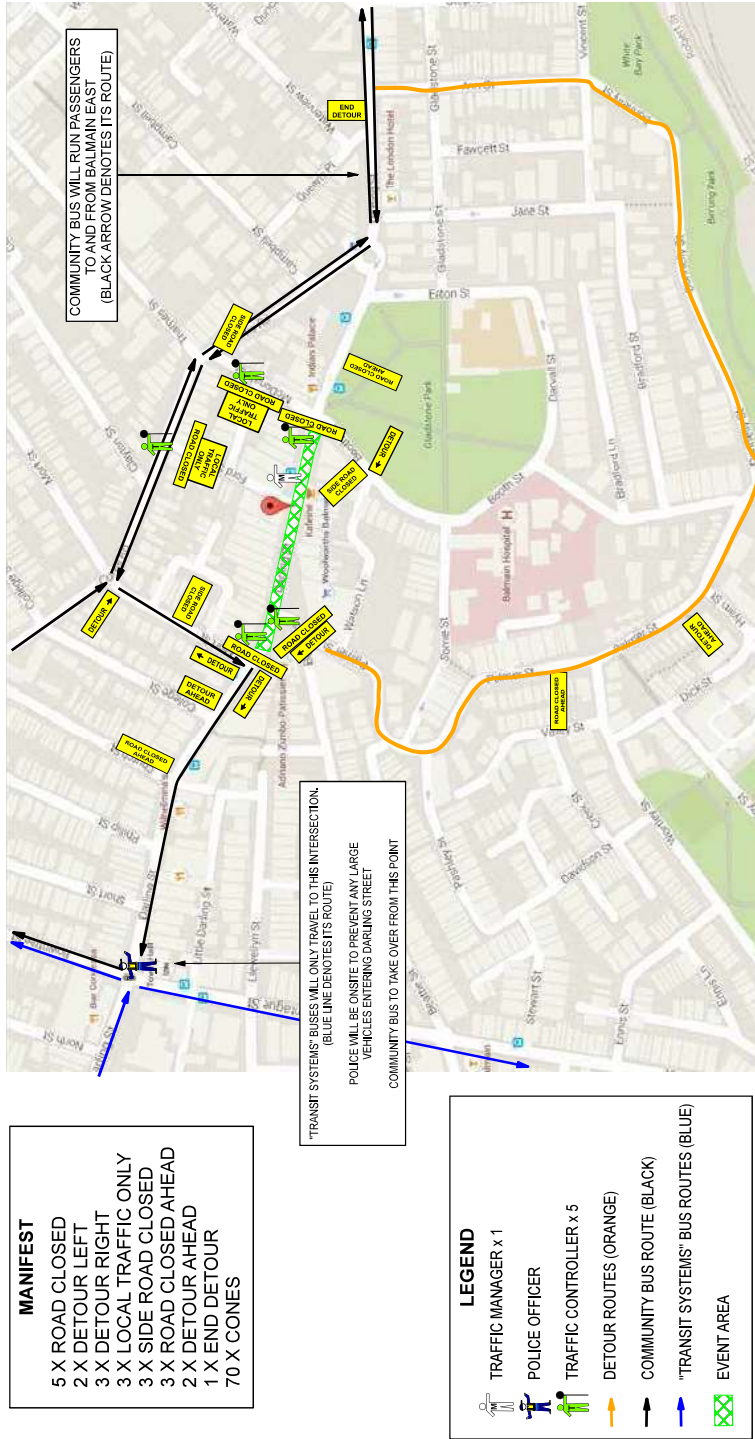
5 X ROAD CLOSED
2 X DETOUR LEFT
3 X DETOUR RIGHT
3 X LOCAL TRAFFIC ONLY
3 X SIDE ROAD CLOSED
3 X ROAD CLOSED AHEAD
2 X DETOUR AHEAD
1 X END DETOUR
70 X CONES

LEGEND

- TRAFFIC MANAGER x 1
- POLICE OFFICER
- TRAFFIC CONTROLLER x 5
- DETOUR ROUTES (ORANGE)
- COMMUNITY BUS ROUTE (BLACK)
- "TRANSIT SYSTEMS" BUS ROUTES (BLUE)
- EVENT AREA



DRAWN BY:	CRAIG HUNTER	TCT 0015830		<input type="checkbox"/> CLOSURE <input type="checkbox"/> DETOUR <input type="checkbox"/> TAMP	<input type="checkbox"/> RMS <input type="checkbox"/> REDIRECT <input type="checkbox"/> CONTRA FLOW	<input type="checkbox"/> ROLLING BLOCK <input type="checkbox"/> BARRICADE <input type="checkbox"/> PORTABLE SIGNS	<input type="checkbox"/> JANE MERGE <input type="checkbox"/> BUS PLAN <input type="checkbox"/> SIGNAGE PLAN	PROJECT:	ANZAC DAY DAWN SERVICE 2025		APPROVAL REQUIRED:	LOCAL <input checked="" type="checkbox"/>	EVENT TIMES: 0600 - 0700	PLAN No:	ANZ-1				
APPROVED BY:	KIERAN CATO	TCT 0040880		<input type="checkbox"/> TAMP <input type="checkbox"/> CONTRA FLOW <input type="checkbox"/> PORTABLE SIGNS	<input type="checkbox"/> RMS <input type="checkbox"/> REDIRECT <input type="checkbox"/> CONTRA FLOW	<input type="checkbox"/> ROLLING BLOCK <input type="checkbox"/> BARRICADE <input type="checkbox"/> PORTABLE SIGNS	<input type="checkbox"/> JANE MERGE <input type="checkbox"/> BUS PLAN <input type="checkbox"/> SIGNAGE PLAN	CLIENT:	Inner West Council		STATE:	<input checked="" type="checkbox"/>	SET UP: 1hr	VARIANT:	(A)	TYPE:	TGS		
POLICY: Cato Location Services P/L do not accept responsibility of any traffic guidance scheme if the traffic management is not implemented by Cato Location Services P/L.				1. THIS TRAFFIC GUIDANCE SCHEME HAS BEEN DRAWN IN ACCORDANCE WITH AS 1742.3 AND "TRAFFIC CONTROL AT WORKSITES" MANUAL V6.1. 2. SIGNS & DEVICES ARE TO BE PLACED IN ACCORDANCE WITH THIS TGS. MODIFICATION MAY BE MADE BY PERSONS HOLDING A RMS "PWZTMP" QUALIFICATION ONLY. 3. ALL SIGNS AND DEVICES USED MUST COMPLY WITH AUSTRALIA STANDARD AS1742.3. 4. THIS DOCUMENT IS COPYRIGHT / PROPERTY OF CATO LOGISTICS PTY LTD. THIS DRAWING SHALL ONLY BE USED FOR THE PURPOSE FOR WHICH IT IS INTENDED. UNAUTHORISED USE OF THIS DRAWING IS PROHIBITED.				LOCATION:	Darling Street, Balmain		CONTACT:	Helaina Gardiner	PHONE:	02 9392 5294	CLOSURE IN:	0230 25/04	EVENT DATE:	25/04/2025	
					ORIGINAL SIZE:	A4 LANDSCAPE	COUNCIL:	Inner West Council		OTHER:	<input type="checkbox"/>	CLOSURE OUT:	0930 25/04	DATE:	25/10/2024				
												PACK DOWN:	1hr						



DRAWN BY:	CRAIG HUNTER	TCT 0015630	APPROVED BY:	MERAN CATO	TCT 0040880	CR+O
POLICY: Cato Location Services Pty, do not accept responsibility of any traffic guidance scheme if the traffic management is not implemented by Cato Location Services Pty.						CR+O
PROJECT: ANZAC DAY DAWN SERVICE 2025						PLAN No: ANZ-1
CLIENT: Inner West Council						VARIANT: (A)
LOCATION: Darling Street, Balmain						TYPE: TGS
CONTACT: Helaina Gardner						REV: 0.0
PHONE: 02 9392 5294						EVENT DATE: 25/04/2025
ORIGINAL SIZE: A4 LANDSCAPE						DATE: 25/10/2024
COUNCIL: Inner West Council						
APPROVAL REQUIRED:						
LOCAL <input checked="" type="checkbox"/>						
STATE <input checked="" type="checkbox"/>						
INSW <input type="checkbox"/>						
OTHER <input type="checkbox"/>						
EVENT TIMES: 0630 - 0700						
SET UP: 1hr						
CLOSURE IN: 0230 25/04						
CLOSURE OUT: 0930 25/04						
PACK DOWN: 1hr						

Item No: LTC1124(1) Item 13

Subject: AUDLEY STREET, AUDLEY LANE, SADLIER CRESCENT, FISHER STREET, AND EASTBOUND PARKING LANES ON NEW CANTERBURY ROAD PETERSHAM – TEMPORARY FULL ROAD CLOSURES FOR SPECIAL EVENT ON SATURDAY 22 MARCH 2025 – PETERSHAM FESTIVAL BAIRRO PORTUGUESE (DAMUM - STANMORE WARD / NEWTOWN ELECTORATE / INNER WEST PAC)

Prepared By: Jennifer Adams - Traffic Engineer

Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the proposed temporary road closure of Audley Street (between New Canterbury Road and Trafalgar Street), Sadlier Crescent (between Audley Street and Abels Lane) and Fisher Street (between Audley Street and Regent Street), Audley Lane north of New Canterbury Road (resident access excepted) as well as the eastbound parking lane on New Canterbury Road (between Audley Street and Audley Lane), Petersham on Saturday 22 March 2025, from 6:00am until 2.00am (Sunday), for the holding of '2025 Bairro Portuguese Petersham Festival', be approved subject to the applicant complying with but not limited to the following conditions:

1. A Traffic Management Plan (TMP) is submitted to and approved by Transport for NSW; and an application for a Road Occupancy Licence and a temporary Speed Zone Authorisation is forwarded to and approved by the Transport Management Centre.
2. Notice of the proposed event is forwarded to all affected residents and businesses, including the N.S.W. Police / Inner West Local Area Commander, Fire and Rescue NSW, NSW Ambulance and Transit Systems.
3. Transit Systems – Inner West Bus Services be requested to implement a revised routing for scheduled bus services in Audley Street on the day of the event and install temporary bus stops as required.
4. A minimum four (4) metre unencumbered passage be available for emergency vehicles through the closed section.
5. The occupation of the road carriageways must not occur until the roads have been physically closed.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Inner West Council will be presenting Petersham Festival - Bairro Portuguese on Saturday 22 March 2025 from 3:00pm until 9:00pm on Audley Street and Fisher Street, Petersham. This event celebrates the very best of Portuguese culture with entertainment, music, dance, cultural activities, and traditional and contemporary foods. To facilitate the event, areas of Petersham

will be closed and there will be road closures necessitating some road detours and bus diversions in surrounding streets.

Roads affected include Audley Street (between New Canterbury Road and Trafalgar Street), Sadlier Crescent (between Audley Street and Abels Lane) and Fisher Street (between Audley Street and Regent Street), Audley Lane north of New Canterbury Road (resident access excepted) as well as the eastbound parking lane on New Canterbury Road (between Audley Street and Audley Lane), Petersham from 6.00am Saturday 22 March 2025 until 2.00am Sunday 23 March 2025. It is recommended that Council agree to the temporary full road closures subject to all standard Council conditions for a temporary full road closure (ENRC/2024/0064). This report outlines the traffic management plan for the 2025 event.

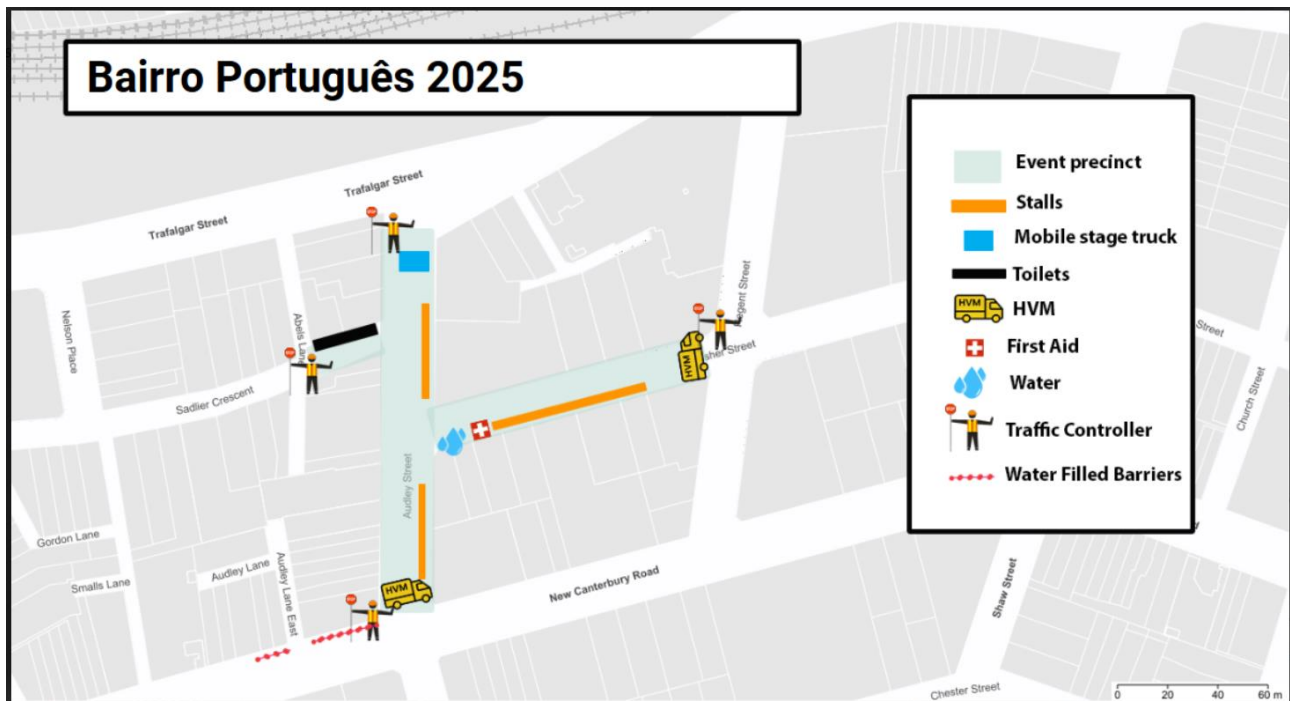
BACKGROUND

Temporary road closures have been successfully implemented in previous years to hold the 'Bairro Portuguese Food and Wine Fair'. This Fair, since its inception in 2003, aims to showcase Petersham's businesses and services, attracts people to the area and has a multicultural theme. On all previous occasions, the closures were effective, and no major problems were experienced.

OFFICERS COMMENTS

Event

The event will be held on Saturday 22 March 2025 between 3.00pm and 9.00pm and will necessitate the temporary closure of the nominated streets in Petersham from 6.00am Saturday 22 March 2025 until 2.00am Sunday 23 March 2025. Refer to the site map below.

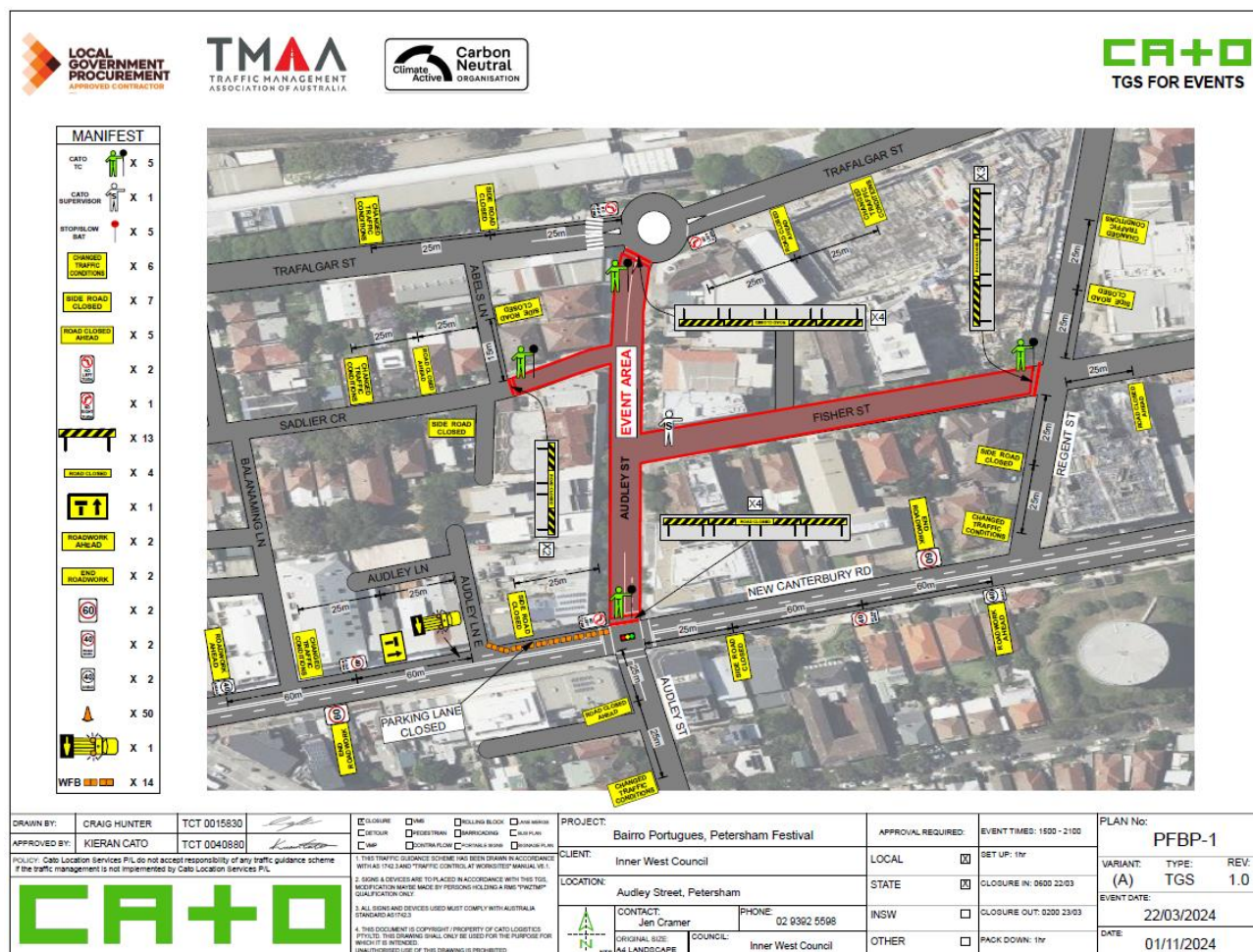


This is an extension of a Class 2 Event under the TfNSW' Special Events Guide where it impacts local traffic and transport systems but does not impact major traffic and transport systems and it disrupts the non-event community in the area around the event but not over a wide area. The event requires the involvement of Police and Local Council and a detailed Transport Management Plan (TMP).

Traffic Management Plan and Traffic Guidance Scheme

A Traffic Management Plan (TMP) has been supplied by CATO Location Services, the scope of which includes the provision for the safe movement of vehicular traffic in and out of the event areas at the Bairro Portuguese Food and Wine Fair on Saturday 22 March 2025. The Traffic Management Plan (TMP) and Traffic Guidance Scheme (TGS) are attached at the end of this report.

Item 13

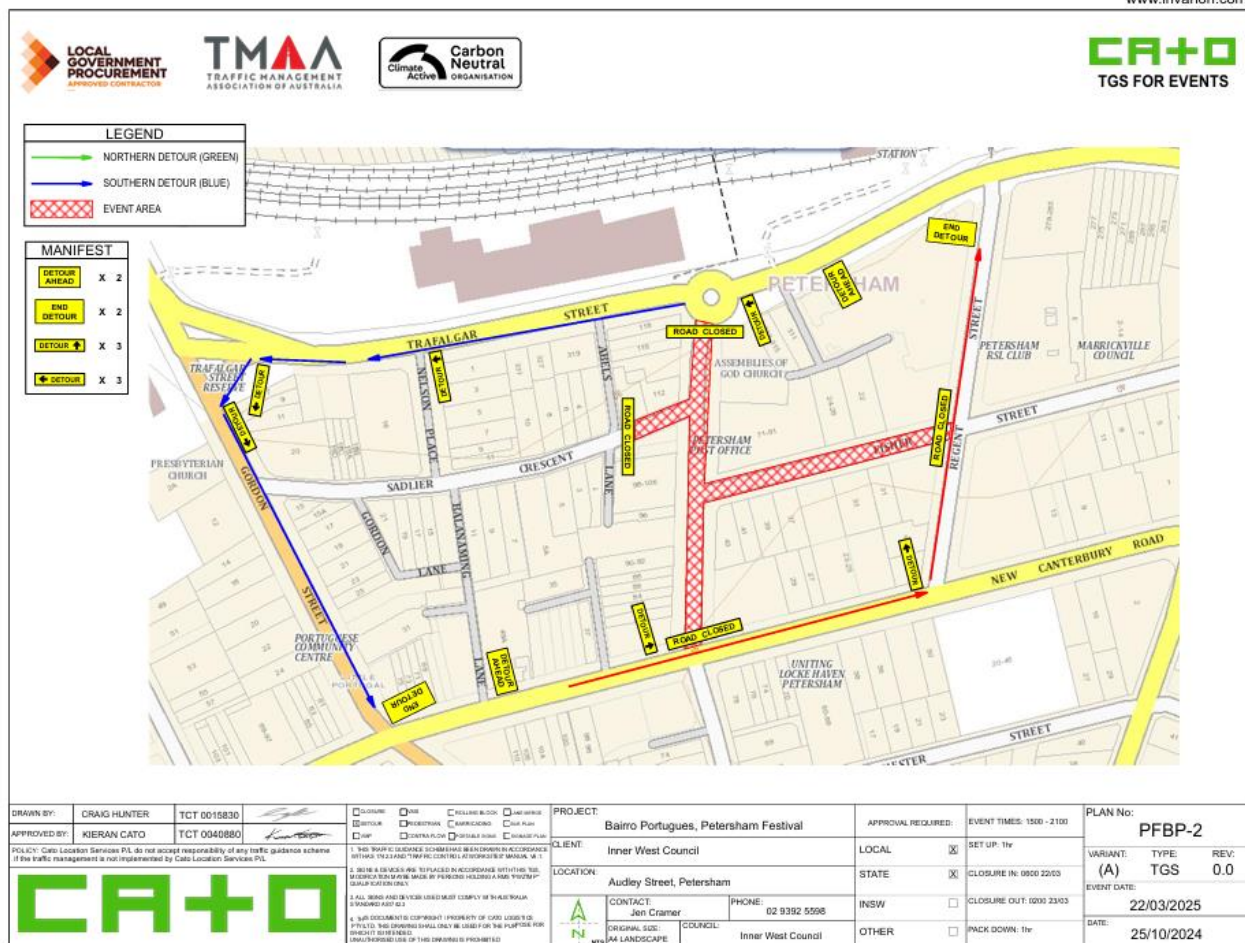


Access around the event site will be maintained by 2 detours. The detour loops will include New Canterbury Road, Regent Street, Trafalgar Street and Gordon Street. Please refer to the Detour Plan reproduced below.

