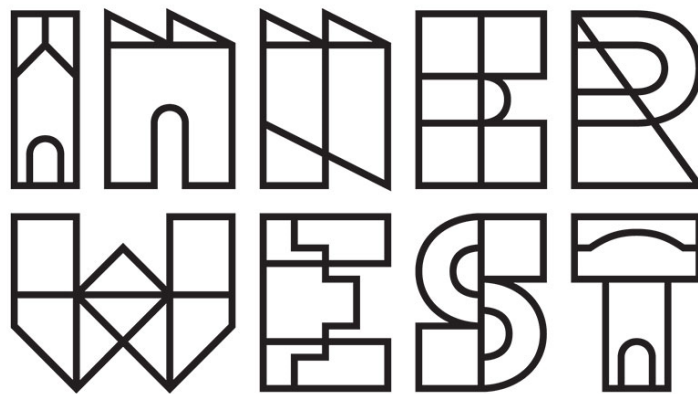


# AGENDA

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## **LOCAL TRAFFIC COMMITTEE MEETING**

**MONDAY 17 MAY 2021**

**10.00AM**

## Function of the Local Traffic Committee

### Background

Roads and Maritime Services (RMS) is legislated as the Authority responsible for the control of traffic on all NSW Roads. The RMS has delegated certain aspects of the control of traffic on local roads to councils. To exercise this delegation, councils must establish a local traffic committee and obtain the advice of the RMS and Police. The Inner West Council Local Traffic Committee has been constituted by Council as a result of the delegation granted by the RMS pursuant to Section 50 of the Transport Administration Act 1988.

### Role of the Committee

The Local Traffic Committee is primarily a technical review and advisory committee which considers the technical merits of proposals and ensures that current technical guidelines are considered. It provides recommendations to Council on traffic and parking control matters and on the provision of traffic control facilities and prescribed traffic control devices for which Council has delegated authority. These matters are dealt with under **Part A** of the agenda and require Council to consider exercising its delegation.

In addition to its formal role as the Local Traffic Committee, the Committee may also be requested to provide informal traffic engineering advice on traffic matters not requiring Council to exercise its delegated function at that point in time, for example, advice to Council's Development Assessment Section on traffic generating developments. These matters are dealt with under **Part C** of the agenda and are for information or advice only and do not require Council to exercise its delegation.

### Committee Delegations

The Local Traffic Committee has no decision-making powers. The Council must refer all traffic related matters to the Local Traffic Committee prior to exercising its delegated functions. Matters related to State Roads or functions that have not been delegated to Council must be referred directly to the RMS or relevant organisation.

The Committee provides recommendations to Council. Should Council wish to act contrary to the advice of the Committee or if that advice is not supported unanimously by the Committee members, then the Police or RMS have an opportunity to appeal to the Regional Traffic Committee.

### Committee Membership & Voting

Formal voting membership comprises the following:

- one representative of Council as nominated by Council;
- one representative of the NSW Police from each Local Area Command (LAC) within the LGA, being Newtown, Marrickville, Leichhardt and Ashfield LAC's.
- one representative from the RMS; and
- State Members of Parliament (MP) for the electorates of Summer Hill, Newtown, Heffron, Canterbury, Strathfield and Balmain or their nominees.

Where the Council area is represented by more than one MP or covered by more than one Police LAC, representatives are only permitted to vote on matters which effect their electorate or LAC.

Informal (non-voting) advisors from within Council or external authorities may also attend Committee meetings to provide expert advice.

### Committee Chair

Council's representative will chair the meetings.

### Public Participation

Members of the public or other stakeholders may address the Committee on agenda items to be considered by the Committee. The format and number of presentations is at the discretion of the Chairperson and is generally limited to 3 minutes per speaker. Committee debate on agenda items is not open to the public.



## AGENDA

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<b>5</b>	<b>Part A – Items Where Council May Exercise Its Delegated Functions</b>	

### Traffic Matters

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LTC0521(1) Item 7	Review- Detailed Works (Traffic & Parking) in Trafalgar Street, Petersham-RSL Petersham Development- DA201800173& Implementation of Regional Bicycle Route 7 (RR7). (Stanmore Ward- Damun/Newton Electorate/Inner West PAC)	<b>147</b>

### Parking Matters

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LTC0521(1) Item 8	Trafalgar Lane, Annandale - Proposed 'No Parking' Restrictions (Baludarri-Balmain/Balmain Electorate/Leichhardt PAC)	<b>154</b>

**Late Items**

Nil at time of printing.

**6      Part B - Items for Information Only**

Nil at the time of printing.

**7      Part C - Items for General Advice**

Nil at the time of printing.

**8      General Business**

**9      Close of Meeting**

## Minutes of Local Traffic Committee Meeting

Held electronically in April 2021

### **ACKNOWLEDGEMENT OF COUNTRY BY CHAIRPERSON**

*I acknowledge the Gadigal and Wangal people of the Eora nation on whose country we are meeting today, and their elders past and present.*

### **COMMITTEE REPRESENTATIVES PRESENT**

Clr Victor Macri	Councillor – Midjumburi-Marrickville Ward (Chair)
Bill Holliday	Representative for Jamie Parker MP, Member for Balmain
Kieren Ash	Representative for Ron Hoenig MP, Member for Heffron
Maryanne Duggan	Representative for Jodi McKay MP, Member for Strathfield
SC Tony Kenny	NSW Police - Inner West Police Area Command
Tanmila Samin Islam	Transport for NSW (TfNSW)

### **NON VOTING MEMBERS IN ATTENDANCE**

Colin Jones	Inner West Bicycle Coalition (IWBC)
Bob Moore	Inner West Bicycle Coalition (IWBC)
Adrian Prichard	Transit Systems – Inner West Bus Services
Clr Marghanita da Cruz	Councillor – Gulgadya-Leichhardt Ward (Alternative Chair)
Manod Wickramasinghe	IWC's Traffic and Transport Planning Manager
George Tsaprounis	IWC's Coordinator Traffic Engineering Services (South)
Sunny Jo	IWC's Coordinator Traffic Engineering Services (North)
Christina Ip	IWC's Business Administration Officer

### **VISITORS**

Nil.

### **APOLOGIES:**

Nil.

### **DISCLOSURES OF INTERESTS:**

Nil.

### **CONFIRMATION OF MINUTES**

The minutes of the Local Traffic Committee Meeting held in March 2021 were confirmed.

### **MATTERS ARISING FROM COUNCIL'S RESOLUTION OF MINUTES**

The Local Traffic Committee recommendations of its meeting held in March 2021 were adopted at Council's meeting held on 13 April 2021 with the following amendments:

- a) That Council refer concerns on Item 7 Unwins Bridge Road, Way Street, Toyer Street & Collins Street, St Peters - Formalising Parking Restrictions around Tempe High School (Midjumburi - Marrickville Ward/ Heffron Electorate/ Inner West PAC) by residents in Toyer Street back to the Local Traffic Committee to amend point 5 to a

'10 metre No Stopping' on the Northern side of Toyer Street east of Collins Street and delete point 6;

- b) That Item 9 Balmain East Precinct Parking Study (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC) be deferred pending a briefing for Balmain ward councillors; and
- c) That Item 3 Detailed works (Traffic & Parking) in Trafalgar Street, Petersham - RSL Petersham Development - DA201800173 & Implementation of the Regional Bicycle Route 7 (RR7) (Stanmore Ward-Damun/Newton Electorate/Inner West PAC) go back to the LTC for further review.

#### **EMAIL CONFIRMATION OF OFFICER'S RECOMMENDATION:**

The representative for NSW Police – Inner West supported the Officer's recommendations for the items in their PAC.

The Transit Systems representative supported all the Officer's recommendations.

#### **LTC0421(1) Item 1 Tempe South Final Local Area Traffic Management (LATM) Study (Midjuburi-Marrickville Ward/Heffron Electorate/Inner West PAC)**

#### **SUMMARY**

After the Public Exhibition of the draft Tempe South Local Area Traffic Management (LATM) study, Council has collated and assessed community feedback and revised the final recommendations as outlined in Attachment 1.