Access for local residents and businesses will be maintained wherever possible. It is noted that the road closure area for the Petersham Festival includes several residences and businesses fronting Audley Street. Pedestrian access to these businesses and residences will be managed within the closure, however businesses vehicle access will not be permitted within the event area. All non-local traffic will be redirected around the area via the detour as per the implemented detour plan. The Event Organiser will notify all residents and businesses impacted by the event including confirmation of the restricted vehicle movements during the event operating times. This will include reviewing any requirements for changes to commercial waste collection times and/or locations.

Access for emergency vehicles will be maintained at all times. A minimum four metre emergency lane will be preserved along the entire closure. There will be no event infrastructure in the emergency lane. Traffic controllers will be onsite to assist emergency vehicle through the closure points.

A Traffic Guidance System (TGS) will be implemented for the closures and detours noted above. Refer to the attached TMP.



Document Set ID: 39768219
Version: 1, Version Date: 31/10/2024

Lane closures of New Canterbury Road

As per previous events held, it is proposed to close the parking lanes on New Canterbury Road (between Audley Lane and Audley Street).

In previous years the event attracted in excess of 15,000 attendees prompting the event coordinators to look at ways to ease congestion at the main site and make access easier from New Canterbury Road. The closure of the parking lanes on New Canterbury Road improves access to the area where the main event is to be conducted and improves the amenity of the footpath dining outside a number of the eateries along New Canterbury Road which are adjacent to the closure.

There is significant improvement in access for people with disabilities as congestion will be reduced and there will be no obstacles such as tables, chairs, planter boxes, light/sign poles and bins in the closed lanes. The solid barriers will separate the pedestrians from the moving traffic on the adjacent lane and will also help prevent young children who may be separated from their custodians escaping onto the main thoroughfare and possibly being hit by passing traffic.

The TGS indicates that a reduced temporary speed limit of 40km/h is required on New Canterbury Road (between Gordon Street and Audley Street) during the closure of the parking lanes for the subject event. Thus, an application for a Road Occupancy Licence will be

necessary. It is noted that New Canterbury Road is a State Road and under the jurisdiction of Transport for NSW (TfNSW).

Duration of road closures

The timespan for the road closure is necessary to allow stalls and stages to be set up before the event and dismantled after the event. The closure will be affected by the placement of barricades at the nominated locations shown in the TGS.

Impacts on buses

Audley Street is used by scheduled Transit Systems bus services and they will need to implement the re-routing of services on the day as in previous years. Transit Systems 445 service will all be impacted by the event road closures.

Local buses will operate on a weekend timetable and will therefore be reduced on the event day in comparison to a weekday. Bus route 445 will be detoured around the event area via 2 detours, 1 detour for eastbound buses and 1 detour for westbound buses as shown below.

- Eastbound detour – New Canterbury Road > Gordon Street > Trafalgar Street
- Westbound detour – Trafalgar Street > Gordon Street > New Canterbury Road

The stops listed below will be affected during the proposed event times. Notices will be put up at impacted bus stops to direct passengers to the nearest stop.

Bus stops impacted – Eastbound, 204937 & 204911
– Westbound 204938 & 204946



Bus stop closure

Bairro Portuguese Petersham Food & Wine Fair

Sunday 19 March 2023
1:30am – 10:00pm

While roads are closed for the event, buses will not run to or from this bus stop. Buses will detour around the road closures

Route affected	Service change
445	<p>Campsie to Balmain via MarketPlace Leichhardt</p> <p>Bus Stops Closed</p> <ul style="list-style-type: none"> • Stop ID: 204937 - Audley St opp Fisher St • Stop ID: 204938 - Audley St at Fisher St • Stop ID: 204946 - New Canterbury Rd before Livingstone Rd
Nearest alternative bus stops	<p>New Canterbury Rd at West St (Stop ID: 204967)</p> <p>Gordon St before New Canterbury Rd (Stop ID: 204953)</p> <p>Petersham Station, Trafalgar St (Stop ID: 204911)</p> <p>Trafalgar St opp Petersham Station (Stop ID: 204912)</p>

NSW Transport Visit transport.nsw.info

Impacts on traffic

There will be no special event clearways. The TMP states that “*The event organisers have engaged Cato Location Services to barricade all parking spaces within the event area the night before the event commences.*” Both sides of Audley Street, between Trafalgar Street and New Canterbury Road, will be barricaded to accommodate a pedestrian friendly event area.

In relation to parking it is stated in the TMP that “*There is limited street parking available for participants of the event, the event organiser will promote public transport as the best way of getting to the event due to its proximity to regular bus and train services.*”

'No Parking - Special Event' signs will be affixed over all existing timed parking restrictions signs in the area to be closed on the afternoon before the day of the event. Residents in Fisher Street will be allowed access into and out of their properties.

The TMP states that “*The closure of Audley St and Fisher St will not significantly affect designated cycling routes through Petersham. Cyclists will still be able to dismount and walk their bikes through the event site. All existing cycle routes will remain in place and operational around the event site.*”

Variable Message Boards

The TMP notes that there will be two (2) VMS installed around the event site to inform road users of the changed traffic conditions one (1) week before the event, and also on the event date.

The locations and messages are as follows:

Location 1	Messages	
	FRI 14/03/25 til 0500 – SAT 22/03/25	0500 – SAT 22/03/25 til 0200 – SUN 23/03/25
TBC (facing southbound traffic)	Petersham Festival SAT 22 MAR	AUDLEY ST AND FISHER ST CLOSED TIL 2 AM
	AUDLEY ST AND FISHER ST CLOSED	FOLLOW DETOUR SIGNS
		EXPECT DELAYS

Location 2	Messages	
	FRI 14/03/25 til 0500 – SAT 22/03/25	0500 – SAT 22/03/25 til 0200 – SUN 23/03/25
TBC (facing northbound traffic)	Petersham Festival SAT 22 MAR	AUDLEY ST AND FISHER ST CLOSED TIL 2 AM
	AUDLEY ST AND FISHER ST CLOSED	FOLLOW DETOUR SIGNS
		EXPECT DELAYS

PUBLIC CONSULTATION

The proposed temporary full road closures have been advertised in accordance with the Roads Act 1993.

Advice of the proposed event must be forwarded to all the appropriate authorities, including emergency services. A copy of the draft notification letter is attached at the end of this report.

The TMP states that:

"The Event Organiser will advertise the road closures via social media and on Council's website. A letterbox drop will be conducted to all resident and businesses in the immediately vicinity of the event sites and road closures as well as the surrounding streets in Petersham".

"The Event Organiser will promote Petersham Festival and the road closures taking place using a variety of methods in the weeks preceding the event including:

- + Council's social media platforms,*
- + Council's website,*
- + Local signage where available,*
- + Flyers,*
- + VMS Boards and,*
- + Local News Paper"*

FINANCIAL IMPLICATIONS

Funding of \$112k has been allocated by Council for organising the 'Bairro Portuguese Food and Wine Fair' event under the 2024/2025 Major Community Events Program.

CONCLUSION

It is recommended that Council approve to the temporary full road closures on Saturday 22 March 2025 subject to complying with the recommendations stated in this report along with all standard conditions for temporary full road closures.

Other conditions that need to be complied with include:

- The Traffic Management Plan is to be submitted to TfNSW for consideration and approval and a Road Occupancy License application is to be submitted to the Transport Management Centre by CATO Location Services.
- Advance notifications signs for the event are strategically installed at least two (2) weeks prior to the event;
- "No Parking - Special Event" signs are affixed over all existing timed parking restriction signs within the sections of streets to be closed on the afternoon of the day prior to the event;
- A 4-metre wide emergency vehicle access must be maintained through the closed road areas during the course of the event;
- The applicant is to consult with all affected residents and/or businesses in the area in writing and to conduct a letter box drop of surrounding properties at least two weeks prior to event; and
- Adequate vehicular traffic control shall be provided for the protection and convenience of pedestrians and motorists including appropriate signage and flagging. Workers shall be specially designated for this role (and carry appropriate certificates), as necessary to comply with this condition. This is to be carried out in accordance with the Australian Standard AS 1742.3 - Traffic Control Devices for works on roads.

EVENT NOTIFICATION

SPECIAL EVENT ROAD CLOSURE – BAIRRO PORTUGUÊS Saturday 22 March 2025

Dear Resident and Business Owners,

Inner West Council is presenting Bairro Português on Saturday 22 March 2025 from 3pm until 9pm on Audley Street and Fisher Street, Petersham.

Setting up of the site will start from 6am Saturday 22 March and the pack down of the event site will commence following the conclusion of the event.

To facilitate the event, areas of Petersham will be closed between 6am 22 March and 2am Saturday 22 March 2025.

The following roads and sites are affected by the closure:

- Audley Street between New Canterbury Road and Trafalgar Street
- Fisher Street between Audley Street and Regent Street
- Sadlier Crescent between Audley Street and Abels Lane
- Audley Lane north of New Canterbury Road

It is important to take note of these road closures. If your vehicle is parked in an external or internal car park within the event site, you will not be able to move your vehicle for the duration of the road closure.

If your vehicle is parked on any of the closed roads your vehicle will be towed.

Council will endeavour to minimise any disruption to local resident amenities, and we hope you can attend this free event. For more information please go to <https://www.innerwest.nsw.gov.au/explore/whats-on/annual-events/bairro> or you can contact me directly on 9392 5259.

Sincerely,



Michael Daly
Manager – Creative Communities

Page 1 of 1

Document Set ID: 39768203
Version: 1, Version Date: 31/10/2024

ATTACHMENTS

1. [Download](#) TMP - Petersham Festival - IWC - 22 March 2025 - V1.3 (003)



TRAFFIC MANAGEMENT PLAN

Bairro Português

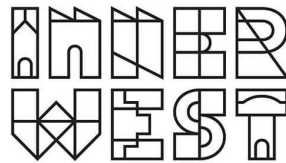
Petersham Festival

AUDLEY ST, PETERSHAM



Saturday 22nd March 2025

PREPARED FOR



by CATO Location Services

TABLE OF CONTENTS

TABLE OF CONTENTS.....	2
STATEMENT OF CONFIDENTIALITY & NON-DISCLOSURE	4
DOCUMENT CONTROL	5
DEFINITIONS	6
REFERENCE DOCUMENTS	6
1. GENERAL EVENT INFORMATION.....	7
1.1. EVENT SUMMARY.....	7
1.2. TRAFFIC IMPACT SUMMARY.....	8
1.3. EVENT DETAILS	8
1.4. KEY EVENT CONTACTS	9
2. EVENT LOCATION	10
2.1. EVENT SITE MAP	10
3. WORKPLACE HEALTH & SAFETY	11
3.1 RISK ASSESSMENT PLANS	11
3.2 PUBLIC LIABILITY INSURANCE	13
3.3 NSW POLICE FORCE	13
3.4 NSW FIRE & RESCUE AND NSW AMBULANCE	13
3.5 EVENT DELAYS, POSTPONEMENT OR CANCELLATION	13
4. TRAFFIC AND TRANSPORT MANAGEMENT	14
4.1 EVENT IMPACT ON ROAD NETWORK.....	14
4.2 DETOURS	14
4.3 SPECIAL EVENT CLEARWAYS.....	14
4.4 VEHICLE ACCESS TO THE EVENT SITE	15
4.5 CHANGES TO CYCLE ROUTES	15
4.6 IMPACTS ON PUBLIC TRANSPORT.....	16
4.6.1. TRAIN SERVICES.....	16
4.6.2. TAXIS AND RIDE-SHARE PROVIDERS	16
4.7 HOSTILE VEHICLE MITIGATION	16
4.8 RE-OPENING ROADS AFTER THE EVENT.....	17
4.9 TRAFFIC CONTROL.....	17
5. MINIMISING IMPACT ON THE NON-EVENT COMMUNITY	18
5.1 ACCESS FOR LOCAL RESIDENTS AND BUSINESSES.....	18



5.2	ACCESS FOR EMERGENCY VEHICLES	18
5.3	ADVERTISING TRAFFIC MANAGEMENT ARRANGEMENTS	18
5.4	EVENT PROMOTION	18
5.5	VARIABLE MESSAGE SIGNS	19
6.	APPROVALS	20
6.1	EVENT ORGANISER APPROVAL	20
6.2	AUTHORISATION TO REGULATE TRAFFIC	20
6.3	PUBLIC LIABILITY INSURANCE	22
7.	ATTACHMENTS	23
7.1	TRAFFIC GUIDANCE SYSTEMS	23



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DOCUMENT CONTROL

This document is uncontrolled once printed – the final version with specifications and site diagrams will be locked for printing and restricted by password.

A copy of the final version will be supplied to the event organiser prior to the event.

Version	Prepared by	Date	Comments	Reviewed by
1.0	Katerina Stewart	25/10/2024	Initial draft	Anna Rosa
1.1	Katerina Stewart	25/10/2024	Minor adjustments to timings and wording.	Anna Rosa
1.2	Craig Hunter	29/10/2024	Street reference revision	Anna Rosa
1.3	Craig Hunter	01/11/2024	Bus detour and speed reduction added	Anna Rosa



DEFINITIONS

Term	Definition
TMP	Transport Management Plan
TGS	Traffic Guidance Scheme
VMS	Variable Message Sign
HVM	Hostile Vehicle Mitigation
TMC	Transport Management Centre
SMP	Security Management Plan
TfNSW	Transport for NSW

REFERENCE DOCUMENTS

Title	Version
Guide to Traffic and Transport Management for Special Events	v3.5, July 2018
Traffic Control at Worksites Technical Manual	v6.1, February 2022
Workplace Health and Safety ACT NSW	2011
Workplace Health and Safety ACT Amendments NSW	2023
Workplace Health and Safety Regulation NSW	2017
Safe Work NSW website – www.safeworkaustralia.nsw.gov.au	Current website.
Working near Sydney Light Rail – www.transdev.com.au/solutions/work-access-permits/	Current website.
Safe Work Code of Practice – First Aid in the workplace	January 2020
Safe Work Code of Practice – Hazardous Manual Tasks	August 2019
Safe Work Code of Practice – Managing the risks of plant in the workplace	December 2022
Safe Work Code of Practice – How to manage work health and safety risks	August 2019
Safe Work Guideline – Traffic Management: Guide for Events	April 2021



1. GENERAL EVENT INFORMATION

1.1. EVENT SUMMARY

Inner West Council will be holding the “Petersham Festival” at the following locations -

- + Audley St, Petersham
- + Fisher St, Petersham
- + Sadlier St, Petersham

Bairro Português event is a spirited celebration of Petersham's rich Portuguese cultural heritage, bringing the community together for a dynamic street festival featuring traditional and contemporary food, music, and entertainment.

As attendees stroll through the festival, spectators can connect with local artisans selling their crafts, participate in interactive seminars, and enjoy activities for all ages. This event promotes local and cultural connections, creating a sense of community among both locals and visitors. Bairro Português event serves as a reminder of the significance of honouring our common past while also embracing the changing nature of cultural expression.

This family friendly event is non-ticketed, free to all guests. The event will commence at 1500 and conclude at 2100 on Saturday 22nd of March.

Inner West Council and Cato Location Services acknowledges the traditional Owners of the Gadigal and Wangal peoples of the Eora Nation. We pay our respect to their Elders past and present.



1.2. TRAFFIC IMPACT SUMMARY

Inner West Councils Petersham Festival involves a partial road closure to vehicles along Audley St and Fisher St, Petersham. To support the event area the following location is to be closed with formal road closures, also known as a “hard road closure”:

- + Audley St, between Trafalgar St & New Canterbury Rd
- + Fisher St, between Audley St and Regent St
- + Sadlier Cr, between Audley St and Abels Ln

Inner West Councils Petersham Festival also involves a partial parking lane closure.

- + New Canterbury Rd, Eastbound between Audley Ln & Audley St

To further support the event area and surrounding traffic movements, locals within the event area will have access to their residency via surrounding streets and a detour around the road closure will be implemented.

Pedestrian access will be maintained along all existing footpaths and crossing points along streets surrounding the event area. Pedestrians will have full access within the event area. To further manage any pedestrian impacts due to the event please refer to the Security Management Plan by contacting the Event Organiser as listed in Section 1.4 of this document.

1.3. EVENT DETAILS

Event Date:	Saturday 22 March 2025
Event Times:	1500 to 2100
Event Venue:	Audley St, Fisher St, Sadlier Cr and Audley Ln Petersham
Expected Attendance:	Approximately 10,000 people
Target Market:	All demographics
Traffic Closure Start:	Saturday 22 March at 0600
Bump-in Starts:	Saturday 22 March at 0600
Bump-out Finishes:	Sunday 23 March at 0200
Traffic Closure End:	Sunday 23 March at 0200



1.4. KEY EVENT CONTACTS

Event Organiser: Inner West Council
Event Manager: Jen Cramer
Event Manager Phone: 02 9392 5598
Event Manager Email: jennifer.cramer@innerwest.nsw.gov.au

Venue Owner: Inner West Council
Venue Manager: Inner West Council
Venue Owner Phone: 02 9392 5598
Venue Owner Email: council@innerwest.nsw.gov.au

Police Area Command: Inner West PAC
Police Contact: TBC
Police Phone: TBC
Police Email: TBC

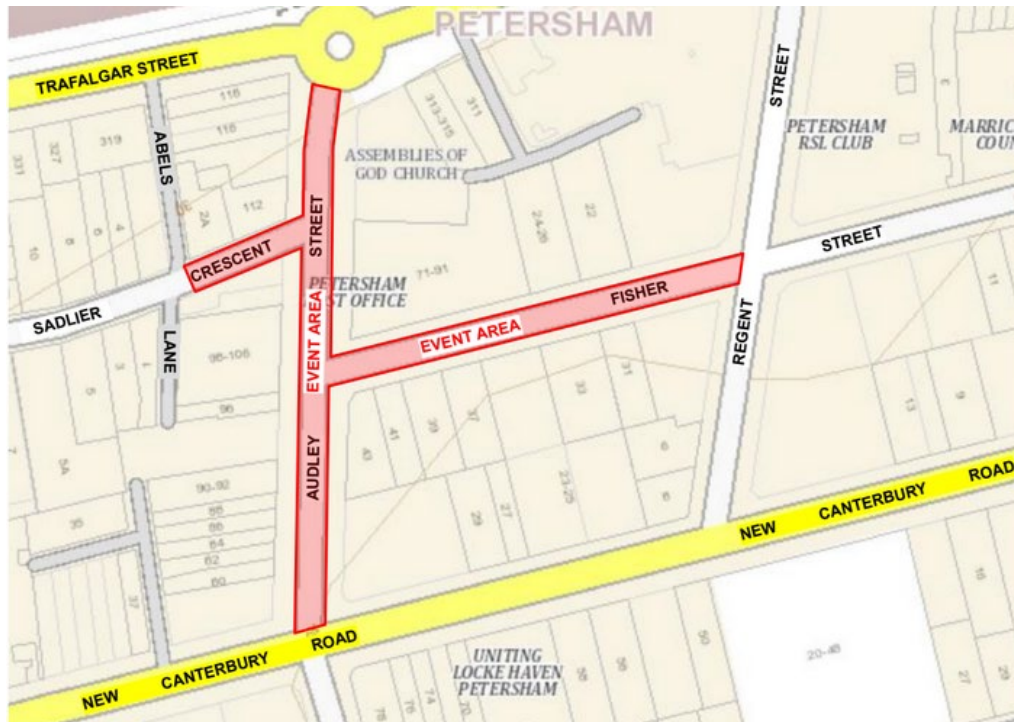
TfNSW Contact: TBC
TfNSW Phone: TBC
TfNSW Email: TBC

Traffic Control Provider: CATO Location Services
Traffic Control Contact: Anna Rosa
Traffic Control Phone: 0466 388 421
Traffic Control Email: Anna@catolocationervices.com.au



2. EVENT LOCATION

Petersham festival will be held on the following roads, Audley St, between Trafalgar St and New Canterbury Rd / Fisher St, between Audley St and Regent St / Sadler Cr, between Audley St and Abels Ln.



Petersham Festival event location (in Red)

2.1. EVENT SITE MAP

A detailed event site plan is being prepared and will be updated closer to the event within the Event Management Plan. Refer to the Event Organiser in section 1.4 of this document for the latest site plan.



3. WORKPLACE HEALTH & SAFETY

3.1 RISK ASSESSMENT PLANS

A Risk Management approach is a fundamental part of the planning for any event. The safety risk identification, assessment and control processes are legal obligations (as per the WHS Act and Regulation 2011) and should be aligned with AS/NZS ISO 31000 Risk Management – Principles and Guidelines. Broader event risk management is best practice and a fundamental part of due diligence.

Inner West Council has compiled Risk Assessments and site-specific safety plans for the events that are not included in this Transport Management Plan.

This section of the Transport Management Plan describes the possible issues/risks that may interfere with the event and the action to be taken to minimise the disturbance of the event.

Issues / Risks	Applicable	Action Taken
All one-way streets are as described	YES	Road closures, barricade and signage installed. Point duty by NSW Police and or authorised Traffic Controllers.
Block access to local businesses	YES	Confirm list of letters to residents, businesses, and car parks. Advertisement of event to general public.
Block Police vehicle access	YES	Confirm access and consultation of routes to and within areas affected by closures with Emergency Services.
Block Ambulance access	YES	Confirm access and consultation of routes to and within areas affected by closures with Emergency Services.
Block fire station access	YES	Normal access to fire station facilities are maintained. Confirm access and consultation of routes to and within areas affected by closures with Emergency Services.
Block heavy vehicle access	YES	All heavy vehicles are diverted before the closure.
Restricted movements banned turns, heavy/high vehicles	YES	All vehicles are diverted before the closure.
Block Public facility (football oval, car park etc.)	YES	Confirm list of letters to residents, businesses, and car parks. Advertisement of event to general public.
Block public transport access	YES	Buses diverted around closure.
Can route use alternatives such as bicycle tracks, paths, parks, bush tracks etc.?	NO	None required
Construction – existing, proposed that may conflict	NO	None required
Numbers of lanes and their width are as described	YES	None required



Road signage existing/temporary	YES	None required Temporary signage Installed and removed by CATO.
Route impeded by traffic calming devices?	NO	None required
Signalised intersections (flashing yellow? Point duty?)	NO	As required by NSW Police
Tidal flows	NO	None required
Traffic generators shopping centres, schools, churches, industrial area, hospitals	YES	Advertisement of event to general public.
Traffic movement contrary to any Notice	YES	Under the direction of traffic controllers
Traffic signals are as described	YES	Controlled by TMC if required
Turning lanes are as described	YES	None required
Letter Drop Zone Maps to indicate precincts mailed	YES	Notification to be arranged by Inner West Council
Heavy Weather	YES	Heavy weather may cause crowds to depart early or organiser consider delaying/cancelling the event
Flood hazard in event area	YES	Event organiser, TMC/TfNSW and Police provide diversions around flooded area.
Flood hazard at the parking area	NO	None required
Parking during Wet weather	YES	Local Car parks only.
Bush fire hazard	NO	For major local/regional bushfire hazard affecting general public health or transport to greater Sydney, take direction from NSW Police
Accident on surrounding roads	YES	If CCTV, monitored by TMC. Facilitate emergency response to area.
Breakdown on surrounding roads	YES	If CCTV, monitored by TMC. Facilitate emergency response to area.
Absence of marshals and volunteers	YES	Re-deploy existing staff as required.
Block public transport access	YES	Divert general public to next available transport, considering safety and circumstances. Relevant transport agency to employ appropriate steps to accommodate.
Delayed Event	YES	At the discretion of the event organiser
Cancellation of Event	YES	Cancellation of any aspect of the event will be communicated by the event organiser.
Security of participants/general public	NO	Provided by event organiser.



3.2 PUBLIC LIABILITY INSURANCE

Inner West Council has Public Liability Insurance to the value of \$20,000,000. This policy covers all activities taking place as part of Petersham Festival. A copy of the current policy is contained in this document.

3.3 NSW POLICE FORCE

Inner West Police Area Command (PAC) will be involved in the planning of The Petersham Festival via Council's Traffic Committee including aspects relating to use of the roadway, closure of selected roads and hostile vehicle mitigation if applicable. They will be formally notified at least two weeks prior to the event taking place. User Pays Police may be engaged by the event organisers based on guidance from PAC.

3.4 NSW FIRE & RESCUE AND NSW AMBULANCE

NSW Fire & Rescue and NSW Ambulance will be notified at least two weeks prior to the event taking place.

3.5 EVENT DELAYS, POSTPONEMENT OR CANCELLATION

Any decision to delay, postpone or cancel the event due to weather impacts or any other reason will be made by the Event Organiser and follow their protocols for emergency management. Once any decision is made in this regard it will be communicated to all relevant stakeholders as per the event's Emergency Management Plan.



4. TRAFFIC AND TRANSPORT MANAGEMENT

4.1 EVENT IMPACT ON ROAD NETWORK

Every effort has been made to minimise the disruption to road users, residents and businesses by implementing a local traffic detour. Access is maintained outside of the event area throughout the event.

4.2 DETOURS

Access around the Audley St and Fisher St closures will be via appropriate detours. The two (2) detours in place will be as follows:

- + Northern detour – New Canterbury Rd > Regent St > Trafalgar St
- + Southern detour – Trafalgar St > Gordon St > New Canterbury Rd

Traffic Guidance Systems (TGSs) will be implemented for the closures and detours noted above. Refer to the attachments to this TMP for copies.

4.3 SPECIAL EVENT CLEARWAYS

The need for a special event clearway has been considered irrelevant due to the event's scale and duration. The event organisers have engaged Cato Location Services to barricade all parking spaces within the event area the night before the event commences.

The following streets will be barricaded to accommodate a pedestrian friendly event area.

Name	Cross Streets	Side
Audley St	Between Trafalgar St and new Canterbury Rd	Both Sides

There is limited street parking available for participants of the event, the event organiser will promote public transport as the best way of getting to the event due to its proximity to regular bus and train services.

For more information: <https://transportnsw.info/trip#/trip>



4.4 VEHICLE ACCESS TO THE EVENT SITE

To ensure public safety, all vehicle access will be at the discretion of the Event Manager and will be strictly limited to walking pace only during bump in and bump out times. Limited access to the event site and road closure will be permitted outside of event hours only.

Once any HVM Measures are installed and the event begins, no vehicle access is permitted, apart from emergency vehicles under the guidance of Event Management and/or traffic controllers.

4.5 CHANGES TO CYCLE ROUTES

The closure of Audley St and Fisher St will not significantly affect designated cycling routes through Petersham. Cyclists will still be able to dismount and walk their bikes through the event site. All existing cycle routes will remain in place and operational around the event site.



4.6 IMPACTS ON PUBLIC TRANSPORT

Local buses will operate on a weekend timetable and will therefore be reduced on the event day in comparison to a weekday. Bus route 445 will be detoured around the event area via 2 detours, 1 detour for eastbound buses and 1 detour for westbound buses as seen below.

- + Eastbound detour – New Canterbury Rd > Gordon St > Trafalgar St
- + Westbound detour – Trafalgar St > Gordon St > New Canterbury Rd

The stops listed below will be affected during the proposed event times, Notices will be put up at impacted bus stops to direct passengers to the nearest stop.

Bus stops impacted – Eastbound, 204937
– Westbound 204938 & 204946

4.6.1. TRAIN SERVICES

It is anticipated that there will be minimal impact on Sydney Trains services and Petersham Station. The existing network provides sufficient existing services for any increased load on the event day.

4.6.2. TAXIS AND RIDE-SHARE PROVIDERS

Taxis and ride-share providers will continue to have access to the road network around the closures as per other road users.

4.7 HOSTILE VEHICLE MITIGATION

The closure of the Petersham Festival is designed to provide a pedestrian-friendly area for the event to operate in and for pedestrians to participate.

The Event Organiser will, in conjunction with the nominated Security Advisor, produce a Hostile Vehicle Mitigation, and Target Hardening Plan if required for the event.

When the closure is installed, applicable hostile vehicle mitigation (HVM) vehicles or barriers may be placed at each entry point as noted on the HVM Risk Assessment. This strategy is designed to prevent access to the site by unauthorised or errant vehicles. Should heavy vehicles be used at the applicable closure points, a driver for each vehicle will always be present in case the vehicle needs to be moved to allow access for emergency vehicles.

Once the applicable HVM strategy is installed, the nominated Security Advisor will be on site if required to authorise the implementation, the positioning and suitability of all devices.

Authorisation is to be granted from the Event Manager for any HVM heavy vehicles to be moved once in position for access when required.



4.8 RE-OPENING ROADS AFTER THE EVENT

The road closure on Audley St and Fisher St is planned to re-open at 0200, however this may occur earlier if the road is clear, and it is safe to do so.

4.9 TRAFFIC CONTROL

The implementation of the traffic guidance schemes, including road closures, will be supervised by the accredited traffic manager.

Temporary traffic control equipment, barricades, and signage must be placed in accordance with the Traffic Guidance Schemes by qualified traffic controllers who possess a TfNSW execute traffic guidance schemes certification.

Other qualifications that are required by the authorised traffic controllers include (but not limited to);

- + General Construction Inductions (also known as “White Card”)
- + Traffic Controller Licence (also known as “Blue Card”)
- + Implement Traffic Control Licence (also known as “Yellow Card”)
- + Prepare Work Zone TMP Licence (also known as “Orange Card”)



5. MINIMISING IMPACT ON THE NON-EVENT COMMUNITY

5.1 ACCESS FOR LOCAL RESIDENTS AND BUSINESSES

The road closure area for Inner West Councils Petersham Festival includes a number of businesses fronting Audley St. Pedestrian access to these businesses and residences will be managed within the closure, however businesses vehicle access will not be permitted within the event area.

All non-local traffic will be redirected around the area via the detour as per the implemented detour plan.

The Event Organiser will notify all residents and businesses impacted by the event including confirmation of the restricted vehicle movements during the event operating times. This will include reviewing any requirements for changes to commercial waste collection times and/or locations.

5.2 ACCESS FOR EMERGENCY VEHICLES

A minimum four metre emergency lane will be maintained along the entire closure. There will be no event infrastructure in the emergency lane. Traffic controllers will be onsite to assist emergency vehicle through the closure points. Whilst any HVM measures are in place, a driver for all vehicles will always be present in case the vehicle needs to be moved to allow access for emergency vehicles.

5.3 ADVERTISING TRAFFIC MANAGEMENT ARRANGEMENTS

The Event Organiser will advertise the road closures via social media and on Council's website. A letterbox drop will be conducted to all resident and businesses in the immediately vicinity of the event sites and road closures as well as the surrounding streets in Petersham.

5.4 EVENT PROMOTION

The Event Organiser will promote Petersham Festival and the road closures taking place using a variety of methods in the weeks preceding the event including:

- + Council's social media platforms,
- + Council's website,
- + Local signage where available,
- + Flyers,
- + VMS Boards and,
- + Local News Paper



5.5 VARIABLE MESSAGE SIGNS

Two (2) VMS will be installed around the event site to inform road users of the changed traffic conditions one (1) week before the event, and also on the event date. The locations and messages are as follows:

Location 1	Messages	
	FRI 14/03/25 til 0500 – SAT 22/03/25	0500 – SAT 22/03/25 til 0200 – SUN 23/03/25
TBC (facing southbound traffic)	Petersham Festival SAT 22 MAR	AUDLEY ST AND FISHER ST CLOSED TIL 2 AM
	AUDLEY ST AND FISHER ST CLOSED	FOLLOW DETOUR SIGNS
		EXPECT DELAYS

Location 2	Messages	
	FRI 14/03/25 til 0500 – SAT 22/03/25	0500 – SAT 22/03/25 til 0200 – SUN 23/03/25
TBC (facing northbound traffic)	Petersham Festival SAT 22 MAR	AUDLEY ST AND FISHER ST CLOSED TIL 2 AM
	AUDLEY ST AND FISHER ST CLOSED	FOLLOW DETOUR SIGNS
		EXPECT DELAYS



6. APPROVALS

6.1 EVENT ORGANISER APPROVAL

TMP Approved by:

.....
(Name)

.....
(Signature) (Date)

6.2 AUTHORISATION TO REGULATE TRAFFIC

Council's traffic management requirements have been met. Regulation of traffic is therefore authorised for all non-classified roads described in the risk management plans and this TMP.

Regulation of Traffic Authorised by:

.....
(Council)

.....
(Name)

.....
(Signature) (Date)



The Transport for New South Wales (TfNSW) traffic management requirements have been met. Regulation of traffic is therefore authorised for all classified roads described in the risk management plans and this TMP.

Regulation of Traffic Authorised by:

.....
(TfNSW)

.....
(Name)

.....
(Signature) (Date)



6.3 PUBLIC LIABILITY INSURANCE

TO BE PROVIDED BY COUNCIL



7. ATTACHMENTS

7.1 TRAFFIC GUIDANCE SYSTEMS

A TGS has been provided on the following pages showing:

- + The traffic management measures in place to facilitate the road closures
- + The detours in place around the road closures during the event



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MANIFEST	
CATO TC	X 5
CATO SUPERVISOR	X 1
STOP/SLOW BAT	X 5
CHANGED TRAFFIC CONDITIONS	X 6
SIDE ROAD CLOSED	X 7
ROAD CLOSED AHEAD	X 5
NO LEFT TURN	X 2
NO RIGHT TURN	X 1
ROAD CLOSED	X 13
ROAD CLOSED	X 4
T	X 1
ROADWORK AHEAD	X 2
END ROADWORK	X 2
60	X 2
40	X 2
40	X 2
CONE	X 50
ROADWORK AHEAD	X 1
WFB	X 14



DRAWN BY: CRAIG HUNTER TCT 0015830
APPROVED BY: KIERAN CATO TCT 0040880

POLICY: Cato Location Services P/L do not accept responsibility of any traffic guidance scheme if the traffic management is not implemented by Cato Location Services P/L.



1. THIS TRAFFIC GUIDANCE SCHEME HAS BEEN DRAWN IN ACCORDANCE WITH AS 1742.3 AND 'TRAFFIC CONTROL AT WORKSITES' MANUAL, V1.1. MODIFICATION MAYBE MADE BY PERSONS HOLDING A RMS 'PWZTMP' QUALIFICATION ONLY.
2. SIGNS & DEVICES ARE TO BE PLACED IN ACCORDANCE WITH THIS TGS.
3. ALL SIGNS AND DEVICES USED MUST COMPLY WITH AUSTRALIA STANDARD AS1742.3.
4. THIS DOCUMENT IS COPYRIGHT / PROPERTY OF CATO LOGISTICS PTY/LTD. THIS DRAWING SHALL ONLY BE USED FOR THE PURPOSE FOR WHICH IT IS INTENDED. UNAUTHORISED USE OF THIS DRAWING IS PROHIBITED.

PROJECT: Bairro Portugues, Petersham Festival
CLIENT: Inner West Council
LOCATION: Audley Street, Petersham
CONTACT: Jen Cramer
PHONE: 02 9392 5598
ORIGINAL SIZE: A4 LANDSCAPE
COUNCIL: Inner West Council

APPROVAL REQUIRED: LOCAL ☒ STATE ☒ OTHER ☐
EVENT TIMES: 1500 - 2100
SET UP: 1hr
CLOSURE IN: 0600 22/03
CLOSURE OUT: 0200 23/03
PACK DOWN: 1hr

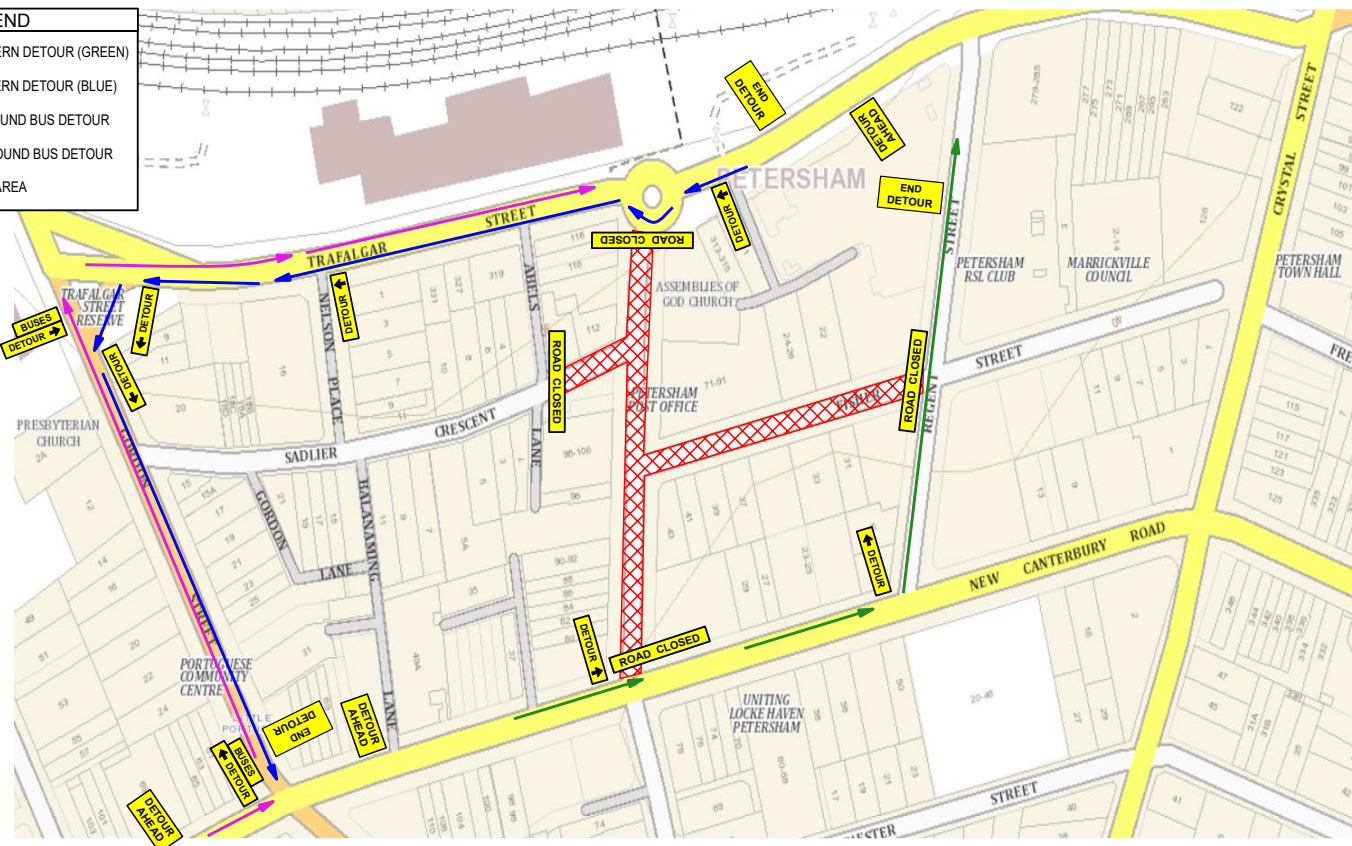
PLAN No: PFBP-1
VARIANT: (A) TYPE: TGS REV: 1.0
EVENT DATE: 22/03/2024
DATE: 01/11/2024

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LEGEND	
	NORTHERN DETOUR (GREEN)
	SOUTHERN DETOUR (BLUE)
	EASTBOUND BUS DETOUR
	WESTBOUND BUS DETOUR
	EVENT AREA

MANIFEST	
	X 3
	X 3
	X 3
	X 4
	X 1



DRAWN BY:	CRAIG HUNTER	TCT 0015830
APPROVED BY:	KIERAN CATO	TCT 0040880

POLICY: Cato Location Services P/L do not accept responsibility of any traffic guidance scheme if the traffic management is not implemented by Cato Location Services P/L.



- ☐ CLOSURE ☐ RMS ☐ ROLLING BLOCK ☐ JUNE MERGE
☐ DETOUR ☐ PEDESTRIAN ☐ BARRICADEING ☐ BUS PLAN
☐ TMP ☐ CONTRA FLOW ☐ PORTABLE SIGNS ☐ SIGNAGE PLAN
1. THIS TRAFFIC GUIDANCE SCHEME HAS BEEN DRAWN IN ACCORDANCE WITH AS 1742.3 AND "TRAFFIC CONTROL AT WORKSITES" MANUAL, V1.1.
2. SIGNS & DEVICES ARE TO BE PLACED IN ACCORDANCE WITH THIS TGS. MODIFICATION MAYBE MADE BY PERSONS HOLDING A RMS "PWZTMP" QUALIFICATION ONLY.
3. ALL SIGNS AND DEVICES USED MUST COMPLY WITH AUSTRALIA STANDARD AS1742.3.
4. THIS DOCUMENT IS COPYRIGHT / PROPERTY OF CATO LOGISTICS PTY LTD. THIS DRAWING SHALL ONLY BE USED FOR THE PURPOSE FOR WHICH IT IS INTENDED. UNAUTHORISED USE OF THIS DRAWING IS PROHIBITED.