The draft report underwent a 28 day public exhibition during November 2020 and January 2021, with letters sent to residents and businesses in the study area inviting to visit the Your Say Inner West project page to see the draft report, proposed concept plans, and provide feedback via an online survey. Comments were also accepted and received through email and post.

Amongst the responses included resident groups from Union Street and Smith Street. Additional submissions from businesses were also received. In general, most submissions indicated some preference to the proposed treatments, with the exception from Union Street where residents opposed both options proposed for the street, and Tramway Street where there was opposition to the contrasting pavements.

Having considered the submissions, a review was undertaken for a 'No Entry' treatment from Smith Street to Union Street at Princess Highway traffic signals, a shared zone treatment in Union Street and a partial road closure in Brooklyn Street. Further consultation for these treatments is required.

Generally, the remaining treatments was supported by the community, and these are outlined in Appendix E: Engagement Outcomes Report.

#### **Officer's Recommendation**

THAT:

1. The final Tempe South Local Area Traffic Management (LATM) Study and proposed treatments be noted;
2. The recommended treatments as outlined in Attachment 2 be adopted excluding proposals at Union Street and Brooklyn Street;
3. Further consultation be undertaken for a partial road closure at Brooklyn Street at Princess Highway, a 'Shared Zone' in Union Street, 'No Entry from Smith Street', 'Left

- and Right Only' restriction at the entrance of Union Street at Princess Highway and Smith Street traffic signals;
4. After a 12 month period following the issue of an Occupation Certificate for the Bunnings Development at 728-750 Princess Highway, a LATM review be undertaken by Council and reported back to the Traffic Committee; and
  5. It be noted that Council is separately investigating the provision of traffic signals at the Bunnings exit driveway at Princess Highway.

### **DISCUSSION**

The TfNSW representative stated that:

- The partial road closure from Princes Highway onto Brooklyn Street will need TfNSW approval. The proposed road closure will result in more traffic on Union Street, which will impact the safe operation of a Shared Zone.
- Restricting through movements from Smith Street onto Union Street will leave motorists almost no option to enter Union Street or lead to a very significant detour and may not be supported by TfNSW. Further investigations will be required.

The Member for Heffron requested that the item be deferred until a determination regarding the signalised exit from Bunnings is received from the Minister for Transport.

The Committee members supported the deferral proposed by the Member for Heffron.

### **COMMITTEE RECOMMENDATION**

**THAT the final Tempe South Local Area Traffic Management (LATM) Study and proposed treatments be deferred until a determination regarding the signalised exit from Bunnings is received from the Minister for Transport.**

**For motion:** Unanimous

**LTC0421(1) Item 2 Garden Street, Marrickville – Sydney Metro SSJ - Notice of a Temporary Full Road Closure for a 4 day period to undertake restoration work (Midjumburi – Marrickville Ward / Newtown Electorate / Inner West PAC)**

### **SUMMARY**

Council has received notice from Sydney Metro SSJ Group for the temporary full road closure of Garden Street, between Shirlow Street and road end, Marrickville for a 4 day period between the dates of 30 April and 3 May 2021 (contingency period of 2 weeks to 17 May 2021) in order to carry out restoration work on Garden Street. It is recommended that the proposed temporary full road closure be approved, subject to the conditions outlined in this report.

### **Officer's Recommendation**

THAT the proposed temporary full road closure of Garden Street, between Shirlow Street and road end, Marrickville for a 4 day period between the dates of 30 April and 3 May 2021 (contingency period of 2 weeks to 17 May 2021) be approved for the purpose of restoration work on Garden Street subject to, but not limited to, the following conditions:

1. A Road Occupancy License be obtained by the applicant from TMC;
2. All affected residents and businesses, including the NSW Police Area Commander, Fire

& Rescue NSW and the NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary full road closure at least 7 days in advance of the closure with the applicant making reasonable provision for stakeholders; and

3. The occupation of the road carriageway must not occur until the road has been physically closed.

### **DISCUSSION**

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

**THAT the proposed temporary full road closure of Garden Street, between Shirlow Street and road end, Marrickville for a 4 day period between the dates of 30 April and 3 May 2021 (contingency period of 2 weeks to 17 May 2021) be approved for the purpose of restoration work on Garden Street subject to, but not limited to, the following conditions:**

- 1. A Road Occupancy License be obtained by the applicant from TMC;**
- 2. All affected residents and businesses, including the NSW Police Area Commander, Fire & Rescue NSW and the NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary full road closure at least 7 days in advance of the closure with the applicant making reasonable provision for stakeholders; and**
- 3. The occupation of the road carriageway must not occur until the road has been physically closed.**

**For motion:** Unanimous

**LTC0421(1) Item 3 Toothill Street Near Victoria Street, Lewisham - Pedestrian Safety Improvement Works – Design Plan 10163 (Damum - Stanmore Ward / Summer Hill Electorate/ Inner West PAC)**

### **SUMMARY**

Council has finalised a design plan for pedestrian safety improvement works in Toothill Street at Victoria Street, Lewisham. The proposed works will raise the existing pedestrian crossing to improve pedestrian and motorist safety and address concerns about pedestrian safety, particularly school children, and driver behaviour in the area.

### **Officer's Recommendation**

THAT the detailed design plan for the raising of the existing pedestrian crossing and new adjacent kerb ramps and associated signs and line markings in Toothill Street at Victoria Street, Lewisham (as per Plan No.10163) be APPROVED.

### **DISCUSSION**

The IWBC representative requested bike symbols on Toothill Street and Victoria Street.

Clr da Cruz provided feedback from a resident who requested a zebra crossing across Victoria Street at Toothill Street. Council Officers stated that this request is outside of the scope of the current project and would need to be investigated separately.

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

**THAT the detailed design plan for the raising of the existing pedestrian crossing and new adjacent kerb ramps and associated signs and line markings in Toothill Street at Victoria Street, Lewisham (as per Plan No.10163) be APPROVED.**

**For motion:** Unanimous

**LTC0421(1) Item 4 Centennial Street, Marrickville – Temporary Road Closure for a Special Event at Henson Park – 2021 Beer, Footy and Food Festival on Saturday 24 July and Saturday 7 August 2021 (Midjuburi-Marrickville Ward /Summer Hill Electorate /Inner West LAC)**

### **SUMMARY**

A S68 Application has been received from the Music & Booze Company together with the Newtown Rugby League Football Club to hold the 2021 Beer, Footy and Food Festival between 10.00am and 10.00pm on Saturday, 24 July 2021 and Saturday, 7 August 2021 at Henson Park Oval. The event requires the temporary full road closure of Centennial Street, Marrickville between Sydenham Road and the entrance gate to Henson Park Oval. It is recommended that Council agree to the temporary full road closure subject to the applicant complying with the following conditions and obtaining concurrence from Transport for NSW (formally RMS) as the closure entails lane closures on Sydenham Road, Marrickville.

### **Officer's Recommendation**

THAT the proposed temporary full road closure of Centennial Street, Marrickville between Sydenham Road and the entrance to Henson Park & Marrickville & District Hardcourt Tennis Club car park, between 10.00am and 10.00pm on Saturday 24 July 2021 and Saturday 7 August 2021 (contingency period of one month to 4 September 2021) for the purpose of holding the 'Beer, Footy and Food Festival 2021' event be APPROVED, subject to the approval of the S68 Application and the applicant complying with, but not limited to, the following conditions:

1. A Road Occupancy License application be obtained by the applicant from the Transport Management Centre;
2. All affected residents and businesses, including NSW Police Local Area Commander, Transit Systems, Fire and Rescue NSW and NSW Ambulance Services, shall be notified in writing by the applicant of the proposed temporary road closure at least 7 days prior to the event, with the applicant making reasonable provision for residents and businesses;
3. The occupation of the road carriageway must not occur until the road has been physically closed; and
4. Subject to written concurrence from Transport for New South Wales.

### **DISCUSSION**



It was noted that this is a Class 3 event.

The IWBC representative suggested a review be undertaken to address missing or inadequate bike facilities in the area, including pram ramps.