PROJECT: Bairro Portugues, Petersham Festival			APPROVAL REQUIRED:		EVENT TIMES: 1500 - 2100		PLAN No: PFBP-2		
CLIENT: Inner West Council			LOCAL <input checked="" type="checkbox"/>		SET UP: 1hr		VARIANT: TYPE: REV:		
LOCATION: Audley Street, Petersham			STATE <input checked="" type="checkbox"/>		CLOSURE IN: 0600 22/03		(A) TGS 0.0		
CONTACT: Jen Cramer			PHONE: 02 9392 5598		CLOSURE OUT: 0200 23/03		EVENT DATE: 22/03/2025		
ORIGINAL SIZE: A4 LANDSCAPE			COUNCIL: Inner West Council		OTHER: <input type="checkbox"/> PACK DOWN: 1hr		DATE: 01/11/2024		

Item No: LTC1124(1) Item 14
Subject: BRUCE STREET, ASHFIELD - PROPOSED 'SHARED PATH'
 (DJARRAWUNANG-ASHFIELD WARD/SUMMER HILL
 ELECTORATE/BURWOOD PAC)
Prepared By: James Nguyen - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the concept plan for the 'shared path' along the existing footpath between Bruce Street and Hume Highway, Ashfield (as detailed in *Attachment 1*) be approved.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

This report identifies bicycle improvements at an existing missing link on Bruce Street adjacent to the on-road bicycle path along Grosvenor Crescent and Elizabeth Street, Ashfield. The proposed 'shared path' will improve bicycle connectivity to the existing on-road bicycle path.

BACKGROUND

Council has received a request from Inner West Bicycle Coalition to convert the existing footpath on the eastern side of Bruce Street, between the cul-de-sac and Hume Highway to a 'shared path'.

DISCUSSION

Bruce Street intersects with an existing on-road bicycle path on Elizabeth Street and Grosvenor Crescent, Ashfield. Currently, there is no formal facility that legally allows cyclists to enter and exit the on-road bicycle path to access Bruce Street. The existing footpath which connects Bruce Street at the cul-de-sac to the junction at Hume Highway, Grosvenor Crescent and Elizabeth Street is not demarcated as a 'shared path'. The current provisions are shown in Figure 1 below:



Figure 1 - Existing provisions

Accordingly, it is proposed to convert this footpath to a 'shared path' to allow cyclists to access Bruce Street from the adjacent on-road bicycle path at Elizabeth Street and Grosvenor Crescent.

The proposal will consist of the following and is shown in *Attachment 1*:

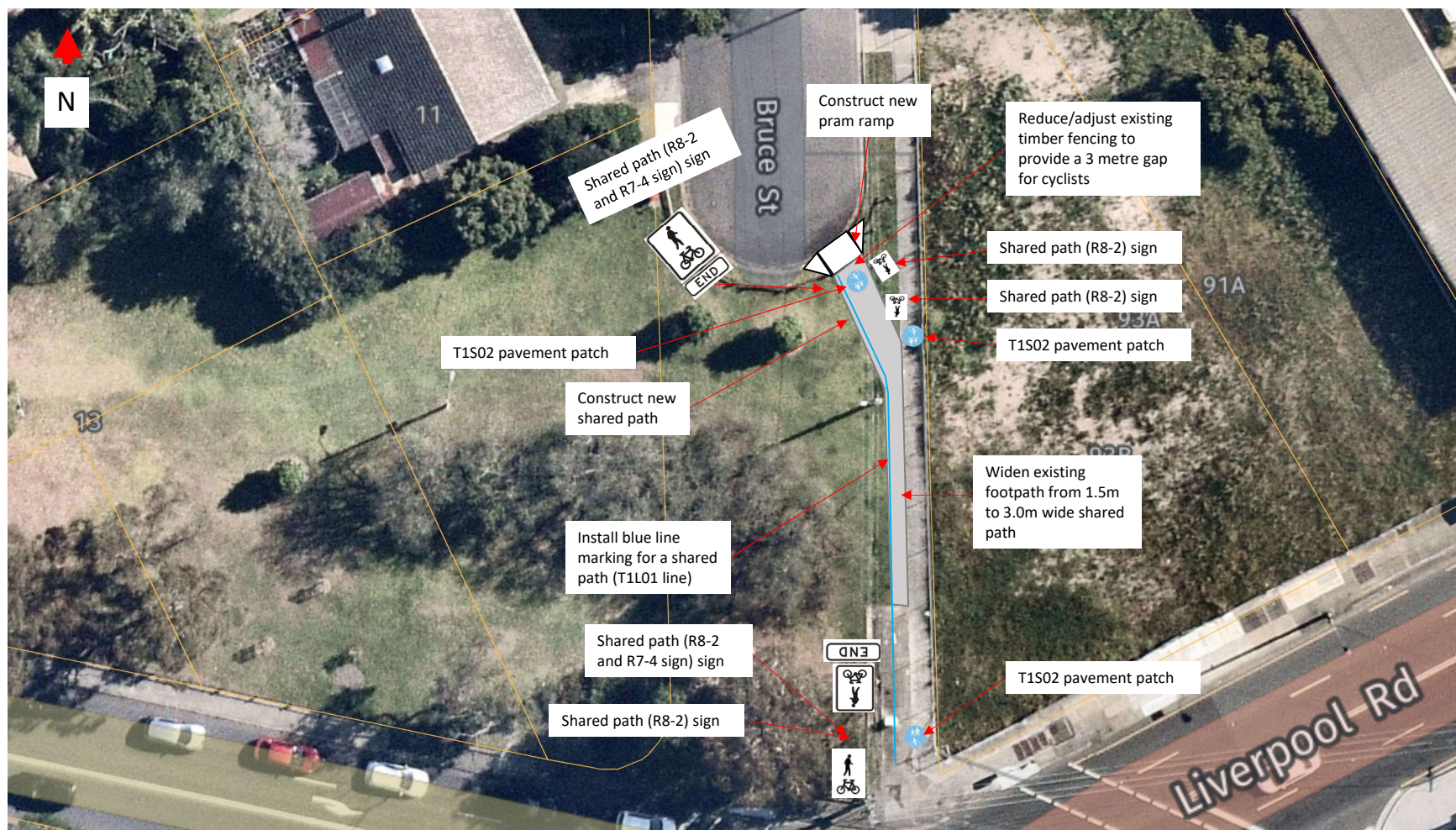
- Install a new kerb ramp at the cul-de-sac of Bruce Street;
- Widen a portion of the existing footpath from 1.5 meters to 3.0 meters and install a new 3.0 metre wide concrete path to connect to the kerb ramp;
- Install shared path line marking (T1L01 line);
- Install shared path pavement patches (T1S02 patch); and
- Install R8-2 and R7-4 signs.

FINANCIAL IMPLICATIONS

The project is estimated to cost \$20,000 and will be funded under Council's capital (cycleway) works program.

ATTACHMENTS

1. [Download](#) Concept plan



Item No: LTC1124(1) Item 15

Subject: LACKEY STREET & SMITH STREET (BETWEEN MOONBIE STREET AND NOWRANIE STREET), SUMMER HILL-ENRC/2024/0062
TEMPORARY FULL ROAD CLOSURE- 'SUMMER HILL SOCIAL' EVENT ON SUNDAY 9 MARCH 2025 (DJARRAWUNANG-ASHFIELD WARD/SUMMER HILL ELECTORATE/BURWOOD PAC)

Prepared By: Boris Muha - Traffic Engineer

Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

THAT the proposed temporary full road closure of Lackey Street & Smith Street (between Moonbie Street and Nowranie Street) Summer Hill, be approved for the purpose of holding a 'Summer Hill Social' event on Sunday 9 March 2025 between 4.00am to 11.00 pm as per the submitted TMP and TGS, and subject to the following conditions and all standard Council conditions for a temporary full road closure:

- a) That a Road Occupancy License application be obtained from the Transport Management Centre.
- b) That notice of the proposed event be forwarded to the NSW Police Local Area Commander, Transit Systems, the NSW Fire Brigade and the NSW Ambulance Services.
- c) That all residents and businesses in and around the affected area are to be notified of the temporary road closure in writing by the applicant in advance (at least 7 days prior to the event) with the applicant making reasonable provision for stakeholders.
- d) That a minimum four (4) metre unencumbered passage be available for emergency vehicles through the closed sections.
- e) That the occupation of the road carriageway must not occur until the road has been physically closed.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Inner West Council is presenting a 'Summer Hill Social' on Sunday 9 March 2025 from 11am until 5pm on Lackey and Smith Street, Summer Hill.

This is a free, family friendly long-lunch style event with an array of food and drinks on offer from local cafes and restaurants, captivating performances by local artists, exciting stalls from the Summer Hill businesses and a fun kids' activity area.

The setting up of the site will start from 4am and will be removed 10pm, after the event.

To facilitate the event, areas of Summer Hill, i.e. Lackey Street and Smith Street (between Moonbie Street and Nowranie Street) will be closed between 4am and 11pm Sunday 9 March 2025. Hardie Avenue will only be open for carpark access off Smith Street.

BACKGROUND

Lackey Street and the above section of Smith Street have been closed on numerous occasions in the past for various food and musical events. The temporary full road closure of Lackey Street and Smith Street has been approved through the Traffic Committee in the past, subject to appropriate traffic control, specific conditions as recommended above and other standard conditions as set out by Council.

DISCUSSION

It is expected that up to 5,000 people may attend the event. The event would be categorised under a class 2 (if not a 3) event under the TfNSW Special Events Guide where it does not impact on the major traffic and transport systems and non-event community over a wide area. The event requires the involvement of Local Council and Police.

Adequate traffic control will be provided, with traffic made to detour through the surrounding streets. Hardie Avenue will only be opened to access the Summer Hill carpark. If the carpark is full, vehicles will be directed to park elsewhere. Service vehicle activity in the area will be encouraged to be done outside the times of the event. Refer to the Traffic Guidance System (TGS) and Detour plan as shown on Attachment 1 *and* Traffic Management Plan (TMP) Attachment 2 to explain any further details on the event.

VMS advance advisory boards will be placed on approach ends to Lackey Street in Carlton Crescent, and static advance advisory signs will be placed elsewhere to alert motorists of the closure prior to the event.

The event-coordinator will apply for a Road Occupancy License (ROL). Lackey Street and Smith Streets are not Bus Routes

The proposed temporary full road closure has will be advertised in accordance with the Road Act 1993.

FINANCIAL IMPLICATIONS

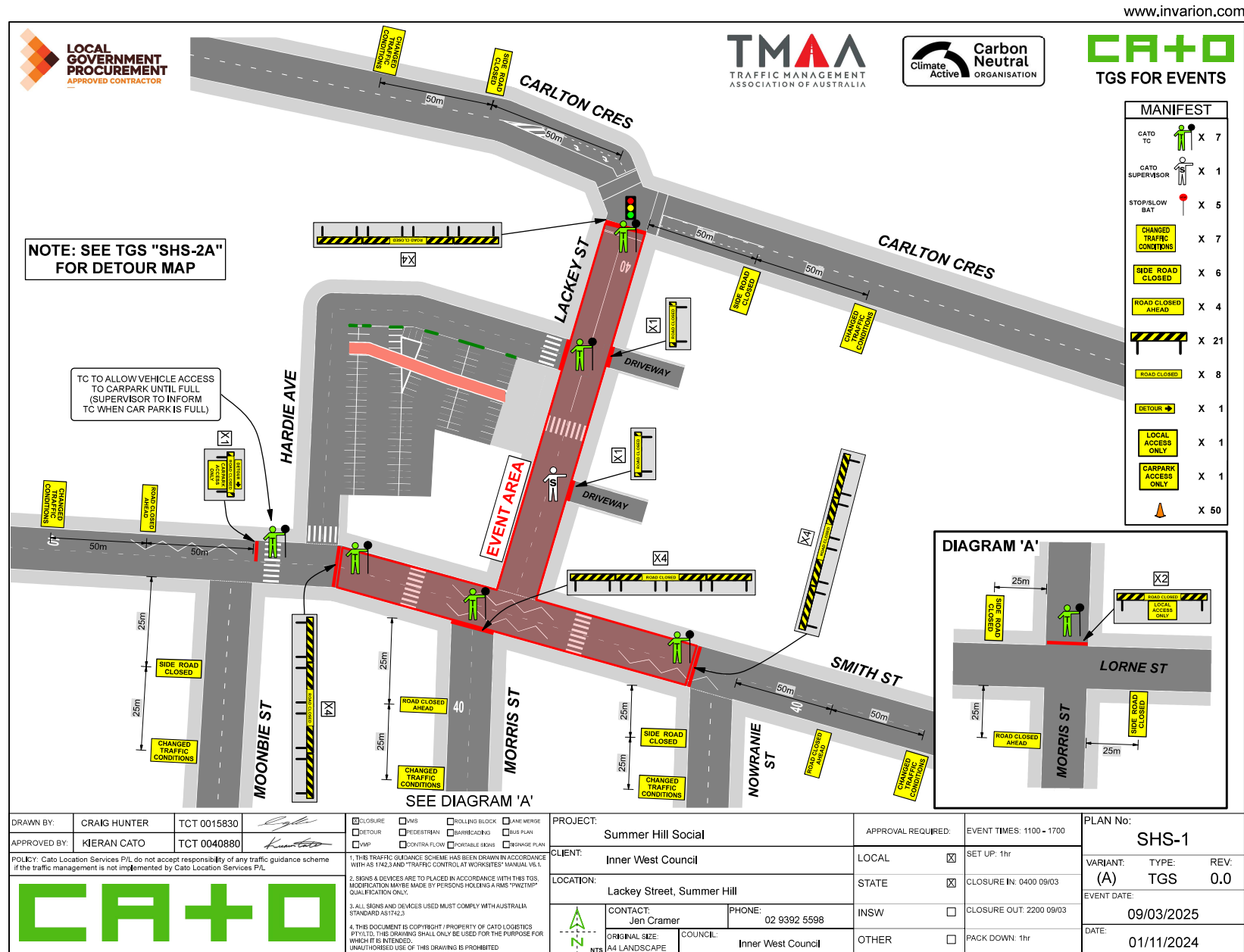
Funding of up to approximately \$72,400 has been allocated by Council for the event under the 2024/2025 Major Community Events Program.

CONCLUSION

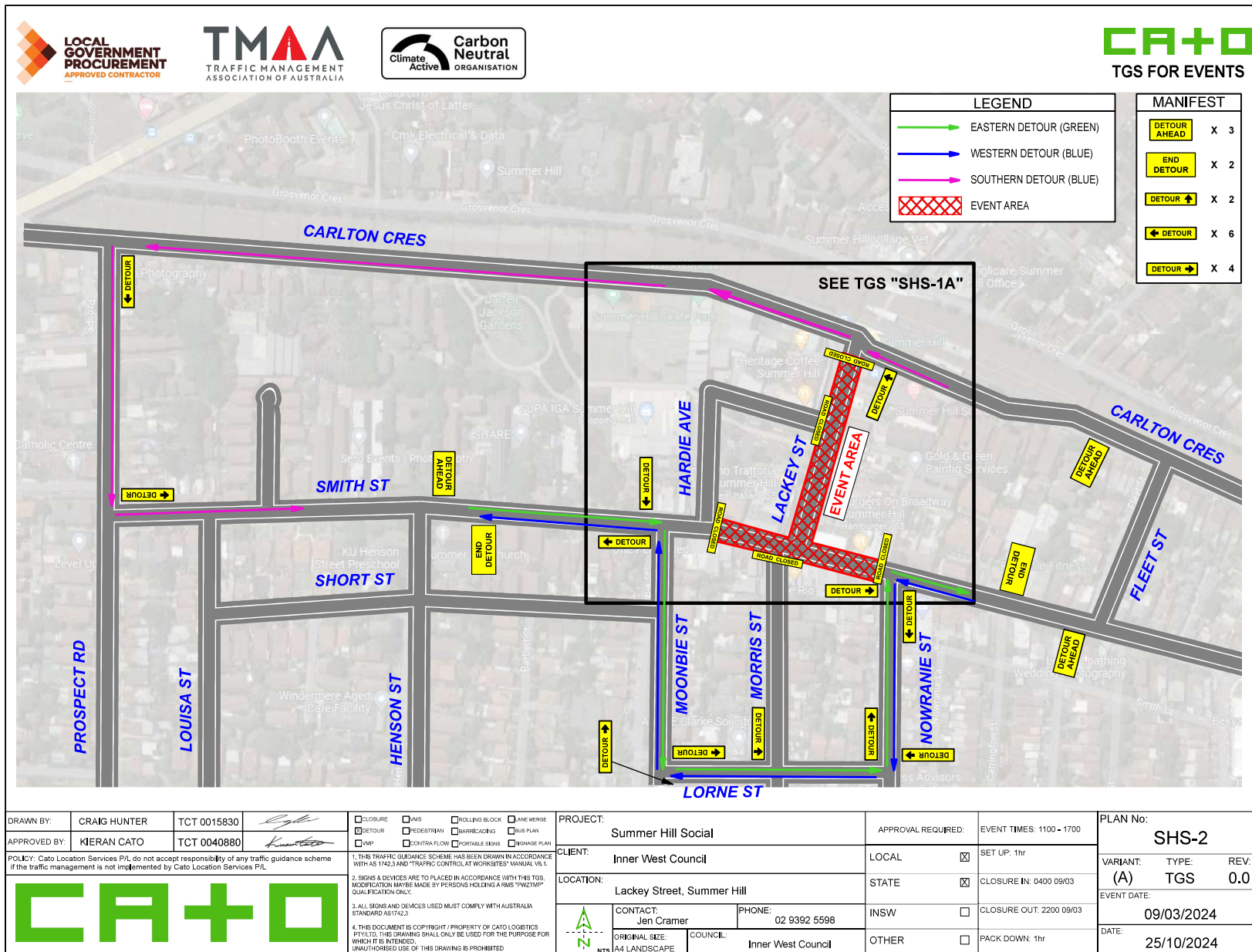
It is recommended that Council agree to the temporary full road closure of Lackey Street and Smith Street (between Moonbie Street and Nowranie Street) Summer Hill, to conduct the 'Summer Hill Social' event between 4am-11pm on Sunday 9 March 2025.

ATTACHMENTS

1. [Traffic Guidance System \(TGS\) - 'Summer Hill Social' 2025.](#)
2. [Traffic Management Plan \(TMP\) - 'Summer Hill Social' 2025](#)



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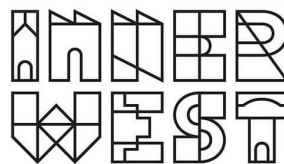


TRAFFIC MANAGEMENT PLAN
SUMMER HILL SOCIAL
LACKEY ST & SMITH ST, SUMMER HILL



Sunday 9th March 2025

PREPARED FOR



by CATO Location Services

TABLE OF CONTENTS

TABLE OF CONTENTS.....	2
STATEMENT OF CONFIDENTIALITY & NON-DISCLOSURE	4
DOCUMENT CONTROL	5
DEFINITIONS	6
REFERENCE DOCUMENTS	6
1. GENERAL EVENT INFORMATION.....	7
1.1. EVENT SUMMARY.....	7
1.2. TRAFFIC IMPACT SUMMARY.....	8
1.3. EVENT DETAILS	8
1.4. KEY EVENT CONTACTS	9
2. EVENT LOCATION	10
2.1. EVENT SITE MAP	10
3. WORKPLACE HEALTH & SAFETY	11
3.1 RISK ASSESSMENT PLANS	11
3.2 PUBLIC LIABILITY INSURANCE	13
3.3 NSW POLICE FORCE	13
3.4 NSW FIRE & RESCUE AND NSW AMBULANCE	13
3.5 EVENT DELAYS, POSTPONEMENT OR CANCELLATION	13
4. TRAFFIC AND TRANSPORT MANAGEMENT	14
4.1 EVENT IMPACT ON ROAD NETWORK.....	14
4.2 DETOURS	14
4.3 SPECIAL EVENT CLEARWAYS.....	14
4.4 VEHICLE ACCESS TO THE EVENT SITE	15
4.5 CHANGES TO CYCLE ROUTES	15
4.6 IMPACTS ON PUBLIC TRANSPORT.....	16
4.6.1. TRAIN SERVICES.....	16
4.6.2. TAXIS AND RIDE-SHARE PROVIDERS	16
4.7 HOSTILE VEHICLE MITIGATION	16
4.8 RE-OPENING ROADS AFTER THE EVENT.....	16
4.9 TRAFFIC CONTROL.....	17
5. MINIMISING IMPACT ON THE NON-EVENT COMMUNITY	18
5.1 ACCESS FOR LOCAL RESIDENTS AND BUSINESSES.....	18



5.2	ACCESS FOR EMERGENCY VEHICLES	18
5.3	ADVERTISING TRAFFIC MANAGEMENT ARRANGEMENTS	18
5.4	EVENT PROMOTION	19
5.5	VARIABLE MESSAGE SIGNS	19
6.	APPROVALS	20
6.1	EVENT ORGANISER APPROVAL	20
6.2	AUTHORISATION TO REGULATE TRAFFIC	20
6.3	PUBLIC LIABILITY INSURANCE	22
7.	ATTACHMENTS	23
7.1	TRAFFIC GUIDANCE SYSTEMS	23



STATEMENT OF CONFIDENTIALITY & NON-DISCLOSURE

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DOCUMENT CONTROL

This document is uncontrolled once printed – the final version with specifications and site diagrams will be locked for printing and restricted by password.

A copy of the final version will be supplied to the event organiser prior to the event.

Version	Prepared by	Date	Comments	Reviewed by
1.0	Katerina Stewart	23/10/2024	Initial draft	Anna Rosa
1.1	Craig Hunter	01/11/2024	Attendance and resident access revised	Anna Rosa



DEFINITIONS

Term	Definition
TMP	Transport Management Plan
TGS	Traffic Guidance Scheme
VMS	Variable Message Sign
HVM	Hostile Vehicle Mitigation
TMC	Transport Management Centre
SMP	Security Management Plan
TfNSW	Transport for NSW

REFERENCE DOCUMENTS

Title	Version
Guide to Traffic and Transport Management for Special Events	v3.5, July 2018
Traffic Control at Worksites Technical Manual	v6.1, February 2022
Workplace Health and Safety ACT NSW	2011
Workplace Health and Safety ACT Amendments NSW	2023
Workplace Health and Safety Regulation NSW	2017
Safe Work NSW website – www.safeworkaustralia.nsw.gov.au	Current website.
Working near Sydney Light Rail – www.transdev.com.au/solutions/work-access-permits/	Current website.
Safe Work Code of Practice – First Aid in the workplace	January 2020
Safe Work Code of Practice – Hazardous Manual Tasks	August 2019
Safe Work Code of Practice – Managing the risks of plant in the workplace	December 2022
Safe Work Code of Practice – How to manage work health and safety risks	August 2019
Safe Work Guideline – Traffic Management: Guide for Events	April 2021



1. GENERAL EVENT INFORMATION

1.1. EVENT SUMMARY

Inner West Council will be holding “Summer Hill Social” at the following locations -

- + Smith St & Lackey St, Summer Hill

Summer Hill Social is an exciting opportunity for residents and visitors alike to come together and celebrate the unique community culture of Summer Hill. This vibrant event offers a day filled with an array of fine and delicious food from local vendors, showcasing the diverse culinary talents of the area. Attendees can indulge in mouthwatering dishes that reflect the flavours of the community, from gourmet street food to artisanal treats.

This family friendly event is non-ticketed, free to all guests. The event will commence at 1100 and conclude at 1700 on Sunday 9th of March.

For this particular location, Inner West Council, in conjunction with CATO Location Services will close a portion of Smith St & Lackey St, Summer Hill.

Inner West Council and Cato Location Services acknowledges the traditional Owners of the Gadigal and Wangal peoples of the Eora Nation. We pay our respect to their Elders past and present.



1.2. TRAFFIC IMPACT SUMMARY

Inner West Councils Summer Hill Social involves a partial road closure to vehicles along Smith St & Lackey St, Summer Hill. To support the event area the following location is to be closed with formal road closures, also known as a “hard road closure”:

- + Smith Street & Lackey St

To further support the event area and surrounding traffic movements, locals within the event area will have access to their residency via surrounding streets and a detour around the road closure will be implemented.

Pedestrian access will be maintained along all existing footpaths and crossing points along streets surrounding the event area. Pedestrians will have full access within the event area. To further manage any pedestrian impacts due to the event please refer to the Security Management Plan by contacting the Event Organiser as listed in Section 1.4 of this document.

1.3. EVENT DETAILS

Event Date:	Sunday 09 March 2025
Event Times:	1100 to 1700
Event Venue:	Smith St & lackey St, Summer Hill
Expected Attendance:	Approximately 5,000 people
Target Market:	All demographics
Traffic Closure Start:	Sunday 09 March at 0400
Bump-in Starts:	Sunday 09 March at 0400
Bump-out Finishes:	Sunday 09 March at 2200
Traffic Closure End:	Sunday 09 March at 2200



1.4. KEY EVENT CONTACTS

Event Organiser: Inner West Council
Event Manager: Jen Cramer
Event Manager Phone: 02 9392 5598
Event Manager Email: jennifer.cramer@innerwest.nsw.gov.au

Venue Owner: Inner West Council
Venue Manager: Inner West Council
Venue Owner Phone: 02 9392 5598
Venue Owner Email: council@innerwest.nsw.gov.au

Police Area Command: Inner West PAC
Police Contact: TBC
Police Phone: TBC
Police Email: TBC

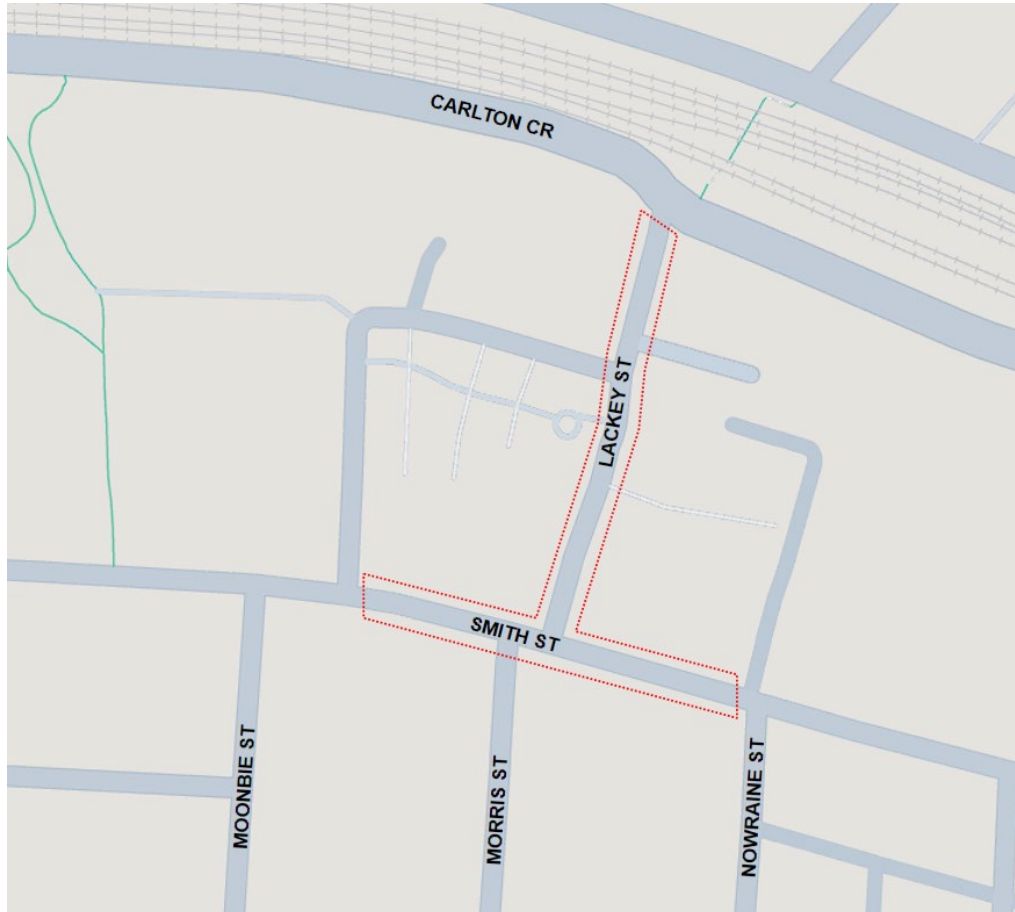
TfNSW Contact: TBC
TfNSW Phone: TBC
TfNSW Email: TBC

Traffic Control Provider: CATO Location Services
Traffic Control Contact: Anna Rosa
Traffic Control Phone: 0466 388 421
Traffic Control Email: Anna@catolocationsservices.com.au



2. EVENT LOCATION

Summer Hill Social will be held on Smith St & Lackey St, Summer Hill.



Summer Hill Social event location (in Red)

2.1. EVENT SITE MAP

A detailed event site plan is being prepared and will be updated closer to the event within the Event Management Plan. Refer to the Event Organiser in section 1.4 of this document for the latest site plan.



3. WORKPLACE HEALTH & SAFETY

3.1 RISK ASSESSMENT PLANS

A Risk Management approach is a fundamental part of the planning for any event. The safety risk identification, assessment and control processes are legal obligations (as per the WHS Act and Regulation 2011) and should be aligned with AS/NZS ISO 31000 Risk Management – Principles and Guidelines. Broader event risk management is best practice and a fundamental part of due diligence.

Inner West Council has compiled Risk Assessments and site-specific safety plans for the events that are not included in this Transport Management Plan.

This section of the Transport Management Plan describes the possible issues/risks that may interfere with the event and the action to be taken to minimise the disturbance of the event.

Issues / Risks	Applicable	Action Taken
All one-way streets are as described	YES	Road closures, barricade and signage installed. Point duty by NSW Police and or authorised Traffic Controllers.
Block access to local businesses	YES	Confirm list of letters to residents, businesses, and car parks. Advertisement of event to general public.
Block Police vehicle access	YES	Confirm access and consultation of routes to and within areas affected by closures with Emergency Services.
Block Ambulance access	YES	Confirm access and consultation of routes to and within areas affected by closures with Emergency Services.
Block fire station access	YES	Normal access to fire station facilities are maintained. Confirm access and consultation of routes to and within areas affected by closures with Emergency Services.
Block heavy vehicle access	YES	All heavy vehicles are diverted before the closure.
Restricted movements banned turns, heavy/high vehicles	YES	All vehicles are diverted before the closure.
Block Public facility (football oval, car park etc.)	YES	Confirm list of letters to residents, businesses, and car parks. Advertisement of event to general public.
Block public transport access	YES	Buses diverted around closure.
Can route use alternatives such as bicycle tracks, paths, parks, bush tracks etc.?	NO	None required
Construction – existing, proposed that may conflict	NO	None required
Numbers of lanes and their width are as described	YES	None required



Road signage existing/temporary	YES	None required Temporary signage Installed and removed by CATO.
Route impeded by traffic calming devices?	NO	None required
Signalised intersections (flashing yellow? Point duty?)	NO	As required by NSW Police
Tidal flows	NO	None required
Traffic generators shopping centres, schools, churches, industrial area, hospitals	YES	Advertisement of event to general public.
Traffic movement contrary to any Notice	YES	Under the direction of traffic controllers
Traffic signals are as described	YES	Controlled by TMC if required
Turning lanes are as described	YES	None required
Letter Drop Zone Maps to indicate precincts mailed	YES	Notification to be arranged by Inner West Council
Heavy Weather	YES	Heavy weather may cause crowds to depart early or organiser consider delaying/cancelling the event
Flood hazard in event area	YES	Event organiser, TMC/TfNSW and Police provide diversions around flooded area.
Flood hazard at the parking area	NO	None required
Parking during Wet weather	YES	Local Car parks only.
Bush fire hazard	NO	For major local/regional bushfire hazard affecting general public health or transport to greater Sydney, take direction from NSW Police
Accident on surrounding roads	YES	If CCTV, monitored by TMC. Facilitate emergency response to area.
Breakdown on surrounding roads	YES	If CCTV, monitored by TMC. Facilitate emergency response to area.
Absence of marshals and volunteers	YES	Re-deploy existing staff as required.
Block public transport access	YES	Divert general public to next available transport, considering safety and circumstances. Relevant transport agency to employ appropriate steps to accommodate.
Delayed Event	YES	At the discretion of the event organiser
Cancellation of Event	YES	Cancellation of any aspect of the event will be communicated by the event organiser.
Security of participants/general public	NO	Provided by event organiser.



Security of very important persons (VIP's)	NO	Provided by event organiser.
--	----	------------------------------

3.2 PUBLIC LIABILITY INSURANCE

Inner West Council has Public Liability Insurance to the value of \$20,000,000. This policy covers all activities taking place as part of Summer Hill Social. A copy of the current policy is contained in this document.

3.3 NSW POLICE FORCE

Inner West Police Area Command (PAC) will be involved in the planning of Summer Hill Social via Council's Traffic Committee including aspects relating to use of the roadway, closure of selected roads and hostile vehicle mitigation if applicable. They will be formally notified at least two weeks prior to the event taking place. User Pays Police may be engaged by the event organisers based on guidance from PAC.

3.4 NSW FIRE & RESCUE AND NSW AMBULANCE

NSW Fire & Rescue and NSW Ambulance will be notified at least two weeks prior to the event taking place.

3.5 EVENT DELAYS, POSTPONEMENT OR CANCELLATION

Any decision to delay, postpone or cancel the event due to weather impacts or any other reason will be made by the Event Organiser and follow their protocols for emergency management. Once any decision is made in this regard it will be communicated to all relevant stakeholders as per the event's Emergency Management Plan.



4. TRAFFIC AND TRANSPORT MANAGEMENT

4.1 EVENT IMPACT ON ROAD NETWORK

Every effort has been made to minimise the disruption to road users, residents and businesses by implementing a local traffic detour. Access is maintained outside of the event area throughout the event.

4.2 DETOURS

Access around the Smith St & Lackey St closure will be via appropriate detours. The two (2) detours in place will be as follows:

- + Eastern detour (Smith St) – Smith St > Moonbie St > Lorne St > Nowrairie St
- + Western detour (Smith St) – Smith St > Nowrairie St > Lorne St > Moonbie St
- + Southern detour (Carlton Cr) – Carlton Cr > Prospect Rd > Smith St

Traffic Guidance Systems (TGSs) will be implemented for the closures and detours noted above. Refer to the attachments to this TMP for copies.

4.3 SPECIAL EVENT CLEARWAYS

The need for a special event clearway has been considered irrelevant due to the event's scale and duration. The event organisers have engaged Cato Location Services to barricade all parking spaces within the event area the night before the event commences.

The following streets will be barricaded to accommodate a pedestrian friendly event area.

Name	Cross Streets	Side
Smith St	Between Hardie Ave and Nowrairie St	Both Sides
Lackey St	Between Carlton Cr and Smith St	Both Sides

There is limited street parking available for participants of the event, the event organiser will promote public transport as the best way of getting to the event due to its proximity to regular bus and train services.

For more information: <https://transportnsw.info/trip#/trip>



4.4 VEHICLE ACCESS TO THE EVENT SITE

To ensure public safety, all vehicle access will be at the discretion of the Event Manager and will be strictly limited to walking pace only during bump in and bump out times. Limited access to the event site and road closure will be permitted outside of event hours only.

Once any HVM Measures are installed and the event begins, no vehicle access is permitted, apart from emergency vehicles under the guidance of Event Management and/or traffic controllers.

4.5 CHANGES TO CYCLE ROUTES

The closure of Lackey St & Smith St will not significantly affect designated cycling routes through Summer Hill. Cyclists will still be able to dismount and walk their bikes through the event site. All existing cycle routes will remain in place and operational around the event site.



4.6 IMPACTS ON PUBLIC TRANSPORT

Local buses will operate on a weekend timetable and will therefore be reduced on the event day in comparison to a weekday. Since buses are not scheduled to run on the proposed event date, the planned event won't have any effect on bus routes.

4.6.1. TRAIN SERVICES

It is anticipated that there will be minimal impact on Sydney Trains services and Summer Hill Station. The existing network provides sufficient existing services for any increased load on the event day.

4.6.2. TAXIS AND RIDE-SHARE PROVIDERS

Taxis and ride-share providers will continue to have access to the road network around the closures as per other road users.

4.7 HOSTILE VEHICLE MITIGATION

The closure of the Summer Hill Social is designed to provide a pedestrian-friendly area for the event to operate in and for pedestrians to participate.

The Event Organiser will, in conjunction with the nominated Security Advisor, produce a Hostile Vehicle Mitigation, and Target Hardening Plan if required for the event.

When the closure is installed, applicable hostile vehicle mitigation (HVM) vehicles or barriers may be placed at each entry point as noted on the HVM Risk Assessment. This strategy is designed to prevent access to the site by unauthorised or errant vehicles. Should heavy vehicles be used at the applicable closure points, a driver for each vehicle will always be present in case the vehicle needs to be moved to allow access for emergency vehicles.

Once the applicable HVM strategy is installed, the nominated Security Advisor will be on site if required to authorise the implementation, the positioning and suitability of all devices.

Authorisation is to be granted from the Event Manager for any HVM heavy vehicles to be moved once in position for access when required.

4.8 RE-OPENING ROADS AFTER THE EVENT

The road closure is planned to re-open at 2200, however this may occur earlier if the road is clear, and it is safe to do so.



4.9 TRAFFIC CONTROL

The implementation of the traffic guidance schemes, including road closures, will be supervised by the accredited traffic manager.

Temporary traffic control equipment, barricades, and signage must be placed in accordance with the Traffic Guidance Schemes by qualified traffic controllers who possess a TfNSW execute traffic guidance schemes certification.

Other qualifications that are required by the authorised traffic controllers include (but not limited to);

- + General Construction Inductions (also known as “White Card”)
- + Traffic Controller Licence (also known as “Blue Card”)
- + Implement Traffic Control Licence (also known as “Yellow Card”)
- + Prepare Work Zone TMP Licence (also known as “Orange Card”)



5. MINIMISING IMPACT ON THE NON-EVENT COMMUNITY

5.1 ACCESS FOR LOCAL RESIDENTS AND BUSINESSES

The road closure area for Inner West Councils Summer Hill Social includes several businesses fronting Lackey St & Smith St. Pedestrian and Vehicle access to these businesses and residences will be managed within the closure, however businesses vehicle access will not be permitted within the event area.

Some 'local residents only' closures (Or "soft closures") will be implemented around Smith St. Vehicle access will be maintained for residents in:

- + Morris St, between Lorne St and Smith St

All non-local traffic will be redirected around the area via the detour as per the implemented detour plan.

The Event Organiser will notify all residents and businesses impacted by the event including confirmation of the restricted vehicle movements during the event operating times. This will include reviewing any requirements for changes to commercial waste collection times and/or locations.

5.2 ACCESS FOR EMERGENCY VEHICLES

A minimum four metre emergency lane will be maintained along the entire closure. There will be no event infrastructure in the emergency lane. Traffic controllers will be onsite to assist emergency vehicle through the closure points. Whilst any HVM measures are in place, a driver for all vehicles will always be present in case the vehicle needs to be moved to allow access for emergency vehicles.

5.3 ADVERTISING TRAFFIC MANAGEMENT ARRANGEMENTS

The Event Organiser will advertise the road closures via social media and on Council's website. A letterbox drop will be conducted to all resident and businesses in the immediately vicinity of the event sites and road closures as well as the surrounding streets in Artarmon.



5.4 EVENT PROMOTION

The Event Organiser will promote Summer Hill Social and the road closures taking place using a variety of methods in the weeks preceding the event including:

- + Council's social media platforms,
- + Council's website,
- + Local signage where available,
- + Flyers,
- + VMS Boards and,
- + Local News Paper

5.5 VARIABLE MESSAGE SIGNS

Two (2) VMS will be installed around the event site to inform road users of the changed traffic conditions one (1) week before the event, and also on the event date. The locations and messages are as follows:

Location 1	Messages	
	MON 30/02/25 til 0500 – SUN 09/03/25	0400 – SUN 09/03/25 til 2200 – SUN 09/03/25
TBC (facing southbound traffic)	Summer Hill Social SAT 09 MAR Smith St & Lackey St Closed	Smith St & Lackey St CLOSED TIL 10 PM FOLLOW DETOUR SIGNS EXPECT DELAYS

Location 2	Messages	
	MON 30/02/25 til 0500 – SUN 09/03/25	0400 – SUN 09/03/25 til 2200 – SUN 09/03/25
TBC (facing northbound traffic)	Summer Hill Social SAT 09 MAR Smith St & Lackey St Closed	Smith St & Lackey St CLOSED TIL 10 PM FOLLOW DETOUR SIGNS EXPECT DELAYS



6. APPROVALS

6.1 EVENT ORGANISER APPROVAL

TMP Approved by:

.....
(Name)

.....
(Signature) (Date)

6.2 AUTHORISATION TO REGULATE TRAFFIC

Council's traffic management requirements have been met. Regulation of traffic is therefore authorised for all non-classified roads described in the risk management plans and this TMP.

Regulation of Traffic Authorised by:

.....
(Council)

.....
(Name)

.....
(Signature) (Date)



The Transport for New South Wales (TfNSW) traffic management requirements have been met. Regulation of traffic is therefore authorised for all classified roads described in the risk management plans and this TMP.