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

**THAT the proposed temporary full road closure of Centennial Street, Marrickville between Sydenham Road and the entrance to Henson Park & Marrickville & District Hardcourt Tennis Club car park, between 10.00am and 10.00pm on Saturday 24 July 2021 and Saturday 7 August 2021 (contingency period of one month to 4 September 2021) for the purpose of holding the 'Beer, Footy and Food Festival 2021' event be APPROVED, subject to the approval of the S68 Application and the applicant complying with, but not limited to, the following conditions:**

- 1. A Road Occupancy License application be obtained by the applicant from the Transport Management Centre;**
- 2. All affected residents and businesses, including NSW Police Local Area Commander, Transit Systems, Fire and Rescue NSW and NSW Ambulance Services, shall be notified in writing by the applicant of the proposed temporary road closure at least 7 days prior to the event, with the applicant making reasonable provision for residents and businesses;**
- 3. The occupation of the road carriageway must not occur until the road has been physically closed; and**
- 4. Subject to written concurrence from Transport for New South Wales.**

**For motion:** Unanimous

### **LTC0421(1) Item 5 Draft Inner West Pedestrian Access and Mobility Plan (PAMP) - For Public Exhibition (All Wards / All Electorates / All PACS)**

#### **SUMMARY**

Council's Integrated Transport Strategy (ITS) identifies the need for a Pedestrian Access and Mobility Plan (PAMP) as a key action. The PAMP study aims to:

- Provide a network of safe, convenient and connected pedestrian routes which will encourage the shift from car dependency to walking.
- Enhance the pedestrian network to allow all pedestrian to enjoy safe, convenient and coherent independent mobility.

The work consisted of reviewing existing policies and strategies, future land use, reviewing historical crash data, community submissions and auditing existing pedestrian routes condition of identified priority routes. An initial community survey was also undertaken to seek the community's insight into being a pedestrian within the LGA. This led to the development of a draft Pedestrian Access and Mobility Plan (PAMP).

The Draft PAMP has been prepared by consultant; Bitzios Consulting. The full draft report is provided in Attachment 1.



### **Officer's Recommendation**

THAT:

1. The Draft Inner West Pedestrian Access and Mobility Plan (PAMP) report be received and noted; and
2. The draft report be placed on Public Exhibition, providing a minimum 28 days for community feedback and the results be reported back to the Local Traffic Committee and Council.

### **DISCUSSION**

The TfNSW representative advised that the PAMP should not making recommendations for State road carriageways or at signals. If there are any recommendations, these are to be reviewed and agreed to by TfNSW. The TfNSW representative also stated that any proposals for constructions along the State road network or at traffic signals will need to be approved by TfNSW.

The IWBC representative suggested a review also be undertaken to address missing or inadequate bike facilities, including pram ramps.

The representative for the Member for Balmain commented that the pedestrian path north of the City West Link northern sound wall is not shown in the PAMP maps or listing. The representative stated that the pavement of the path, which connects Charles Street to James Street via Hubert Street and Francis Street, has been lifted by tree roots. The lifting of pavements makes pedestrian access dangerous at night and is no longer wheelchair accessible. The representative for the Member for Balmain also requested for vegetation to be cleared from the footpath along the northern side of the City West Link, between Timbrell Drive and the footbridge opposite Waratah Street, Haberfield.

Clr da Cruz stated that the blue-green grid routes and a number of pedestrian routes have been omitted from the PAMP. Council Officers advised that the PAMP focuses primarily on auditing existing routes rather than establishing significant new routes. The blue-green grid is being separately investigated by Council's Urban Strategy Team and will establish a framework in which subsequent active transport planning will be carried out.

Additional footpath issues identified will be passed onto the relevant Council Officers for review.

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

THAT:

1. **The Draft Inner West Pedestrian Access and Mobility Plan (PAMP) report be received and noted; and**
2. **The draft report be placed on Public Exhibition, providing a minimum 28 days for community feedback and the results be reported back to the Local Traffic Committee and Council.**

**For motion:** Unanimous

**LTC0421(1) Item 6 Traffic Assessment on the Ashfield Aquatic Centre (160-178 Elizabeth Street, Ashfield) (Gulgadya - Leichhardt Ward/Summer Hill Electorate/Burwood PAC)**

**SUMMARY**

A Notice of Motion was raised at Council's meeting held on 9 March 2021 reading as follows:

**'THAT as a matter of urgency, given the major impact the new Ashfield Pool is having on traffic, parking and safety, that this be referred to the Local Traffic Committee and be reported to an Ordinary Council meeting in May 2021.'**

In response to the above, the following report accounts for the operation of the carparks associated with the new Ashfield Aquatic Centre, and the traffic flow, pedestrian and parking activity within the carparks and that of the adjacent roads as a result of the newly developed Ashfield Aquatic Centre.

Similarly, the report entails what measures have been incorporated and/or will be incorporated to address certain impacts on traffic, parking and safety. A Road Safety Audit will also be carried out to assess the measures incorporated and determine if further mitigating actions are required if necessary.

Traffic Committee approval is sought on regulatory measures on-road to support various actions required for improved operations and traffic safety around the Ashfield Aquatic Centre.

**Officer's Recommendation**

THAT:

1. The report be received and noted.
2. Double barrier (BB) centrelines be extended from the pedestrian crossing in Elizabeth Street (east of Etonville Parade) all the way to the signalised intersection of Elizabeth Street and Frederick Street, Ashfield.
3. 'All Traffic Left' and 'No Right Turn' be introduced at the Elizabeth Street driveway coming off the car park at the corner Frederick Street/Elizabeth.
4. 'No Entry' be placed at the Frederick Street driveway coming off the carpark at the corner of Fredrick Street/Elizabeth Street.
5. 'No Parking' be signposted around the dead end (turn around) area of Bastable Street as shown in Attachment 4.

**DISCUSSION**

Advice was provided to address concerns and questions raised by Cllr a Cruz:

- How vehicles egressing carpark will access Frederick Street following the installation of a 'Left Turn Only' sign in the Frederick Street/Elizabeth Street carpark: Council Officers advised that drivers wishing to turn right could easily turn left and detour around the surrounding streets or elect to use the Bastable Street carpark.
- Request for a high pedestrian activity 30km/h speed limit for the area: TfNSW advised that the NSW Speed Zoning Guidelines currently only make provisions for 40km/h speed limits, which are provided for High Pedestrian Activity areas and Local Traffic areas, as well as school zones, where there is a need to protect vulnerable road users in accordance with the Guidelines. As such, TfNSW are not considering any additional 30km/h speed limits until such a time that the Guidelines recommend such speed limits. Council Officers also advised that Elizabeth Street (between Frederick Street and Edwin Street North) has been included in Council's request to TfNSW for a 40km/h speed limit on local roads.

- What consultation was undertaken with schools/community/clubs/pool operations: Council Officers advised that the proposal was developed in consultation with the pool management and relevant project managers and that further notification of changes will be undertaken prior to implementation.
- Request for 'rear to kerb' parking in carparks: Council Officers advised that 'rear to kerb' parking restrictions are not common in carparks particularly where access to rear vehicle storage is desirable. The slow speed environment of carparks and limited vehicle movements allow both 'rear to kerb' and 'front to kerb' parking to occur safely. The pathways provided and safety concerns will be further reviewed during the Road Safety Audit process.
- Request to open the outdoor area of the creche to allow people to access the pool from the carpark: Council Officers advised this feedback can be passed onto the relevant project managers/pool managers. However, it was noted that the access gates to the outdoor space/creche are in place for emergency egress only and general access cannot be provided due to child protection requirements.

Clr da Cruz also raised concerns with the loss of landscaping outside the ACC and how that would affect temperatures in the outdoor area of the creche and noted that there is very little bicycle parking provided on site.