Regulation of Traffic Authorised by:

.....
(TfNSW)

.....
(Name)

.....
(Signature) (Date)



6.3 PUBLIC LIABILITY INSURANCE

TO BE PROVIDED BY COUNCIL



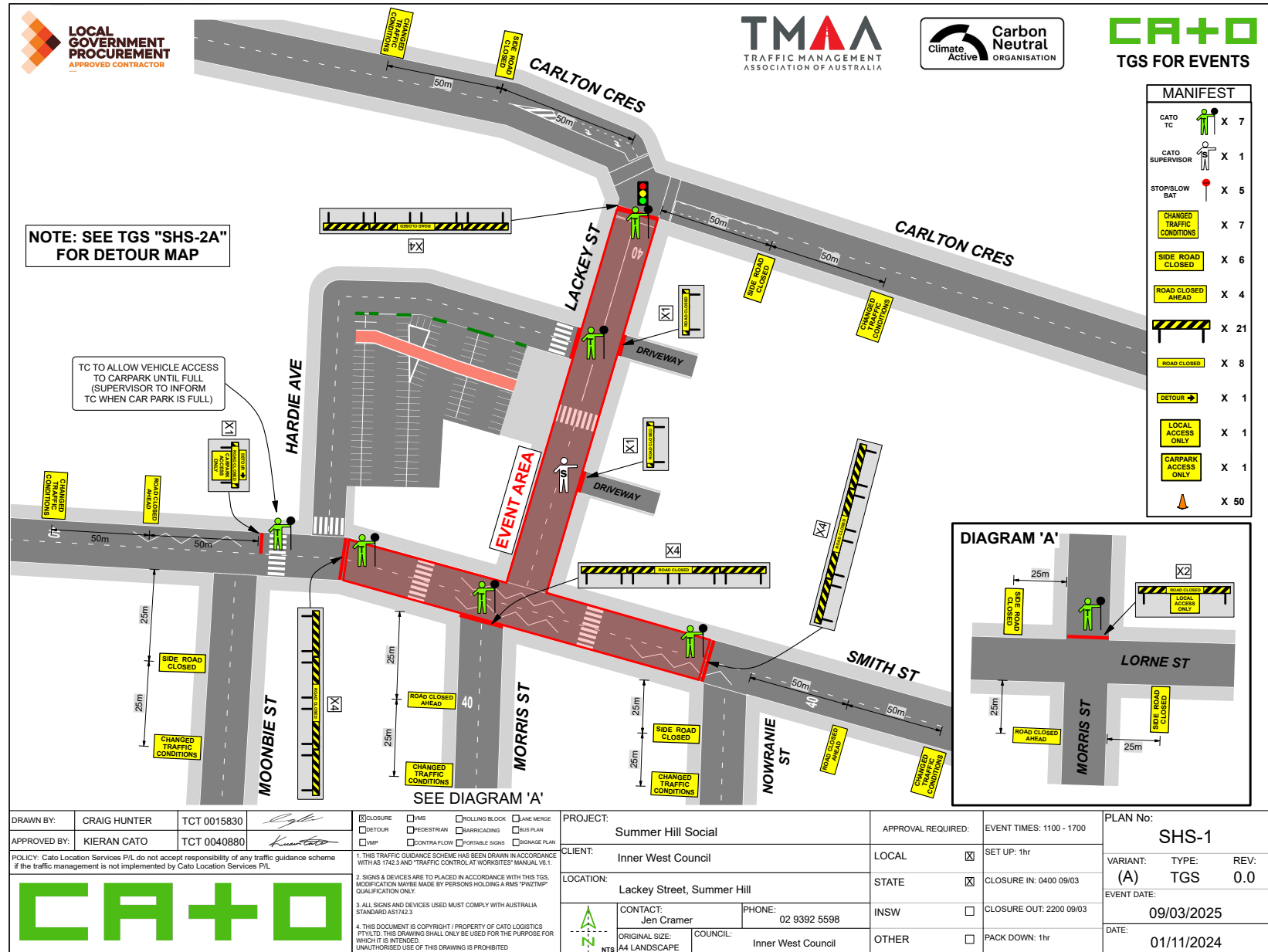
7. ATTACHMENTS

7.1 TRAFFIC GUIDANCE SYSTEMS

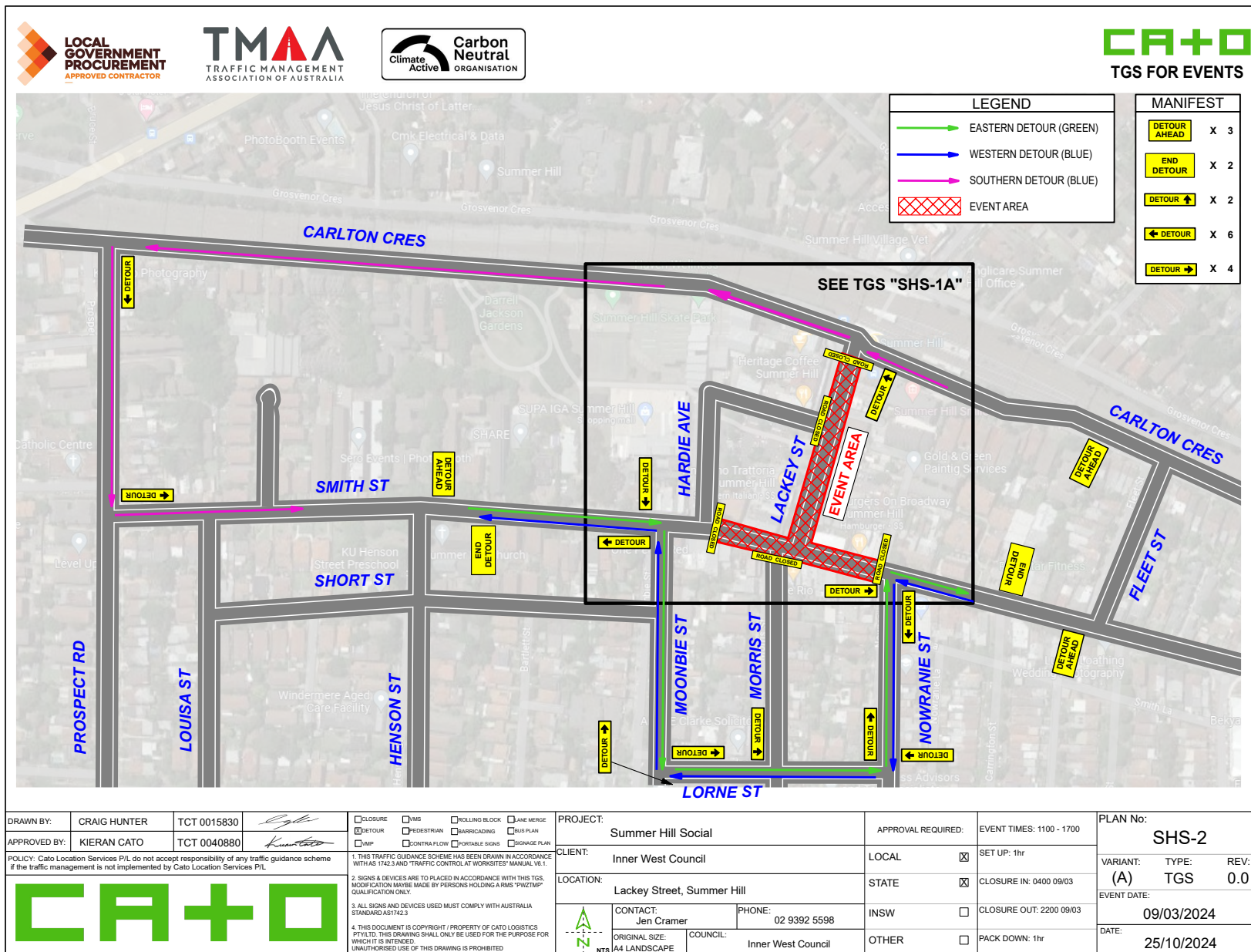
A TGS has been provided on the following pages showing:

- + The traffic management measures in place to facilitate the road closures
- + The detours in place around the road closures during the event





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Item No: LTC1124(1) Item 16
Subject: **MACKEY PARK AND CARRINGTON ROAD SURVEY AREA, MARRICKVILLE - REQUEST FOR EXTENSION OF M2 RESIDENTIAL PARKING SCHEME - RESIDENT PARKING QUESTIONNAIRE SURVEY RESULTS (MIDJUBURI-MARRICKVILLE WARD/SUMMER HILL ELECTORATE/INNER WEST PAC)**
Prepared By: James Nguyen - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

1. That the proposed three-month trial of timed permit parking restrictions on Thornley, Cary, Renwick, Ruby and Junction Streets, and Warren Road not proceed due to the lack of community response and support.
2. That the proposed '2P 3pm-9pm Mon-Fri; 4P 8am-6pm Sat-Sun' restrictions on the western side of Richardson Crescent south of Carrington Road be installed as shown in *Attachment 2*.
3. That the current on-going enforcement of unattended and unauthorised vehicles under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers to improve parking conditions for households that are affected by current parking behaviours on the local streets be noted.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

Following a Notice of Motion being raised, Council resolved to undertake a community consultation for a proposed three-month trial to extend the Resident Permit Parking Scheme Area M2 to the industrial precinct around Carrington Road, Cary and Renwick Streets. This report presents results of the parking surveys and community consultation completed in the following streets:

- Thornley Street, south of Premier Street
- Richardson Crescent, east of Carrington Road
- Carrington Road between Premier and Richardson Crescent
- Cary Street between Carrington Road and Johnston Lane
- Renwick Street between Carrington Road and Johnston Lane
- Warren Road between Carrington Road and Johnston Street
- Ruby Street between Carrington Road and High Street
- Junction Street between Ruby Street and Schwebel Street

The parking surveys completed indicate high utilisation surrounding Mackey Park and the nearby streets perpendicular to Carrington Road, and households with no or limited off-street

parking may be affected by current parking conditions, and any future potential parking restrictions on Mackey Park.

The results from the community consultation indicate a low response rate from residents for a three-month trial of the extension of the M2 permit parking area despite those responding generally in support of the scheme. Given the low response rate, Resident Parking Scheme restrictions are not recommended at this time.

It is recommended to proceed to install timed parking restrictions on the western side of Richardson Crescent, south of Carrington Road to prevent long-term parking adjacent to the park and improve parking opportunities for park users during the weekday evening and weekend daytime periods.

BACKGROUND

A notice of motion was raised by Council to undertake a community consultation for a proposed three-month trial to extend the Resident Permit Parking Scheme Area M2 to the industrial precinct around Carrington Road, Cary and Renwick Streets.

A parking survey was undertaken in the streets surrounding the industrial precinct near Carrington Road and Mackey Park to assess parking conditions and determine if parking restrictions (timed parking and permit parking restrictions) are necessary. The parking survey was completed on Monday 1 July 2024 between 7am and 7pm.

Based on this survey it was proposed that timed parking restrictions signposted as '2P 3pm-9pm Mon-Fri; 6P 8am-6pm Sat-Sun' be considered on Carrington Road and Richardson Crescent, adjacent to Mackey Park, and that timed permit parking restrictions are considered in conjunction at nearby streets to minimise any impact that the timed parking restriction adjacent to Mackey Park may have (36 spaces), particularly to nearby streets that have high levels of parking utilisation, and limited to no off-street parking.

Accordingly, a Resident Parking questionnaire survey proposing '2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2' was undertaken with affected households in:

- Thornley Street, south of Premier Street
- Cary Street between Carrington Road and Johnston Lane
- Renwick Street between Carrington Road and Johnston Lane
- Warren Road between Carrington Road and Johnston Street
- Ruby Street between Carrington Road and High Street
- Junction Street between Ruby Street and Schwebel Street

There were no proposed parking changes on Premier Street given most households have off-street parking, and on-street parking utilisation was low.

Council Policy/Guidelines

Council's adopted Policy for the introduction of a Permit Parking Area states "that before implementing a resident parking scheme in any area, a survey of residents be undertaken to ascertain the level of support for such a scheme and that such support should be in excess of 65% of submissions received provided that rate of return of submissions is reasonable (higher than 30%)".

DISCUSSION

Table 1 below presents a summary of the parking survey results and investigations into determining which streets may benefit from timed permit parking restrictions. It should be

noted that parking utilisation thresholds were in most cases close but did not meet Council's requirements of 85% occupancy. Nevertheless, due to the closeness of the utilisation data to the council thresholds, Council Officers followed up with a survey to determine the level of support for Residential Parking Scheme from the community prior making a final recommendation.

Table 1 - Parking survey results and remarks

Street	Between	Average Parking Occupancy (both sides)	Remarks	Consider RPS extension (Y/N)	Proposed changes
Carrington Road	Premier and Cary Streets	79%	<ul style="list-style-type: none"> Average for both sides less than 85% utilisation threshold Parking utilisation on eastern side adjacent to the park was 88% Timed parking restrictions adjacent to the park may transfer demand (approximately 13 spaces) to adjacent streets 	No ⁺	2P 3pm-9pm Mon-Fri; 6P 8am-6pm Sat-Sun ⁺ <small>⁺ these parking restrictions do not provide exemptions to RPS permit holders.</small>
Richardson Crescent	South of Carrington Road	86%	<ul style="list-style-type: none"> Parking utilisation on southern side adjacent to the park was 91% Timed parking restrictions adjacent to the park may transfer demand (approximately 23) to adjacent streets 	No ⁺	2P 3pm-9pm Mon-Fri; 6P 8am-6pm Sat-Sun ⁺ <small>⁺ these parking restrictions do not provide exemptions to RPS permit holders.</small>
Premier Street	Between Carrington Road and Johnston Lane	68%	<ul style="list-style-type: none"> Average for both sides less than 85% utilisation threshold Most households have off-street parking 	No	No change
Thornley Street	South of Premier Street	82%	<ul style="list-style-type: none"> Average for both sides slightly less than 85% utilisation threshold Some households on eastern side have no off-street parking and transfer of parking demand may affect these households 	Yes	2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2

Street	Between	Average Parking Occupancy (both sides)	Remarks	Consider RPS extension (Y/N)	Proposed changes
Cary Street	Carrington Road and Johnston Lane	78%	<ul style="list-style-type: none"> Average for both sides slightly less than 85% utilisation threshold Some households on both sides have limited or no off-street parking and transfer of parking demand may affect these households 	Yes	2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2
Renwick Street	Carrington Road and Johnston Lane	82%	<ul style="list-style-type: none"> Average for both sides slightly less than 85% utilisation threshold Parking utilisation on northern side was 93% Most households on southern side do not have off-street parking 	Yes	2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2
Warren Road	Carrington Road and Johnston Lane	82%	<ul style="list-style-type: none"> Average for both sides slightly less than 85% utilisation threshold Parking utilisation on southern side adjacent to the park was 84% Most households on both sides do not have off-street parking and transfer of parking demand may affect these households 	Yes	2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2
Ruby Street	Between Carrington Road and Ivanhoe Street	69%	<ul style="list-style-type: none"> Average for both sides less than 85% utilisation threshold Parking utilisation on southern side was 81% and higher than the total average between Carrington Road and Ivanhoe Street 	Yes	2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2

Street	Between	Average Parking Occupancy (both sides)	Remarks	Consider RPS extension (Y/N)	Proposed changes
			<ul style="list-style-type: none">Most households on both sides have limited to no-off street parking and transfer of parking demand may affect these households		
Junction Street	Between Ruby and Schwebel Streets	81%	<ul style="list-style-type: none">Average for both sides slightly less than 85% utilisation thresholdMost households on both sides have limited to no-off street parking and transfer of parking demand may affect these households	Yes	2P 8am-6pm Mon-Fri Permit Holders Excepted Area M2

Council subsequently consulted on the following parking changes shown in Figure 1 below:

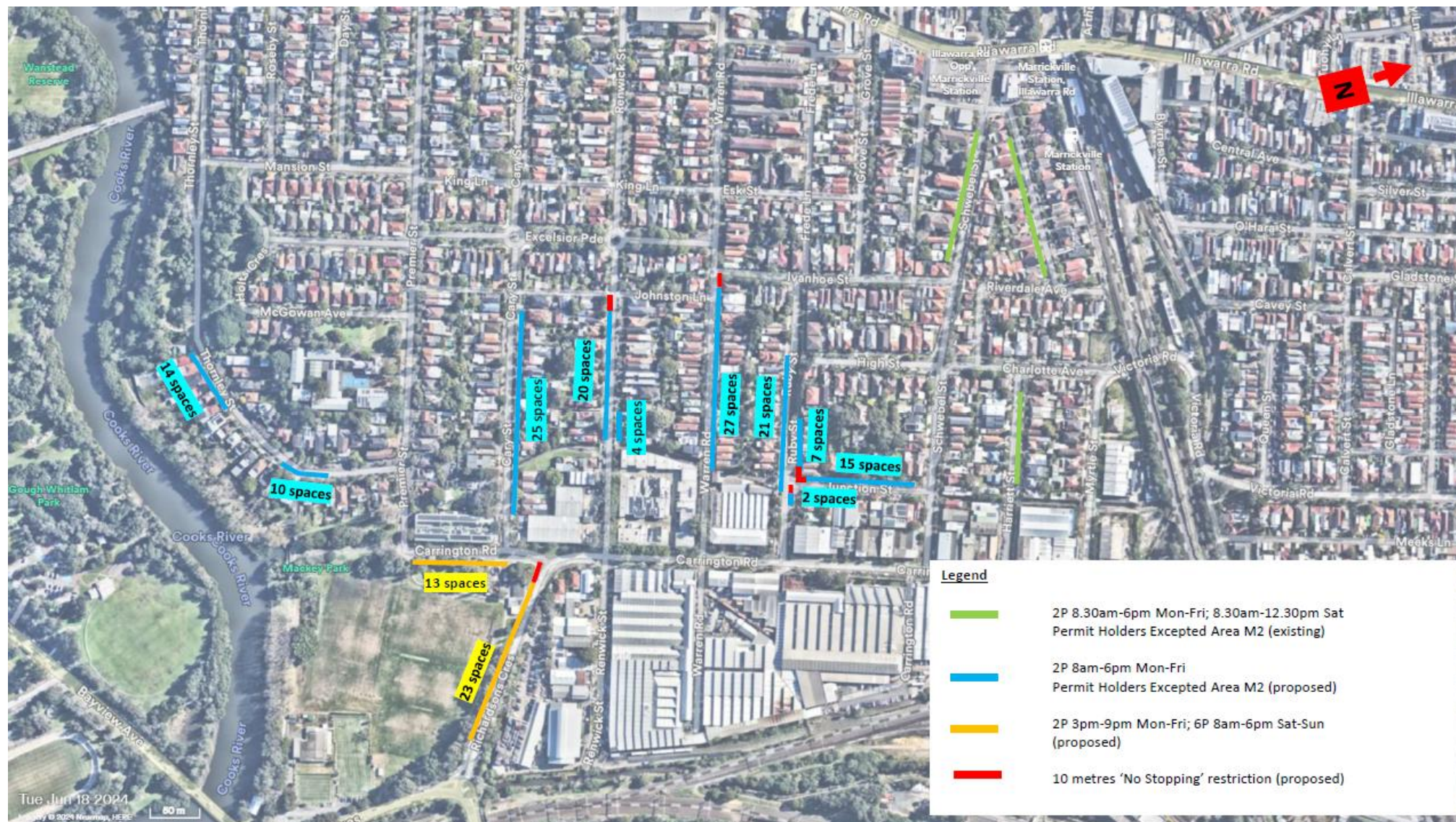


Figure 1 - Proposed parking changes (consultation)

PUBLIC CONSULTATION

A total of 347 consultation letters outlining the proposal were sent out on Monday 22 July 2024. In conjunction, the proposal was also publicly exhibited on Inner West Council's haveyoursay page from Tuesday 23 July 2024 to Monday 19 August 2024.

A total of 66 responses to the proposal were received, of which 48 from Inner West Council's haveyoursay page and 18 were from direct email. The results are summarised in Table 2 below:

Table 2 - Consultation results (summary)

Number of Properties	347
Total Responses	66
Total Support	51
Total Support Rate	77%
Total Oppose	8
Total Oppose Rate	12%
Total – neutral	7
Total – neutral Rate	11%
Overall Response Rate	19%
Overall Support Rate	77%
Overall Oppose rate	12%

Based on Council's Resident Parking Policy, a minimum response rate of 30% of households and a minimum of 65% support from respondents is required in order to for Council to proceed with implementation. The overall minimum response rate did not meet the required threshold, however the support rate exceeded the required 65% threshold as shown in Table 3 below. In the break of street by Street, only Cary Street had a response rate approached that of the minimum response rate required by Council's Policy, however in this case the support from residents was only at 58% which did not meet the support rate as required by Council's Policy of 66%. This effectively meant that essentially only one of the criteria required for implementation of a Residential Parking Scheme has been met.

Table 3 - Consultation results (by street)

	Street						
	Thornley Street	Cary Street	Renwick Street	Warren Road	Ruby Street	Junction Street	Carrington Road
No. of properties	44	47	79	76	31	18	56
No. of responses	6	12	13	14	6	3	4
No. of responses from eligible properties	3	7	7	10	3	2	0
No. of eligible properties	13	20	21	29	16	11	0
No. of	5	7	11	13	4	2	3

support							
No. of	0	5	2	0	1	0	1
object							
No. of	1	0	1	1	1	1	0
neutral							
Overall response rate	13%	26%	16%	18%	19%	17%	7%
Overall support rate	83%	58%	84%	93%	66%	66%	75%
Overall oppose rate	0%	42%	15%	0%	17%	0%	25%

Table 4 below presents the responses received during the consultation period.