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

#### **THAT:**

1. **The report be received and noted.**
2. **Double barrier (BB) centrelines be extended from the pedestrian crossing in Elizabeth Street (east of Etonville Parade) all the way to the signalised intersection of Elizabeth Street and Frederick Street, Ashfield.**
3. **'All Traffic Left' and 'No Right Turn' be introduced at the Elizabeth Street driveway coming off the car park at the corner Frederick Street/Elizabeth.**
4. **'No Entry' be placed at the Frederick Street driveway coming off the carpark at the corner of Fredrick Street/Elizabeth Street.**
5. **'No Parking' be signposted around the dead end (turn around) area of Bastable Street as shown in Attachment 4.**

**For motion:** Unanimous

**LTC0421(1) Item 7 168 Norton Street (between Carlisle Street and Maccauley Street),  
Leichhardt - Road Occupancy (Gulgadga - Leichhardt/ Balmain  
Electorate/ Leichhardt PAC)**

### **SUMMARY**

Council has received an application from Growth Built Pty Ltd for approval of a temporary full road closure of Norton Street (in front of 168 Norton Street), between Carlisle Street and Macauley Street, Leichhardt from 9:00pm Wednesday, 2 June to 5:00am Thursday, 3 June 2021 (with a contingency period of four weeks) for dismantling of a tower crane at 168 Norton Street, Leichhardt.

### **Officer's Recommendation**

THAT the proposed temporary full road closure of Norton Street, between Carlisle Street and Macauley Street, Leichhardt from 9:00pm Wednesday, 2 June 2021 to 5:00am Thursday, 3 June 2021 (with a contingency period of four weeks – only between Sundays and Wednesdays) be approved for dismantling of a tower crane at 168 Norton Street, Leichhardt subject to, but not limited to, the following conditions:

1. A Traffic Management Plan (TMP) for the attached Traffic Control Plan (TCP) be submitted to TfNSW prior to the start of works;
2. A Road Occupancy License be obtained by the applicant from the Transport Management Centre;
3. All affected residents and businesses, including, Transit Systems, STA, NSW Police Area Command, Fire & Rescue NSW and the NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary full road closure at least 7 days in advance of the closure with the applicant making reasonable provision for stakeholders; and
4. The occupation of the road carriageway must not occur until the road has been physically closed.

### **DISCUSSION**

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

**THAT the proposed temporary full road closure of Norton Street, between Carlisle Street and Macauley Street, Leichhardt from 9:00pm Wednesday, 2 June 2021 to 5:00am Thursday, 3 June 2021 (with a contingency period of four weeks – only between Sundays and Wednesdays) be approved for dismantling of a tower crane at 168 Norton Street, Leichhardt subject to, but not limited to, the following conditions:**

- 1. A Traffic Management Plan (TMP) for the attached Traffic Control Plan (TCP) be submitted to TfNSW prior to the start of works;**
- 2. A Road Occupancy License be obtained by the applicant from the Transport Management Centre;**
- 3. All affected residents and businesses, including, Transit Systems, STA, NSW Police Area Command, Fire & Rescue NSW and the NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary full road closure at least 7 days in advance of the closure with the applicant making reasonable provision for stakeholders; and**
- 4. The occupation of the road carriageway must not occur until the road has been physically closed.**

**For motion:** Unanimous

**LTC0421(1) Item 8 Sydney Metro West, Power Enabling Works - Full Road Closures (Baludarri-Balmain Ward/Balmain Electorate/Leichhardt PAC)**

**SUMMARY**

Council has received an application from Quickway Constructions for approval of temporary full road closures at various locations and stages in Rozelle, as part of the Sydney Metro West Power Enabling Works. This approval is for the overarching road closures as part of CTMP 1 (Manning Street to Darling Street), CTMP 2 (Darling Street), CTMP 3 (Merton St to Mullens Street) and CTMP 4 (Mullens St to The Bays Station).

The road closures start on 31 May 2021 and last road closures end on 9 January 2022 (with a contingency period of 2 weeks for all closures) for civil construction works, trenching, conduit installation, cable pulling, restorations and associated works.

**Officer's Recommendation**

THAT the proposed temporary full road closures as part of the Sydney Metro West, Power Enabling Works in Rozelle from starting on 31 May 2021 to 9 January 2022 (with a contingency period of two weeks for each stage) be approved for civil construction works, trenching, conduit installation, cable pulling, restorations and associated works:

1. A Traffic Management Plan (TMP) for the attached Traffic Control Plan (TCP) be submitted to TfNSW prior to the start of works;
2. A Road Occupancy License be obtained by the applicant from the Transport Management Centre;
3. All affected residents and businesses, including, Transit Systems, STA, NSW Police Area Command, Fire & Rescue NSW and the NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary full road closure at least 7 days in advance of the closure with the applicant making reasonable provision for stakeholders; and
4. The occupation of the road carriageway must not occur until the road has been physically closed.

**DISCUSSION**

The TfNSW representative advised that the applicant should send the CTMP to TfNSW Sydney West Metro Network Integration team for review. The representative also advised that the ROLs and relevant approvals for closures of Darling Street, Victoria Road and any impact to traffic signals must be attained from the Transport Management Centre and not CJP.

The IWBC representative requested making allowances to cyclists to get through road closures including detour signage as the road works will affect a number of local bicycle routes. The new Australian Standards require that more attention be given to cycling through road works. Council Officers contacted Sydney Metro who advised that as the OCTMP is an over-arching, principles document, it does not contain the detail of how cyclists would be redirected. Sydney Metro stated that specific details addressing this concern will be provided in the CTMPs for each area, which will be provided to Council for review and approval.

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

**THAT the proposed temporary full road closures as part of the Sydney Metro West, Power Enabling Works in Rozelle from starting on 31 May 2021 to 9 January 2022 (with a contingency period of two weeks for each stage) be approved for civil construction works, trenching, conduit installation, cable pulling, restorations and associated works:**

- 1. A Traffic Management Plan (TMP) for the attached Traffic Control Plan (TCP) be submitted to TfNSW prior to the start of works;**
- 2. A Road Occupancy License be obtained by the applicant from the Transport Management Centre;**
- 3. All affected residents and businesses, including, Transit Systems, STA, NSW Police Area Command, Fire & Rescue NSW and the NSW Ambulance Services be notified in writing, by the applicant, of the proposed temporary full road closure at least 7 days in advance of the closure with the applicant making reasonable provision for stakeholders; and**
- 4. The occupation of the road carriageway must not occur until the road has been physically closed.**

**For motion: Unanimous**

**LTC0421(1) Item 9 Eaton Street, Balmain - Disabled Parking Zone (Baludarri - Balmain Ward/ Balmain Electorate/ Leichhardt PAC)**

### **SUMMARY**

Council has received requests to review the existing part-time 'Disabled Parking' zone in Eaton Street, Balmain to better service users of the space.

### **Officer's Recommendation**

THAT the existing part time 'Disabled Parking'/ 'P10min'/ '4P ticket' zone on the western side of Eaton Street, north of Darvall Street, Balmain, be converted to a full time 'Disabled Parking' zone.

### **DISCUSSION**

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

**THAT the existing part time 'Disabled Parking'/ 'P10min'/ '4P ticket' zone on the western side of Eaton Street, north of Darvall Street, Balmain, be converted to a full time 'Disabled Parking' zone.**

**For motion: Unanimous**



**LTC0421(1) Item 10 South Avenue, Petersham - Extension of 'No Parking' restrictions due to width of roadway (Damun - Stanmore Ward/ Newtown Electorate/ Inner West PAC)****SUMMARY**

A resident of Searl Street, Petersham (with rear lane access on South Avenue) raised concerns of vehicular access into and out of South Avenue, Petersham when vehicles are legally parked in South Avenue, immediate west of existing 'No Parking' area alongside boundary of Property No. 2 South Avenue, Petersham.

Surrounding residents have been notified of the proposal to extend the current 'No Parking' restrictions an additional 5 metres west along the northern side of South Avenue to attempt to remove a squeeze point thus improve vehicular access into South Avenue.

**Officer's Recommendation**

THAT the existing full-time 'No Parking' restrictions on the northern side of South Avenue, Petersham be extended 5 metres west, along the frontage of Property No. 2 South Avenue, Petersham.

**DISCUSSION**

The Committee members agreed with the Officer's recommendation.

**COMMITTEE RECOMMENDATION**

**THAT the existing full-time 'No Parking' restrictions on the northern side of South Avenue, Petersham be extended 5 metres west, along the frontage of Property No. 2 South Avenue, Petersham.**

**For motion:** Unanimous

**LTC0421(1) Item 11 Edgeware Road, Newtown between Wells & Darley Street - Proposed extension of Permit Parking Area M14 (Damun - Stanmore Ward/ Newtown Electorate/ Inner West PAC)****SUMMARY**

Following representations to Council, an extension of the existing M14 permit parking restrictions was proposed to achieve consistent parking management in the area. The proposal was distributed to residents along Edgeware Road, Newtown between Wells Street and Darley Street for comment. Currently, M14 permit parking restrictions exist on Darley Street, Wells Street, Lord Street and Edgeware Road between Darley Street and Lord Street.

**Officer's Recommendation**

THAT the '2P 8am – 10pm Permit Holders Excepted Area M14' parking restrictions in Wells Street and Darley Street be extended to the eastern side of Edgeware Road, Newtown between Wells Street and Darley Street to be consistent with the adjoining streets.

**DISCUSSION**

The Committee members agreed with the Officer's recommendation.

**COMMITTEE RECOMMENDATION**

**THAT the '2P 8am – 10pm Permit Holders Excepted Area M14' parking restrictions in Wells Street and Darley Street be extended to the eastern side of Edgeware Road, Newtown between Wells Street and Darley Street to be consistent with the adjoining streets.**

**For motion: Unanimous**

**LTC0421(1) Item 12 Piper Street and White Street, Lilyfield - Proposed 'No Stopping' restrictions (Baludarri-Balmain Ward/ Balmain Electorate/ Leichhardt PAC)**

### **SUMMARY**

Council has received concerns from residents regarding vehicles obstructing sight lines and manoeuvring space by parking too close to the intersection of Piper Street and White Street, Lilyfield. An investigation has now been completed and is presented in this report.

### **Officer's Recommendation**

THAT the following statutory restrictions be signposted:

1. 10m 'No Stopping' zone on the western side of White Street, north of Piper Street;
2. 10m 'No Stopping' zone on the northern side of Piper Street, west of White Street.
3. 10m 'No Stopping' zone on the northern side of Piper Street, east of White Street.

### **DISCUSSION**

The Committee members agreed with the Officer's recommendation.

### **COMMITTEE RECOMMENDATION**

THAT the following statutory restrictions be signposted:

1. 10m 'No Stopping' zone on the western side of White Street, north of Piper Street;
2. 10m 'No Stopping' zone on the northern side of Piper Street, west of White Street.
3. 10m 'No Stopping' zone on the northern side of Piper Street, east of White Street.

**For motion: Unanimous**



**LTC0421(1) Item 13 Proposed Landscaping Works - Railway Parade, Annandale  
(Baludarri-Balmain Ward/ Balmain Electorate/ Leichhardt PAC)**

**SUMMARY**

Transport for NSW (TfNSW) are proposing to convert ten (10) existing parking spaces on the grassed area of Railway Parade, Annandale (opposite to properties between No.70-72 and No.62 Railway Parade) into a landscaped area. Existing parking restrictions at this location are 'No Parking 7am-7pm vehicles under 4.5t GVM excepted' and it is proposed to be changed to 'No Parking' restrictions as part of this initiative to plant trees to allow for a visual screening to the new Whites Creek Link bridge. Consultation undertaken indicated strong support for the removal of parking.

**Officer's Recommendation**

THAT the existing ten (10) angle parking spaces on the northern side of Railway Parade between Whites Creek and Railway Parade (opposite properties between No.70-72 and No.62 Railway Parade) be converted from 'No Parking 7am-7pm vehicles under 4.5t GVM excepted' to 'No Parking' restrictions.

**DISCUSSION**

The Committee members agreed with the Officer's recommendation.

**COMMITTEE RECOMMENDATION**

**THAT the existing ten (10) angle parking spaces on the northern side of Railway Parade between Whites Creek and Railway Parade (opposite properties between No.70-72 and No.62 Railway Parade) be converted from 'No Parking 7am-7pm vehicles under 4.5t GVM excepted' to 'No Parking' restrictions.**

**For motion:** Unanimous

**LTC0421(1) Item 14 Draft Car Share Policy (All Wards / All Electorates / All PACs)**

**SUMMARY**

At its meeting on Tuesday 9 March 2021, Council endorsed the public exhibition of a proposed Car Share policy for the Inner West. The draft Policy is now available for comment via Council's website.

The policy outlines procedures and criteria for assessing new applications for car share parking on public streets or in council car parks. Consultation with car share companies, relevant staff and some community has been carried out throughout development of the draft policy.

**Officer's Recommendation**

THAT this report be received and noted.

**DISCUSSION**

The Committee members agreed with the Officer's recommendation.

## **COMMITTEE RECOMMENDATION**

**THAT this report be received and noted.**

**For motion:** Unanimous

## **General Business**

### **LTC0421 Item 15 Parking and traffic management in The Boulevarde, Dulwich Hill**

A resident of The Boulevarde has requested, through Cllr Macri, for:

- 2P Resident Parking to be investigated again for The Boulevard, between Eltham Street and Piggott Street, due to parking being utilised by staff and students from Christian Brothers High School;
- centre road lines marked on The Boulevarde to prevent drivers travelling on the wrong side of the road when manoeuvring through the curves in the road; and
- installation of traffic calming devices on The Boulevarde to prevent speeding.

Council Officers will investigate these requests.

### **LTC0421 Item 16 Request for 'No Stopping' or 'No Parking' signage on Arthur Street, Marrickville**

A request has been received from a resident of Livingstone Road, through Cllr Macri, for 'No Stopping' or 'No Parking' signage to be installed on Arthur Street at the corner of the unnamed laneway. The resident stated that vehicles park illegally and encroach the entry of the unnamed laneway from Arthur Street, limiting sightlines for drivers entering and exiting the laneway and requiring numerous reversing manoeuvres to enter the laneway to access garages. Council Officers will investigate this request.

### **LTC0421 Item 17 Traffic management in Styles Street, Leichhardt**

A resident of Styles Street, Leichhardt, has raised concern about pollution and noise levels caused by increased general traffic and heavy vehicles in the street. The resident has requested, through Cllr da Cruz, that traffic management in Styles Street be investigated.

### **LTC0421 Item 18 Resurfacing of Lilyfield Road, Lilyfield**

The IWBC representative requested that Lilyfield Road be resurfaced, particularly the section east of Catherine Street. This matter was raised about a year ago after WestConnex works for the Rozelle Interchange was completed on the road, however action has not yet been taken. Council Officers will pass this request onto the appropriate authority.

This concluded the business of the meeting.

**Item No:** LTC0521(1) Item 1

**Subject:** WESTCONNEX M5 – ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2: PROPOSED CYCLEWAY EXTENSION AT BEDWIN ROAD (MIDJUBURI - MARRICKVILLE AND DAMUN – STANMORE WARD / SUMMER HILL AND NEWTOWN ELECTORATE / INNER WEST PAC)

**Prepared By:** Stephen Joannidis - Urban Amenity Improvement - Delivery Manager

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

As part of Westconnex Planning Condition (B51, Pedestrian and Cycle Implementation Strategy), Council has engaged a Consultant to design and construct the connection between Council's existing bicycle network on Edgeware Road, Darley Street and Lord Street to the newly constructed Bedwin Road Bridge. The design will provide:

- A dedicated off-road cycle path as part of the newly constructed Bedwin Road Bridge and Council's existing bicycle network on Darley Street, Lord Street and Edinburgh Road,
- Upgrades to the signalised intersection of Edgeware Road, Edinburgh Road and Bedwin Road, and
- Upgraded infrastructure to the existing network on Edgeware Road and Darley Street.

Council is seeking the approval of the Local Traffic Committee to progress with construction of the measures contained herein.

## RECOMMENDATION

**THAT the detailed design plans from Certus Projects (drawing no's 0224-RP-003, 0224-RD-0101 and 0224-RD-0102 dated 16 April 2021) be approved for construction commencement.**

## BACKGROUND

The proposed cycle network connection forms part of the planning approval of the Westconnex M5 St Peters Interchange Active Transport Works. The purpose of the active transport works is to provide the safe movement of pedestrians, cyclists and motorists while improving the active transport network.

The recently constructed Bedwin Road Bridge will provide a dedicated off-road cycle facility from St Peters to Marrickville, however, the proposed works will complete the connection from the Bridge to Council's existing bicycle network.