In summary, households raised issues about taxi's and businesses parking their vehicles on the street which occupied parking spaces in front or nearby their homes. There were concerns raised about the proposed timed permit parking restrictions being proposed on one side of the streets as this would increase parking pressure or shift the current problems to this side of the street. It should be noted that extending timed permit parking restrictions to both sides of the street would affect households that do not qualify for permit parking, who need to park additional vehicles on the street.

Table 4 - Consultation responses

Street	Theme	Resident comments	Officer response
Thornley Street	Taxi's and businesses	<ul style="list-style-type: none"> On-street parking is occupied by Taxi's and cars belonging to smash repairs Taxi's and staff vehicles of businesses will move to the unrestricted areas. Additional restricted parking would need to be considered on Thornley and Premier Street 	Council's parking services unit are completing on-going enforcement of unattended vehicles and taxis under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers continue to change parking behaviours.
	Resident permit parking	<ul style="list-style-type: none"> Extend the permit parking restrictions on the southern side of Thornley Street to include the full length of Thornley Street, and the southern side of Premier Street between Thornley Street and Carrington Road. 	Permit parking restrictions are not recommended on Thornley Street given the low overall response rate (13%) from community consultation.
Cary Street	Taxi's and businesses	<ul style="list-style-type: none"> Taxi's and vehicles from smash repairs have been parking in the street and affecting parking for residents. The cars are often left unattended for many weeks. 	Council's parking services unit are completing on-going enforcement of unattended vehicles and taxis under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers continue to change parking behaviours.
	Resident permit parking	<ul style="list-style-type: none"> Extend the timed permit parking restrictions to 8pm or 9pm as taxi's will park their vehicles at 4pm and leave them idle overnight Make both sides of Cary Street, timed permit parking; making one side timed permit parking will shift the problem (taxi's, trailers, unattended vehicles, commuters) to the other side. Both sides are impacted by the problem. This will push visitor and excess parking to the surrounding streets and make it worse for other residents 	Permit parking restrictions are not recommended on Cary Street given the low overall response rate (26%) from community consultation.
	Parking	<ul style="list-style-type: none"> Cary Street would benefit from angle parking 	<ul style="list-style-type: none"> Council officers can assess this further subject to a formal

Street	Theme	Resident comments	Officer response
	Pedestrian and cyclist safety	<p>to slow traffic down and create more parking</p> <ul style="list-style-type: none"> The shared path along the western side of Carrington Road, just north of Cary Street is unsafe due to larger trucks parking at the intersection and blocking visibility for pedestrians and cyclists. Please consider a 'No Stopping' restriction at this intersection Please consider a pedestrian crossing across Cary Street at Carrington Road. There is high pedestrian activity due to sporting events and may meet the warrants. It will also provide a good connection to the crossing at Carrington Road, making it more attractive to walk and cycle to Mackey Park. A pedestrian island would benefit at the intersection of Carrington Road to improve safety and slow vehicles down 	<p>request of 10 signatures from households on Cary Street</p> <ul style="list-style-type: none"> Council has approved raising the existing pedestrian crossing on Carrington Road. Currently, there are no plans to install an additional pedestrian crossing across Cary Street. This section of Cary Street is located in the overland flow path and may have drainage issues. Council officers will investigate further a 'No Stopping' sign on the northern side of Cary Street, west of Carrington Road.
Renwick Street	Taxi's and businesses	<ul style="list-style-type: none"> Permit parking restrictions without proper enforcement from rangers will shift parking to adjacent streets and needs proper enforcement Taxi's, smash repair vehicles and other business vehicles are left on Renwick Street for extended periods Taxi's and business vehicles need to be enforced 	Council's parking services unit are completing on-going enforcement of unattended vehicles and taxis under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers continue to change parking behaviours.
	Resident permit	<ul style="list-style-type: none"> The permit parking restrictions should include the whole of Renwick Street, otherwise the 	Permit parking restrictions are not recommended on Renwick Street given the low overall response rate (16%) from community

Street	Theme	Resident comments	Officer response
	parking	<p>issues will be transferred to the unrestricted parking spaces and push traffic further up the street and affect other homes</p> <ul style="list-style-type: none"> Request to extend RPS towards Illawarra Road 	consultation.
	Parking	<ul style="list-style-type: none"> Request for 'No Parking' restrictions in Johnston Lane 	Council officers can investigate this further
Warren Road	Resident permit parking	<ul style="list-style-type: none"> There were some streets that had only one side of the street with restricted parking, but it wasn't clear from the proposal if all residents in the area (including those living opposite restricted areas) would receive permits. I would think this would be an important point to make as it could skew support for the proposal. Need more resident parking permits Permit parking restrictions on one-side of the street will affect the other side which is unrestricted. There are problems with parking on Warren Rod on the southern side with vehicles parking across driveways. The proposal will make it worse on the southern side. 	Permit parking restrictions are not recommended on Warren Road given the low overall response rate (18%) from community consultation.

Street	Theme	Resident comments	Officer response
Warren Road	Taxi's and businesses	<ul style="list-style-type: none"> Businesses and industrial site workers are parking along the street and taking up spaces. Residents are required to park sometimes three (3) streets away. The restrictions to not go far enough. Taxi's will park further uphill and continue to affect those who live in the area. This occurs in the middle of the night at taxi shift changeover when no-one is around. 	Council's parking services unit are completing on-going enforcement of unattended vehicles and taxis under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers continue to change parking behaviours.

Street	Theme	Resident comments	Officer response
Ruby Street	Taxi's and businesses	<ul style="list-style-type: none"> The proposal will not solve the problem of taxi's and cars parking on Richardson Crescent. This is what pushes the cars up onto the adjoining streets. The proposal will help remove taxi's left on the side of the road adjacent to Mackey Park which blocks families and children from accessing the area. Businesses should be considerate with the surrounding community and operate within their property and not take up parking within the community Businesses should provide sufficient parking spaces for their staff to use 	Council's parking services unit are completing on-going enforcement of unattended vehicles and taxis under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers continue to change parking behaviours.

Street	Theme	Resident comments	Officer response
Ruby Street	Resident permit parking	<ul style="list-style-type: none"> Ruby Street should have 8am-5pm not 6pm and the time period should be 4P not 2P The proposal will help those with mobility issues who cannot park close to their homes 	Permit parking restrictions are not recommended on Ruby Street given the low overall response rate (19%) from community consultation.

Street	Theme	Resident comments	Officer response
Ruby Street	Ineligible households	<ul style="list-style-type: none"> I have a driveway to the carport and thus have been classified as having 2 parking spots which is stupid and unrealistic to actually park 2 cars. Otherwise I would of probably approved the parking restrictions. 	Permit parking restrictions are not recommended on Ruby Street given the low overall response rate (19%) from community consultation.

Street	Theme	Resident comments	Officer response
Carrington Road	Impact to businesses	Timed permit parking restrictions on Renwick Street will affect businesses on Carrington Road as business would be required to move all day. There is available parking on the street and if the issue with unattended taxi's was resolved, this would solve the issue	Permit parking restrictions are not recommended on Renwick Street given the low overall response rate (16%) from community consultation.

Street	Theme	Resident comments	Officer response
Carrington Road	Impact to businesses	<p>A business is requesting that Council extend restricted parking along the southern side of Renwick Street and northern side of Cary Street further south terminating at Carrington Road. And that the 2P be changed to 7P.</p> <p>They have advised implementing restricted parking in only some areas of Renwick Street and Cary Street, it will make it extremely competitive for the remaining unrestricted car spaces.</p> <p>Other businesses, visitors, taxi operators and the like will be competing for the unrestricted spaces on business frontages, which can be addressed through expanding parking restrictions further south down Renwick Street and Cary Street.</p> <p>Business is requesting for 5-6 permits and 2 visitors parking. Whilst, Staff can park onsite, there are times we may need to park on the street, with added temp staff and visitors.</p> <p>Timed parking can be Mon- Fri 7am to 6pm Saturday 7am – 12pm and open on Sunday to accommodate Mackey Park for training and playing on weekends.</p>	<p>Permit parking restrictions are not recommended on Renwick Street given the low overall response rate (16%) from community consultation.</p>

CONCLUSION

Given the low response rate received during consultation, it is not recommended to proceed with the proposed three-month trial to extend the Area M2 resident parking scheme to the streets where they were originally proposed. The low response rate suggests that current parking problems are affecting a small percentage of households rather than the wider precinct, and that permit parking uptake may be low. The installation of permit parking restrictions may also push existing parking issues to nearby streets as noted by households during community consultation.

Council engineers have discussed with the Parking Services unit on the most appropriate action to improve parking conditions for the small percentage of households that are affected by taxi's or nearby businesses. Council's Parking Services unit have advised of enforcement action taking place to manage unattended vehicles and taxi's. Accordingly, it is recommended that this on-going enforcement of unattended vehicles and taxis under the Public Spaces (Unattended Property) Act 2021 by Council's Parking Officers continue to change parking behaviours. This will increase parking supply on the streets surrounding Carrington Road and adjacent to Mackey Park at Richardson Crescent.

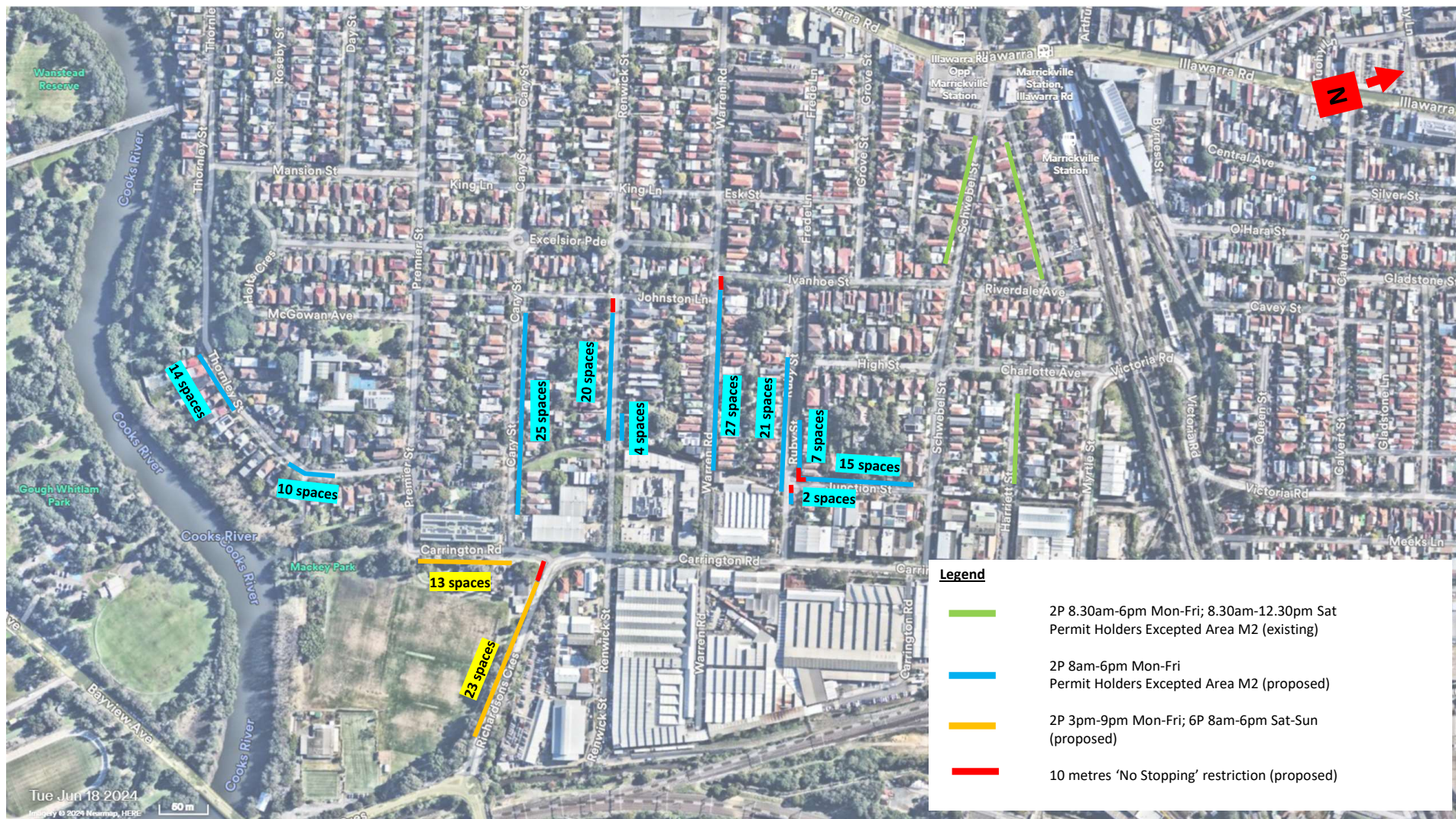
Furthermore, it is recommended to install timed parking restrictions signposted as '2P 3pm-9pm Mon-Fri; 4P 8am-6pm Sat-Sun' on the western side of Richardson Crescent, south of Carrington Road to prevent long-term parking adjacent to the park and improve parking opportunities for park users during the weekday evening and weekend daytime periods. This will reduce parking demand of park-users to nearby residential streets. This proposal is shown in Attachment 2.

FINANCIAL IMPLICATIONS

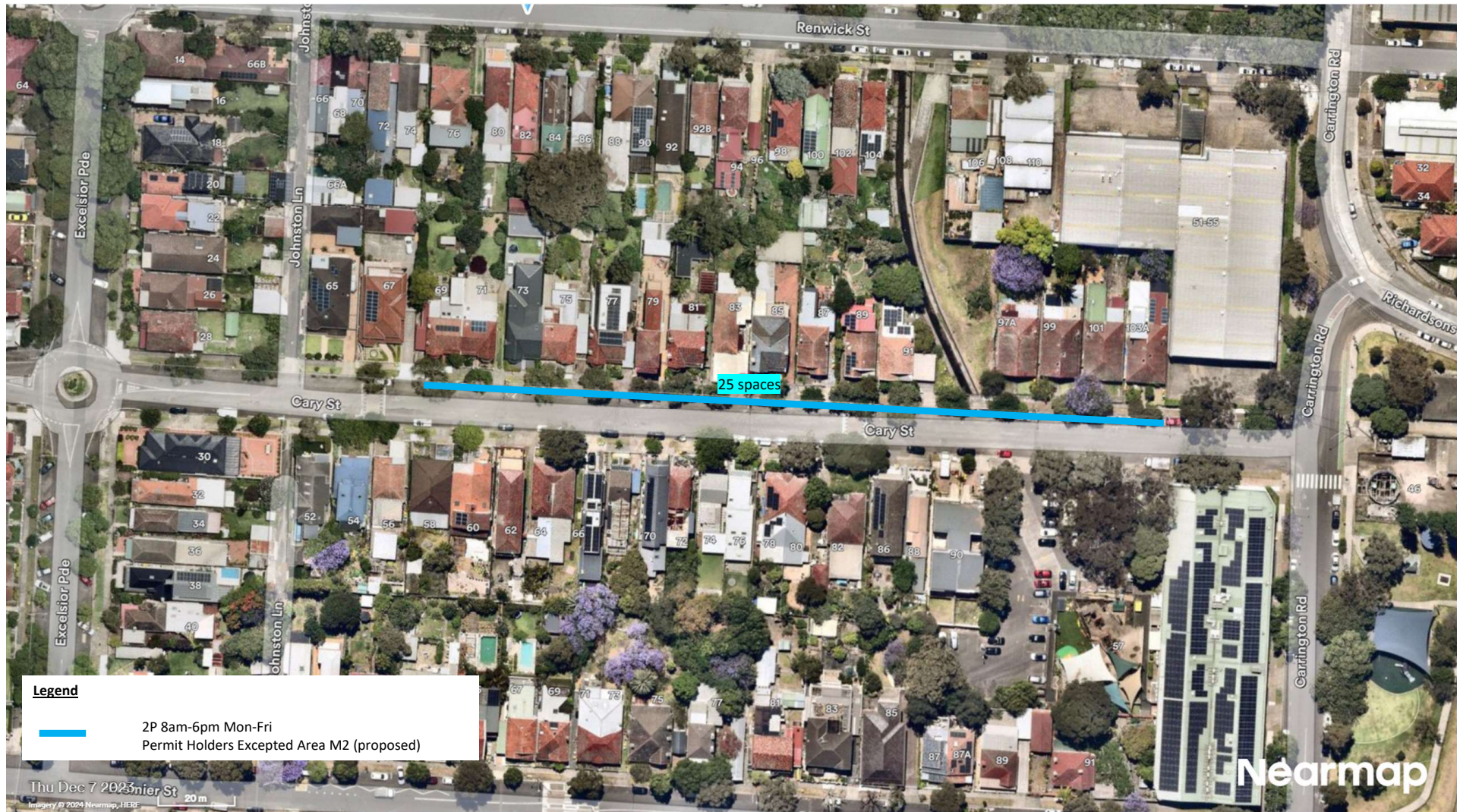
There are no financial implications associated with the implementation of the proposed recommendations outlined in the report.

ATTACHMENTS

1. [↓](#) Consultation plans
2. [↓](#) Proposed parking changes - Richardson Crescent, Marrickville



















Item No: LTC1124(1) Item 17
Subject: TEMPE RESERVE - PARKING STUDY (MIDJUBURI-MARRICKVILLE WARD/HEFFRON ELECTORATE/INNER WEST PAC)
Prepared By: James Nguyen - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the request for consideration of timed permit parking restrictions on streets surrounding Tempe Reserve not be implemented due to adequate parking capacity within the Tempe Reserve car parks.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

This report outlines the parking investigation completed to assess parking conditions at Tempe Reserve and the surrounding adjacent streets. It presents the parking survey results completed on the weekday evening and weekend peak periods. The results indicate there is adequate parking supply in the streets adjacent to Tempe Reserve, and that parking demand generated from park-users at Tempe Reserve during the evening weekday and weekend peak period has minimal impact to parking to the adjacent residential streets. Accordingly, a resident parking scheme in these streets is not recommended.

BACKGROUND

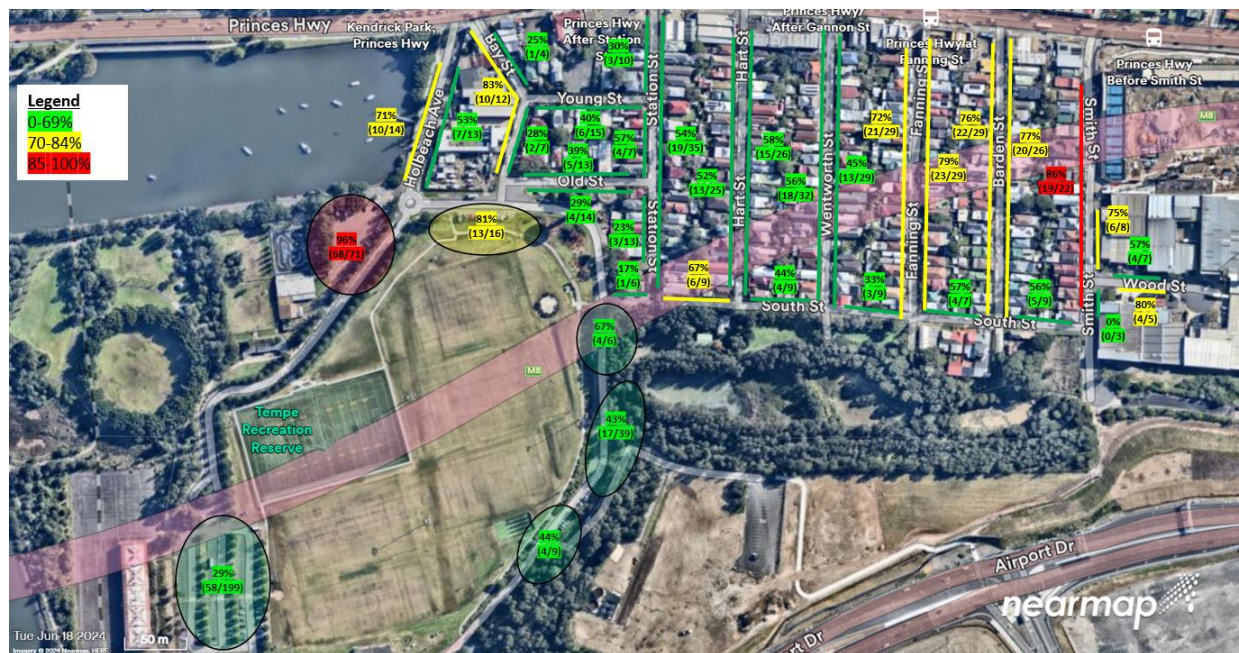
Council has received a petition from households on Smith Street, Tempe requesting for consideration for a resident permit parking scheme on Smith Street and all nearby streets adjacent to Tempe Reserve. Households have raised concerns that parking in front of homes on these streets are occupied by park users at Tempe Reserve, particularly in the evenings and on the weekend.