## FINANCIAL IMPLICATIONS

The project is fully funded by Transport for NSW to improve safety outcome for cyclists, pedestrians, and motorists.

## OFFICER COMMENTS

With the Bedwin Road Bridge works nearing completion, the proposed design will provide a crucial link with a dedicated bicycle network between Edinburgh Road and Sydney Steel Road, Marrickville and local cycle routes in Newtown.

The Consultant has prepared detailed design documentation that includes:

- The widening of the footpath between Bedwin Road Bridge and Edgeware Road
- Modifications to the traffic signals at Edgeware Road and Edinburgh Road (TCS 4898) including:
  - Incorporation of bicycle lanterns to the northern crossing leg on Edgeware Road,
  - Replacing the existing zebra crossing on the slip lane from Edinburgh Road to Edgeware Road with a signalised crossing with pedestrian and bicycle lanterns,
  - Widening of kerb ramps,
  - Removal of on-street cycle lane marking on Edgeware Road.
- New islands and kerb adjustments on Edgeware Road between Darley Street and Lord Street, and
- New line marking and signage to support the dedicated cycle lanes, pedestrian access and changed operational conditions.

## PUBLIC CONSULTATION

As part of the design documentation development, stakeholder consultation was undertaken with Council hosting meetings between the Consultant and user groups with this feedback considered in the design.

## CONCLUSIONS

It is recommended that the committee support the proposal for the construction of the bicycle network link between Bedwin Road Bridge and Edgeware Road, Darley Street and Lord Street.

## ATTACHMENTS

1. [WCX M5 SPIATW -P2-CP03-0224-RD-100% Civil COMBINED](#)
2. [WCX-M5-SPIATW-P2-CER2101 RSA - Bedwin Bridge Cycle Path - Second RSA LE Comments 20210428](#)
3. [WCX-M5-SPIATW-P2-Cyclway Extension Detailed Design Compliance Memo](#)

## CERTUS PROJECTS

### WESTCONNEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2

### PROPOSED CYCLEWAY EXTENTION AT BEDWIN RD

### 100% DETAILED DESIGN

#### DRAWING SCHEDULE

NUMBER	DESCRIPTION
0224-RD-0001	COVER SHEET AND DRAWING SCHEDULE
0224-RD-0002	GENERAL NOTES
0224-RD-0003	DESIGN SCHEDULES AND LEGEND
0224-RD-0101	GENERAL ARRANGMENT PLAN
0224-RD-0102	SIGNAGE PLAN
0224-RD-0103	DEMOLITION PLAN
0224-RD-0301	LONGITUDINAL SECTION
0224-RD-0401	CROSS SECTIONS
0224-RD-0402	CROSS SECTIONS
0224-RD-0403	CROSS SECTIONS

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FROM A SMALL SIZE ORIGINAL

DRAWING FILE LOCATION / NAME C:\125\Roads\Sydney\0204 Certus Inner West Council Ordinance\04 CAD\Drawings\0224-RD-0001.dwg										PLOT DATE / TIME 16/04/2021 7:19:40 PM			PLOT BY Charles Green			DRAWINGS / DESIGN PREPARED BY Designer			CLIENT		TITLE WESTCONNEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2			A3
EXTERNAL REFERENCE FILES										REV	DATE	AMENDMENT / REVISION DESCRIPTION					APPROVAL	SCALE ON A3 SIZE DRAWING		TITLE	NAME	DATE		
										0	25/06/2020	BIM ED FOR CONCEPT DESIGN								DRAWING	M. JOYCE	16/04/21		
										1	16/04/2021	BIM ED FOR 100% DETAILED DESIGN								ORG CHECK	G. DUFFELL	16/04/21		
																				DESIGN	G. GREENAWAY	16/04/21		
																				DESIGN CHECK	G. DUFFELL	16/04/21		
																				DESIGN MINOR	G. DUFFELL	16/04/21		
																				PROJECT MINOR	G. DUFFELL	16/04/21		
																							</	

## GENERAL NOTES

- LEVELS ARE TO AUSTRALIAN HEIGHT DATUM (AHD).
- CO-ORDINATES ARE TO MAP GRID AUSTRALIA (MGA) CO-ORDINATE SYSTEM (ZONE 56), GDA94.
- ALL DIMENSIONS ARE IN METRES U.N.O. ALL LEVELS, CHAINAGES, STATIONS AND CO-ORDINATES ARE EXPRESSED IN METRES.
- SURVEY DATA WAS OBTAINED FROM - 200236-DT-UT (2020-07-22).
- UTILITY DATA WAS OBTAINED FROM - TEA PSF CD 054.
- UTILITY INFORMATION SHOWN ON THE DRAWINGS DOES NOT DEPICT ANY MORE THAN THE PRESENCE OF A SERVICE BASED ON AVAILABLE DOCUMENTARY EVIDENCE. THE PRESENCE OF A UTILITY SERVICE, ITS SIZE AND LOCATION SHOULD BE CONFIRMED BY FIELD INSPECTION AND POTHOLING PRIOR TO THE COMMENCEMENT OF ROADWORKS, AND THE RELEVANT UTILITY PLANS SHOULD BE OBTAINED BY DIALING 1100 OR FAXING 1300 652 077 (DIAL BEFORE YOU DIG). CAUTION SHOULD BE EXERCISED WHEN WORKING IN THE VICINITY OF ALL UTILITY SERVICES.
- STOP VALVE FITTING ADJUSTMENT TO BE DONE IN-LINE WITH SYDNEY WATER GUIDELINES.
- NO LIGHTING DESIGN HAS BEEN UNDERTAKEN.
- SYDNEY WATER APPROVAL IS REQUIRED FOR PROTECTION OF THEIR ASSET AND PILING ADJACENT TO THE BRICK SEWER IN THE TUNNEL.
- TRANSGRID APPROVAL IS REQUIRED FOR WORKS OVER THE PROPOSED 330kV CONDUITS/CABLES.
- SYDNEY METRO APPROVAL IS REQUIRED FOR PROTECTION OF THEIR ASSET, WORKS AND PILES WITHIN THE 1st AND 2nd RESERVE AREAS.
- SIGNAGE AND LINEMARKING PLANS MUST BE READ IN CONJUNCTION WITH TCS PLANS.
- ALL SIGNAGE MUST BE CLEAR OF ALL VEGETATION AND OBSTRUCTIONS.
- INTERFACE COORDINATION IS REQUIRED WITH BRIDGE CONTRACTOR AND DESIGNER FOR CONNECTION OF CYCLEWAY.
- REFER TO DESIGNER FOR 12D SETOUT FILE FOR PROJECT.

THE FOLLOWING ITEMS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE RELEVANT STANDARD DRAWINGS LISTED.	
ITEM	STANDARD TO REFERENCE
KERBS AND MEDIAN ISLANDS	INNER WEST COUNCIL STANDARD ROADS DRAWING R1 (V1)
PRAM RAMP	INNER WEST COUNCIL STANDARD FOOTPATH DRAWING F4 (V1)
FOOTPATHS	INNER WEST COUNCIL STANDARD FOOTPATH DRAWING F2 (V1)
CYCLE PATHS PAVEMENT AND JOINTING	INNER WEST COUNCIL STANDARD CYCLEWAY DRAWING C1 (V1)
PAVEMENT MARKING	RMS DELINEATION GUIDELINES AND RMS QA SPECIFICATIONS R142 AND R145
MESSAGES ON PAVEMENT	RMS DELINEATION GUIDELINE SECTION 9
SIGNAGE	AS1742

CONTROL LINE MK01 - HORIZONTAL SEGMENTS									
APPROACH SEGMENT	DEPARTURE SEGMENT	TANGENTIAL	CHAINAGE	COORDINATES			APPROACH BEARING	DEPARTURE BEARING	APPROACH RADIUS
				X	Y	Z			
	LINE		0.000	331261.811	6246483.147	10.182		332°53'37.00"	
LINE	ARC	YES	34.023	331246.309	6246513.433	7.384	332°53'37.00"	332°53'37.00"	30.000
ARC	LINE	YES	50.584	331243.107	6246529.468	6.890	4°31'27.42"	4°31'27.42"	30.000
LINE			59.522	331243.812	6246538.378	6.556	4°31'27.42"		