DISCUSSION

Council engineers have carried out parking occupancy surveys to assess parking utilisation on these streets and the car parks adjacent to Tempe Reserve during the peak and off-peak soccer seasons. The parking surveys assessed whether there were any 'spill-over' parking from the car parks at Tempe Reserve into the adjacent streets. They were carried out on the following dates:

- Thursday 7 December 2023 between 5.30pm and 6.30pm (off-peak season)
- Wednesday 15 May 2024 between 4.00pm and 10pm (peak season)
- Saturday 18 May 2024 between 8.00am and 2.00pm (peak season)

The parking occupancy survey results are shown below in Figures 1, 2 and 3.



Based on Figure's 1 and 2 above, the parking demand generated by training in the evening period varies between seasons, days and the type of training that may take place.

Figure 1 shows high utilisation of the carparks adjacent to Holbeach Avenue with 96 per cent and 81 per cent utilisation. There is parking capacity in the adjacent residential streets such as Old, Bay and Station Streets for residents. Parking spill-over may occur, however given there is available parking capacity in the adjacent streets, any spill over may be minimal and have minimal impact to households.

Figure 2 shows adequate capacity within the car parks at Tempe Reserve during the peak soccer season training period, and there is unlikely to be any 'spill-over' of parking to the adjacent residential streets. Parking demand generated by training in the evening periods are

likely to be wholly contained within the car parks. Adjacent streets such as Old, Bay and Station Streets are likely to mostly be utilised by residents.

Parking on Smith Street is highly occupied during the evening period, however this is likely to be parking occupied by residents and households. Given the proximity to Tempe Reserve, it is highly unlikely parking on Smith Street is utilised by park-users. A resident permit parking scheme in the evening period is not recommended as parking generation is mostly generated by residential households.



Figure 3 - Parking Occupancy Survey - Saturday 18 May 2024 - 10.00am-11.00am

Figure 3 above shows the peak parking occupancy rates on 'game day' at Tempe Reserve and the surrounding nearby streets. Parking utilisation in the car parks adjacent to Holbeach Avenue had the highest utilisation of 98 and 100 per cent. The other car parks surrounding Tempe Reserve have parking capacity. Accordingly, parking 'spill-over' from the car parks closest to Holbeach Avenue may occur to the nearby residential streets. However, based on the available parking occupancy rates in nearby residential streets, parking 'spill-over' from the car parks is likely to be minimal, and will have minimal impact on nearby streets. Similarly to Figures 1 and 2, Smith Street experiences high parking utilisation, however given the distance to Tempe Reserve, it is highly unlikely that these parking spaces are utilised by park-users. Parking utilisation in Smith Street is likely to mostly be adjacent residents/households. Accordingly, permit parking restrictions is not recommended during the evening weekday weekend periods on streets adjacent to Tempe Reserve.

FINANCIAL IMPLICATIONS

There are no financial implications associated with the implementation of the proposed recommendations outlined in the report.

ATTACHMENTS

Nil.

Item No: LTC1124(1) Item 18
Subject: LEICHHARDT WEST - PROPOSED RESIDENTIAL PARKING SCHEME EXPANSION (BALUDARRI-BALMAIN WARD/BALMAIN ELECTORATE/LEICHHARDT PAC)
Prepared By: Jackie Ng - Graduate Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the installation of Resident Parking Scheme '2P 8am-6pm Mon-Fri, Permit Holders Excepted, Area L1' zone be approved as shown in *Attachment 1*:

- a) Western and Eastern side of Burfitt Street, Leichhardt – Between No.2 to No.48 and No.1 to No.45 Burfitt Street
- b) Eastern side Edith Street, Leichhardt – Between No.37 to No.65A Edith Street
- c) Western and Eastern side of Elswick Street, Leichhardt – Between No.118 to No.162 and No.105 to No.141 Elswick Street

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

EXECUTIVE SUMMARY

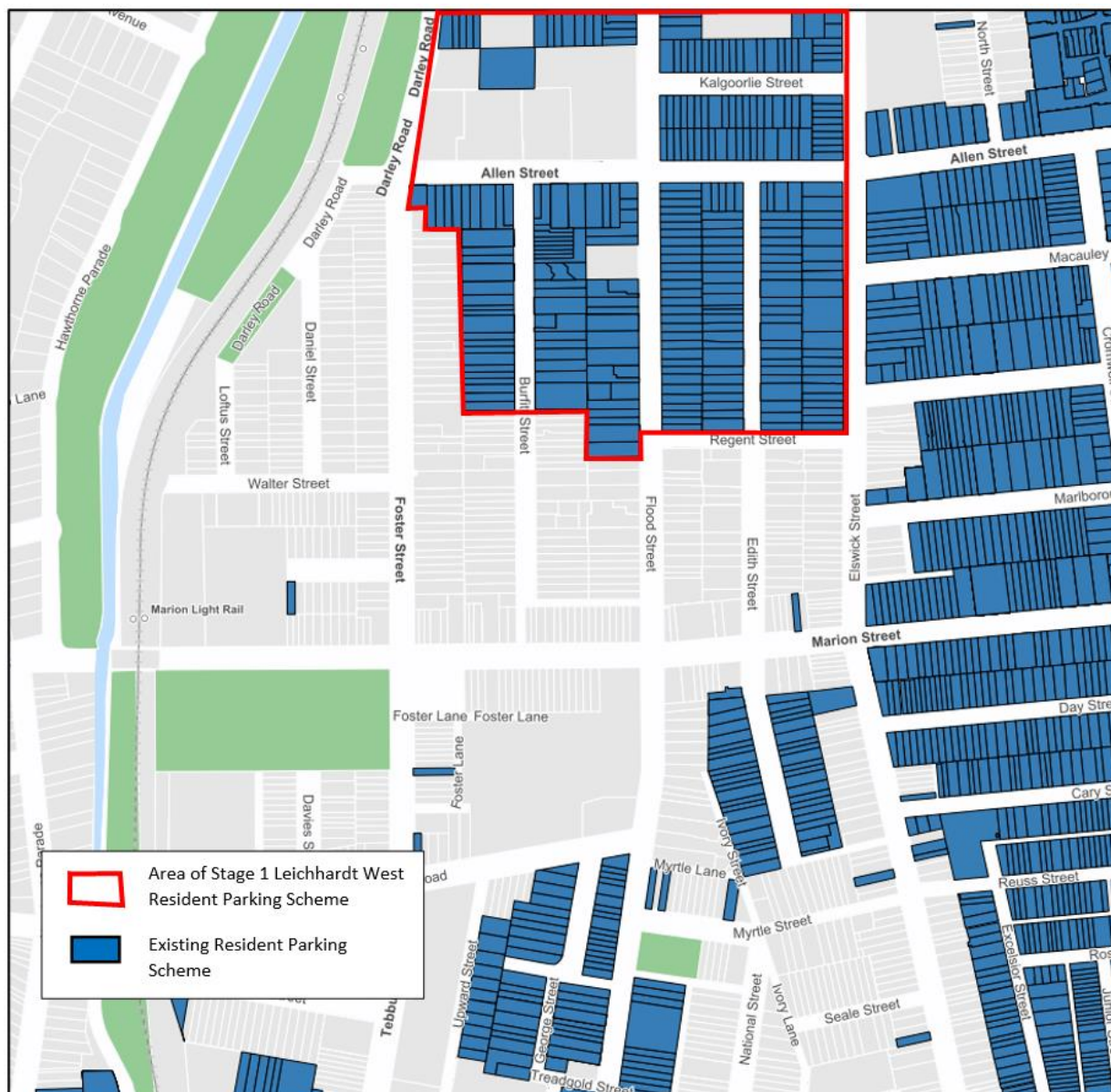
This report outlines the outcome from the Community Consultation and the recommended treatment for the expansion of the existing Leichhardt West Resident Parking Scheme (RPS) on section of Burfitt Street, Flood Street, Edith Street and Elswick Street, Leichhardt.

It is recommended that the RPS '2P 8AM-6PM, Mon-Fri, Permit Holders Excepted, Area L1' be expanded into the rest of Burfitt Street, eastern side of Edith Street and Elswick Street from the existing RPS boundary to Marion Street.

BACKGROUND

Council introduced an RPS in the Leichhardt West area in late 2023 which was one of the recommendations from the adopted Leichhardt West Precinct Parking Study. Following the implementation, many requests were received from residents to expand the Stage 1 RPS further south to Marion Street, as it has resulted in a section of unrestricted parking with higher parking demand.

A map showing the Stage 1 RPS for the Leichhardt West area is shown is illustrated below:



A post implementation review was expedited after the implementation of the Stage 1 RPS, including observing and collecting parking data during different times and days. It was generally found that the unrestricted parking on sections of streets adjacent to the RPS had high occupancy rates.

Hence, Council consulted the community proposing that the RPS would be expanded into Burfitt Street, Flood Street, Edith Street and Elswick Street between the existing RPS boundary to Marion Street, Leichhardt.

DISCUSSION

Between June and September 2024, Council undertook Community Engagement inviting residents to provide feedback expanding the Stage 1 RPS further south to Marion Street.

The community engagement for the proposed RPS expansion in Leichhardt West received a low response rate, and whilst it did not strictly achieve the support level required for the former Leichhardt Resident Parking Scheme Policy, some streets meet the requirements for the Inner West Public Domain Parking Policy and showed a high support rate for the proposal as shown in Table 1 and Figure 1.

Street Name	Side	Support Rate ¹ of Response Rate ²
Burfitt Street	West	83%
	East	70%
Flood Street	West	33%
	East	50%
Edith Street	West	38%
	East	67%
Elswick Street	West	67%
	East	83%

Table 1 – Support Rate in each section

1. Support Rate is the number of valid votes in the subject section that support the RPS expressed as a percentage of the number of responses for the section.
2. Response Rate is the number of valid votes compared to the number of properties for the section expressed as a percentage.

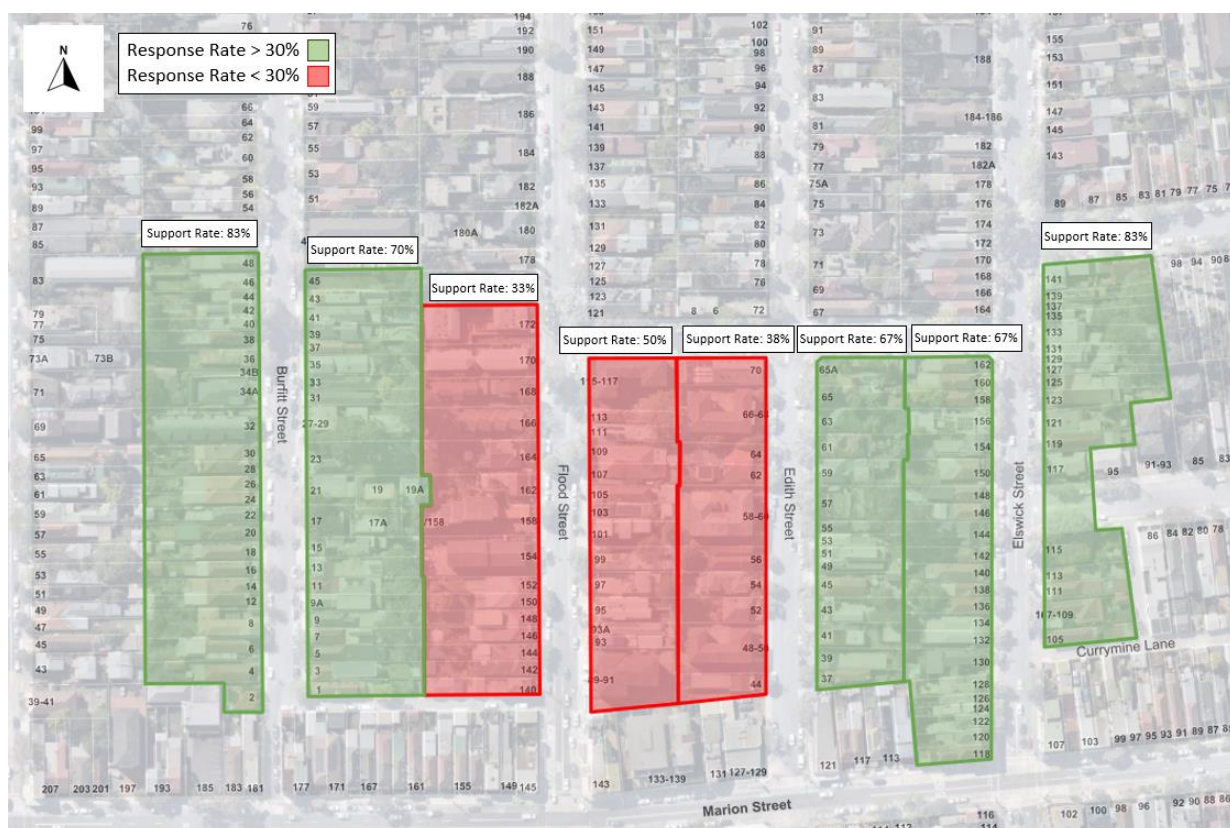


Figure 1 – Support Rate and Response Rate from Council's Engagement in Leichhardt West

Due to the high support rate of those who responded to the consultation, it is recommended that the existing Resident Parking Scheme be expanded into the following sections:

- a. Western and Eastern side of Burfitt Street, Leichhardt – Between No.2 to No.48 and No.1 to No.45

- b. Eastern side Edith Street, Leichhardt – Between No.37 to No.65A (permits will be allocated to eligible properties on both sides of the street)
- c. Western and Eastern side of Elswick Street, Leichhardt – Between No.118 to No.162 and No.105 to No.141

Council will generally not proceed with implementation of a parking scheme or changes to an existing parking scheme in isolation from a precinct wide parking study unless at least 65% of respondents, from different households within the proposed zone, support the proposal and provided a minimum response rate of 30% of households is achieved in Council's survey.

A summary of comments from the Community Engagement, including Council response on each issue has been tabled below.

Resident Comments	Council Response
10 metre 'No Stopping' The 10 metre 'No Stopping' rule seems very unnecessary and counterproductive. Removing 10 metres of parking in an area with already limited parking does not help the situation.	The proposal will not reduce any legal parking spaces in the area. The NSW Road Rules states that, drivers are not permitted to stop within 10 metres of an intersection unless there are parking control signs or if you are parked along the continuous side of a T-intersection.
Shared Housing The current proposal does not support the area demographic. There are many adults in shared houses, each owning their own vehicle. Two permits will not be able to support the demand.	The management of permits is a challenge for Council as parking demand far outweighs the supply of existing on-street spaces, and the permit allocation has remained unchanged for many years. Allocation of extra permits will simply result in less chance of finding available permit parking as they number of permits will outnumber the number of parking spaces.
Visitor Parking The proposal is not beneficial for people visiting family and friends in the area. The inconvenience of limit visitor parking concerns me.	The RPS allocates one (1) visitor permit for each property subject to eligibility criteria of Council's resident parking policy. This permit is transferrable and can be used for any vehicle visiting the area. The proposed 2P 8am-6pm Mon-Fri restriction provide parking opportunities by allowing for visitors without permits to stay after 4pm on weekdays until 10am next day. Parking on weekends remains unrestricted.
Parking Permit You neglect to say that people have to pay yearly for the 2 nd permit.	The current parking fees and charges for the Leichhardt West RPS does not require payment for a second permit.
Public & Active Transport Currently, the public transportation in the area does not support a car free population as much as we'd all like to help our environment by cutting our emissions.	The RPS supports Council's initiative to reduce car ownership in the Inner West. Additionally, Council has a number of projects and funding, prioritizing on active transport projects which have reciprocal benefits on public transport use.
Unattended Vehicles There are many trailers permanently parked on the street and are too many houses without parking spaces. Please ensure these	Unattended trailers may be enforced under the <i>Public Spaces (Unattended Property) Act 2021</i> . More information can be found on Council's website under "Unattended Vehicles".

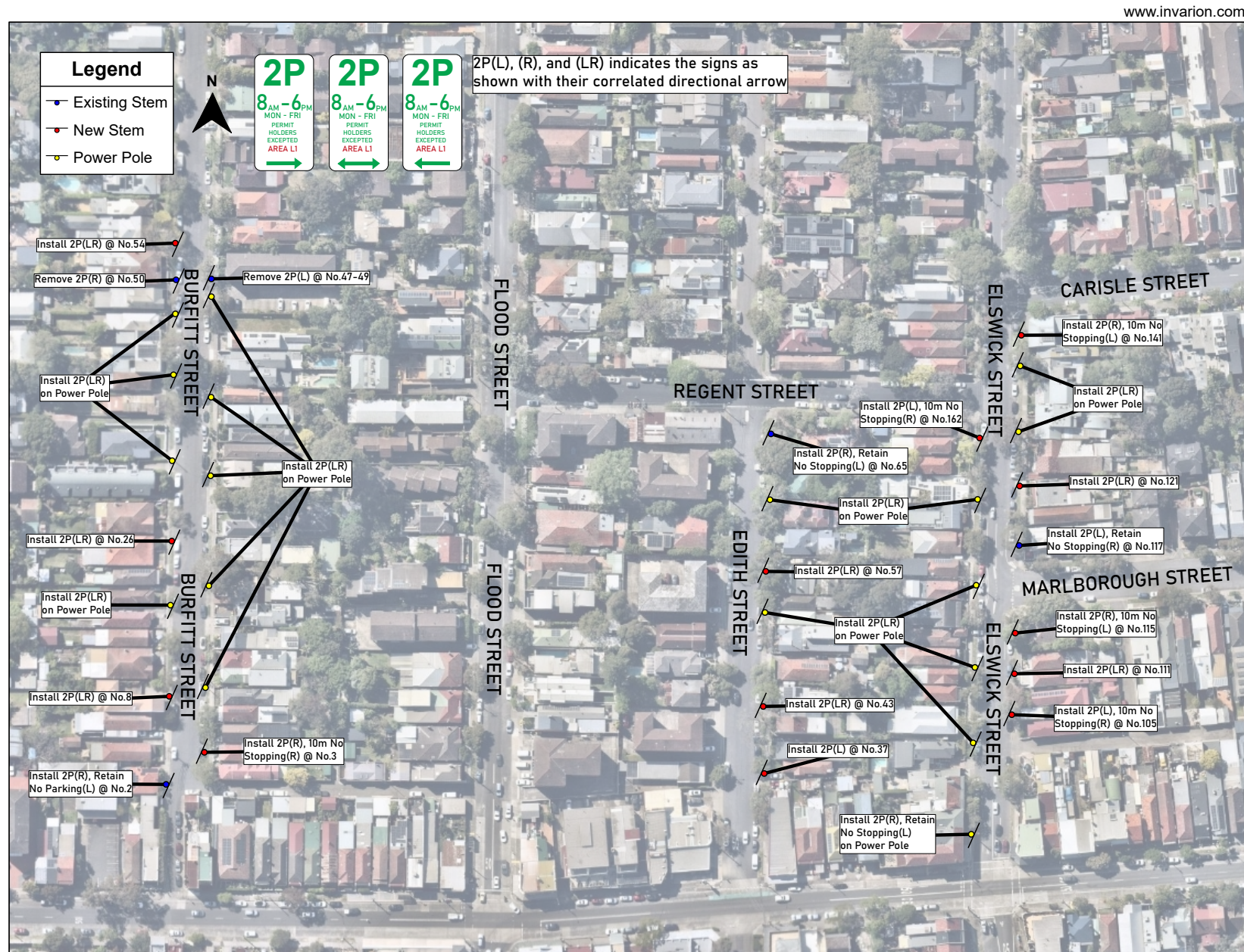
are subject to the same rules as motor vehicles in this parking scheme.	
Effect on Businesses We feel that businesses near Marion Street will be greatly affected and thus losing the character of the area.	Please see comment regarding visitor parking. The RPS aims to reduce long-term parking in residential areas which may provide more availability of short-term parking for visitors to find parking during business hours.

FINANCIAL IMPLICATIONS

The cost of installation of new restrictions as recommended can be funded within Council's signs and line market budget.

ATTACHMENTS

- [1.](#) Leichhardt West - Resident Parking Scheme Expansion - Signage Plan



Item No: LTC1124(1) Item 19
Subject: TRAFFIC COMMITTEE SCHEDULE 2025
Prepared By: Charbel El Kazzi - Traffic Engineer
Authorised By: Manod Wickramasinghe - Traffic and Transport Planning Manager

RECOMMENDATION

That the proposed schedule of meetings of the Local Traffic Committee for the 2025 calendar year be received and noted.

STRATEGIC OBJECTIVE

This report supports the following strategic directions contained within Council's Community Strategic Plan:

2: Liveable, connected neighbourhoods and transport

BACKGROUND

To assist Committee members with forward planning, the schedule of meetings of the Local Traffic Committee for 2025 is detailed below.

DISCUSSION

All meetings will be held on the 3rd Monday of each month, excluding January during which no meeting is held and April and December which has been brought 1 week forward to avoid Easter Monday and the Christmas holiday period. The meetings will commence at 11.00am. The proposed meeting dates for 2025 are as follows:

Monday, 17 February 2025	11.00am
Monday, 17 March 2025	11.00am
Monday, 14 April 2025	11.00am
Monday, 19 May 2025	11.00am
Monday, 16 June 2025	11.00am
Monday, 21 July 2025	11.00am
Monday, 18 August 2025	11.00am
Monday, 15 September 2025	11.00am
Monday, 20 October 2025	11.00am
Monday, 17 November 2025	11.00am
Monday, 8 December 2025	11.00am

ATTACHMENTS

Nil.