## CONCRETE NOTES:

- PLACING AND CURING OF CONCRETE SHALL COMPLY WITH AS5100.5
- CONCRETE EXPOSURE CLASSIFICATION B1.
- EDGE SHALL BE CHAMFERED 20x20 AND RE-ENTRANT ANGLES FILLETED 20x20 UNO.
- CONCRETE GRADE AND NOMINAL COVER TO REINFORCEMENT (UNO):  
CAST AGAINST GROUND: N32/85mm  
ELSEWHERE: N32/55mm

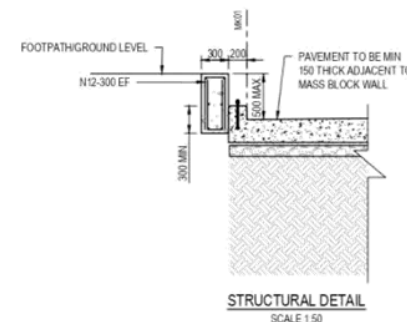
## REINFORCEMENT NOTES:

- ALL REINFORCEMENT SHALL COMPLY WITH AS 4671 AND D500N
- COGS, HOOKS, SPLICES AND PIN DIAMETERS SHALL BE IN ACCORDANCE WITH AS5100.5.
- REINFORCEMENT UNO MAY BE DISPLACED SLIGHTLY WHERE NECESSARY TO CLEAR STEEL DOWELS ANCHOR BOLTS, FORMED HOLES AND RECESSES.
- UNLESS OTHERWISE SPECIFIED, THE MINIMUM DEVELOPMENT LENGTHS AND LENGTHS OF LAPS SHALL BE AS FOLLOWS:

BARB SIZE:	N12
(A) HORIZONTAL BARS >300mm OF CONCRETE CAST BELOW THE BAR	500
(B) OTHER BARS	350

## REINFORCEMENT NOTES (CONTINUED):

- WHERE LAPPED SPLICES ARE USED, THE SEPARATION BETWEEN PORTIONS OF BARS SHALL NOT EXCEED 3 x BAR SIZE.
- LAP LENGTHS FOR UNEQUAL BAR SIZES MUST BE BASED UPON THE SMALLER BAR DIAMETER UNO.7
- WHERE REBATES OR RECESSES ARE REQUIRED, MINIMUM CLEAR COVER TO THE REINFORCEMENT SHALL BE MAINTAINED.
- REINFORCEMENT IS SHOWN DIAGRAMMATICALLY. IT IS NOT NECESSARILY SHOWN IN TRUE PROJECTION.
- SPLICES OF REINFORCEMENT SHALL ONLY BE MADE IN POSITIONS SHOWN.
- WHERE LAPS ARE NOT SHOWN, THEY SATISFY THE REQUIREMENTS OF SPECIFICATION R53.
- DO NOT WELD REINFORCEMENT UNLESS SHOWN ON DRAWING.



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DRAWING FILE LOCATION/NAME  
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EXTERNAL REFERENCE FILES

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0	25/06/2020	ISSUED FOR CONCEPT DESIGN			
1	21/03/2021	ISSUED FOR DRAFT WORK DETAILED DESIGN			
2	16/04/2021	ISSUED FOR 100% DETAIL DESIGN			

TITLE	NAME	DATE
DRAWN	M. JOYCE	16/04/21
DRG CHECK	G. DUFFELL	16/04/21
DESIGN	G. DUFFELL	16/04/21
DESIGN CHECK	G. DUFFELL	16/04/21
DESIGN MINOR	G. DUFFELL	16/04/21
PROJECT MANAGER	G. DUFFELL	16/04/21

COORDINATE SYSTEM	HIGHEST DATUM
MGA ZONE 56	AHD

DESIGNER	DESIGNER
TURNBULL ENGINEERING	

CUSTOMER	TITLE	SHEET NO.	SHEET TOTAL
CERTUS PROJECTS	WESTCONEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2	RD-0002	1 OF 1
INNER WEST	CERTUS PROJECTS PROPOSED CYCLEWAY EXTENSION AT BEDWIN RD GENERAL NOTES	RD-0002	1 OF 1



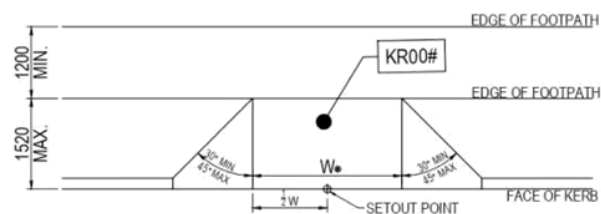
### GENERAL NOTES

LABEL	SETOUT POINT		WIDTH (MEASURED PERPENDICULAR) "W"
	EASTING	NORTHING	
KR001	31220 0561	6246532 955	2.56
KR002	331223 226	6246536 682	3.45
KR003	331218 319	6246536 698	3.48
KR004	331214 309	6246539 515	3.46
KR005	331238 381	6246533 719	3.41
KR006	331246 169	6246540 384	4.40
KR007	331249 501	6246534 917	1.50

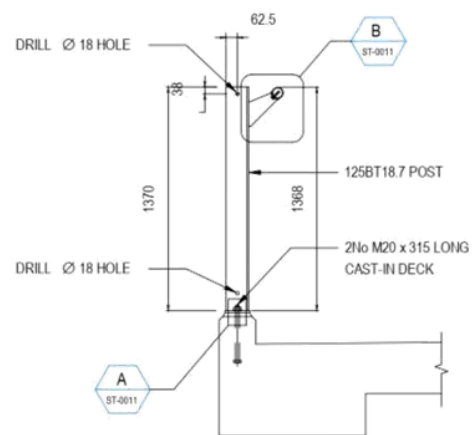
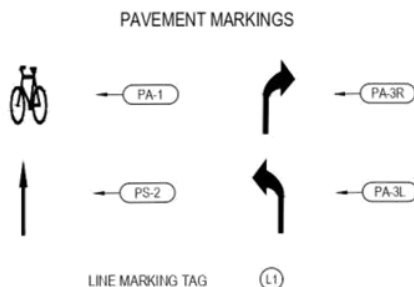
KERB RAMPS ADJUSTMENTS ON KR001 TO KR005 ARE BASED ON TCS PLAN 4898 ISSUE B  
DATED 16/02/2021

BALUSTRADE TABLE				TABLE REFERENCE
TYPE	LENGTH (mm)	MOVE	NUMBER OF	
BT6	1885	2.71	14	TABLE 1
BT8	1995	1.87*	2	TABLE 1
BT12	290	1.87*	1	TABLE 2
BT13	2260	0.85*	2	TABLE 1

REFER TO STRUCTURAL DRAWING 0224-ST-0012 FOR DETAILS. NOTE THESE ARE ADDITIONAL UNITS OF BALUSTRADE ON TOP OF THOSE SPECIFIED IN THE STRUCTURAL DRAWINGS


























KERB RAMP SETOUT  
N.T.S



TYPICAL STEEL POST

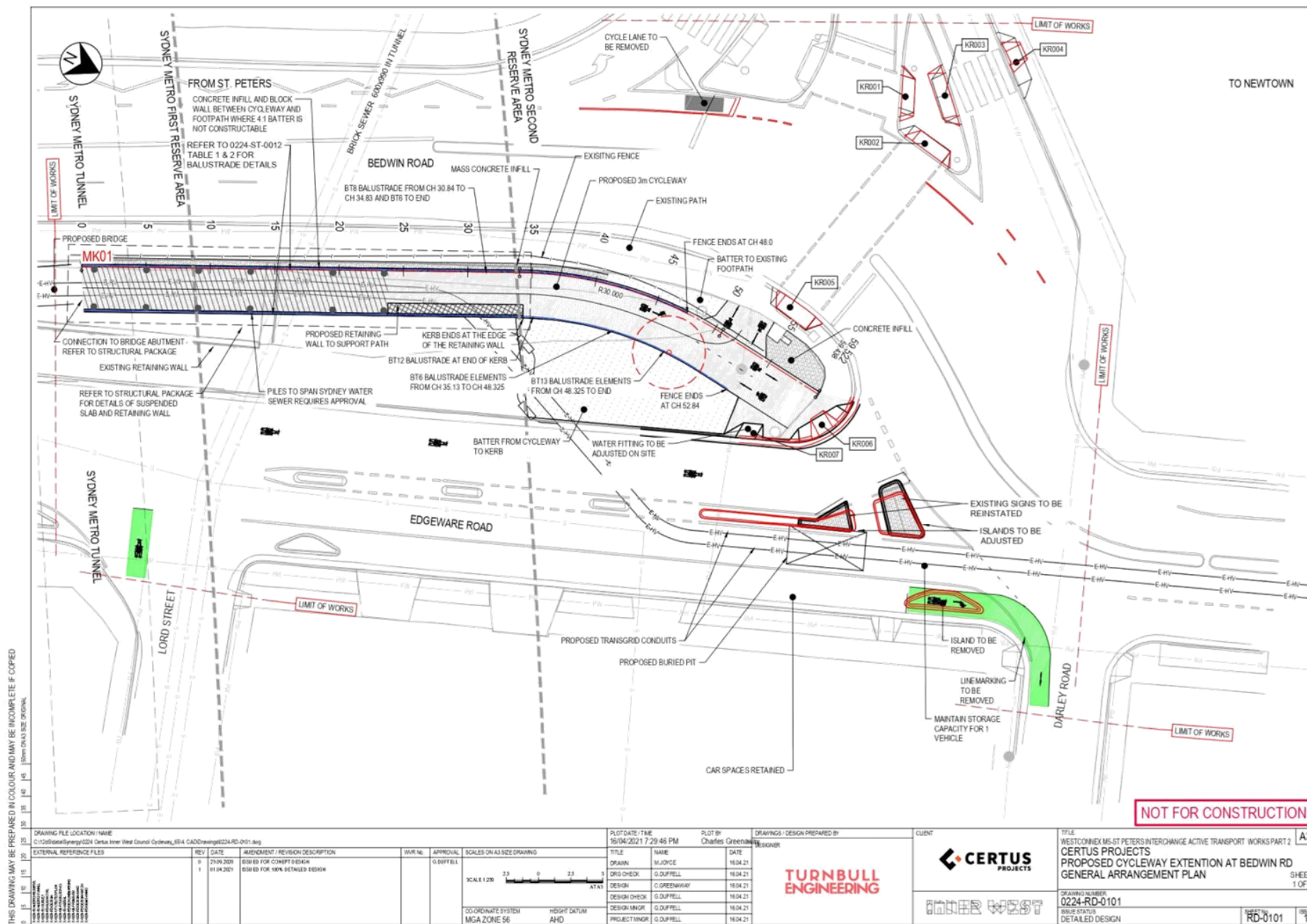
DIMENSIONS AND ANNOTATIONS SYMMETRICAL ABOUT  
CENTRE LINE DECK / APPROACH SLAB  
N.T.S

LEGEND

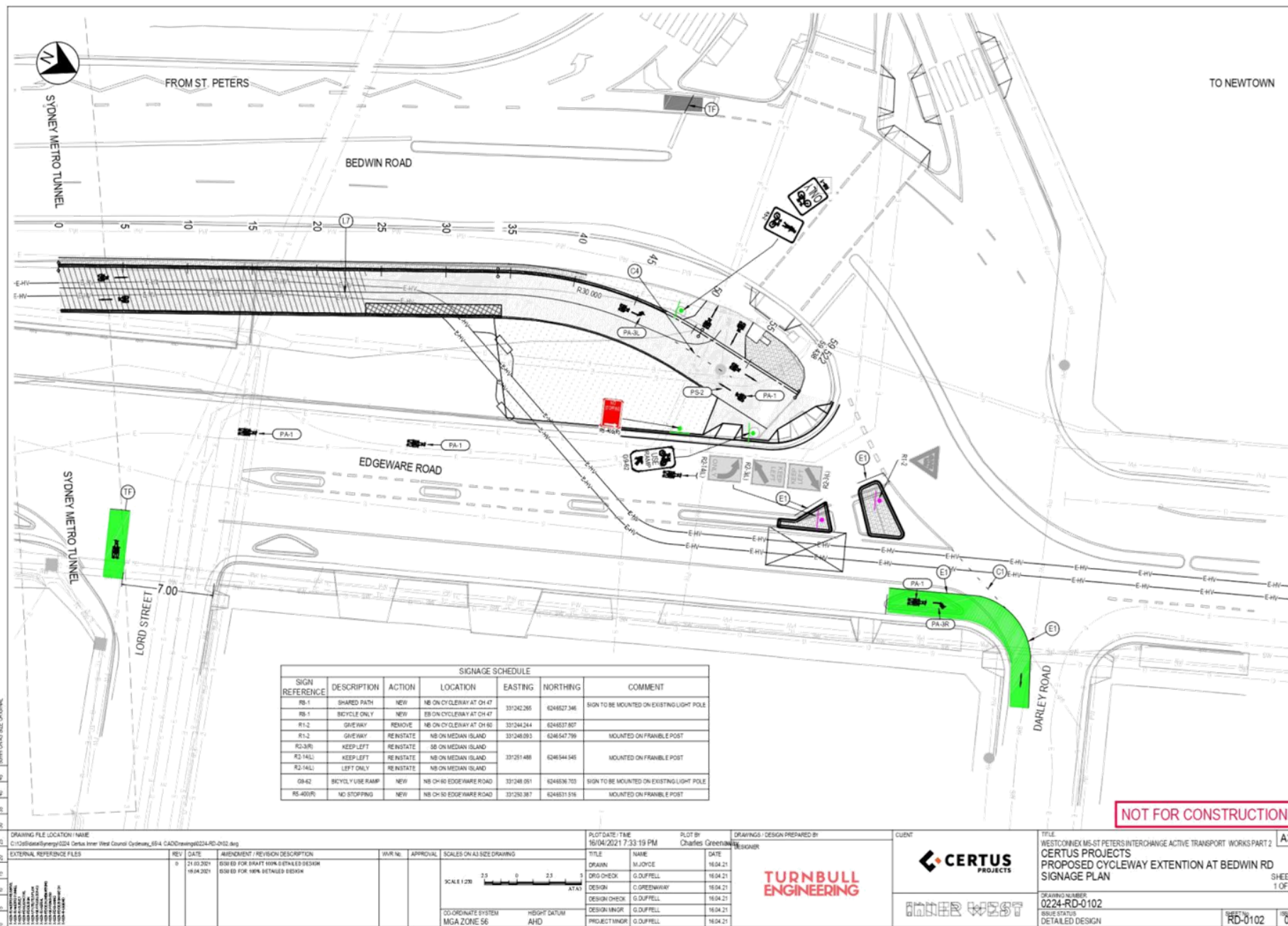
- |   |  |
|---|--|
|    | SURVEY                                       |
|    | DESIGN                                       |
|    | SYDNEY METRO FIRST RESERVE EXTENTS           |
|    | SYDNEY METRO SECOND RESERVE EXTENTS          |
|    | EXISTING UNDERGROUND ELECTRICAL LINE         |
|    | EXISTING GAS MAIN                            |
|    | EXISTING SEWER MAIN                          |
|    | EXISTING TELECOMMUNICATIONS LINE             |
|    | EXISTING POTABLE WATER MAIN                  |
|    | EXISTING STORM WATER MAIN                    |
|    | PROPOSED STRIGRID CONDUITS                   |
|    | PROPOSED FENCE                               |
|    | APPROXIMATE PILE LOCATION                    |
|    | CYCLEWAY                                     |
|    | 4:1 BATTER                                   |
|    | CONCRETE                                     |
|    | RETAINING WALL - REFER TO STRUCTURAL PACKAGE |
|    | SUSPENDED SLAB - REFER TO STRUCTURAL PACKAGE |
|    | EXISTING SIGN POST TO BE REMOVED             |
|    | RELOCATED EXISTING SIGN POST                 |
|  | PROPOSED SIGN POST                           |
|  | PROPOSED SIGN FACE                           |
| R5-400(R)   | SIGN REFERENCE NUMBER                        |
|  | RELOCATED SIGN FACE                          |
| R5-400(L)<br>(RELOCATED)  |  |

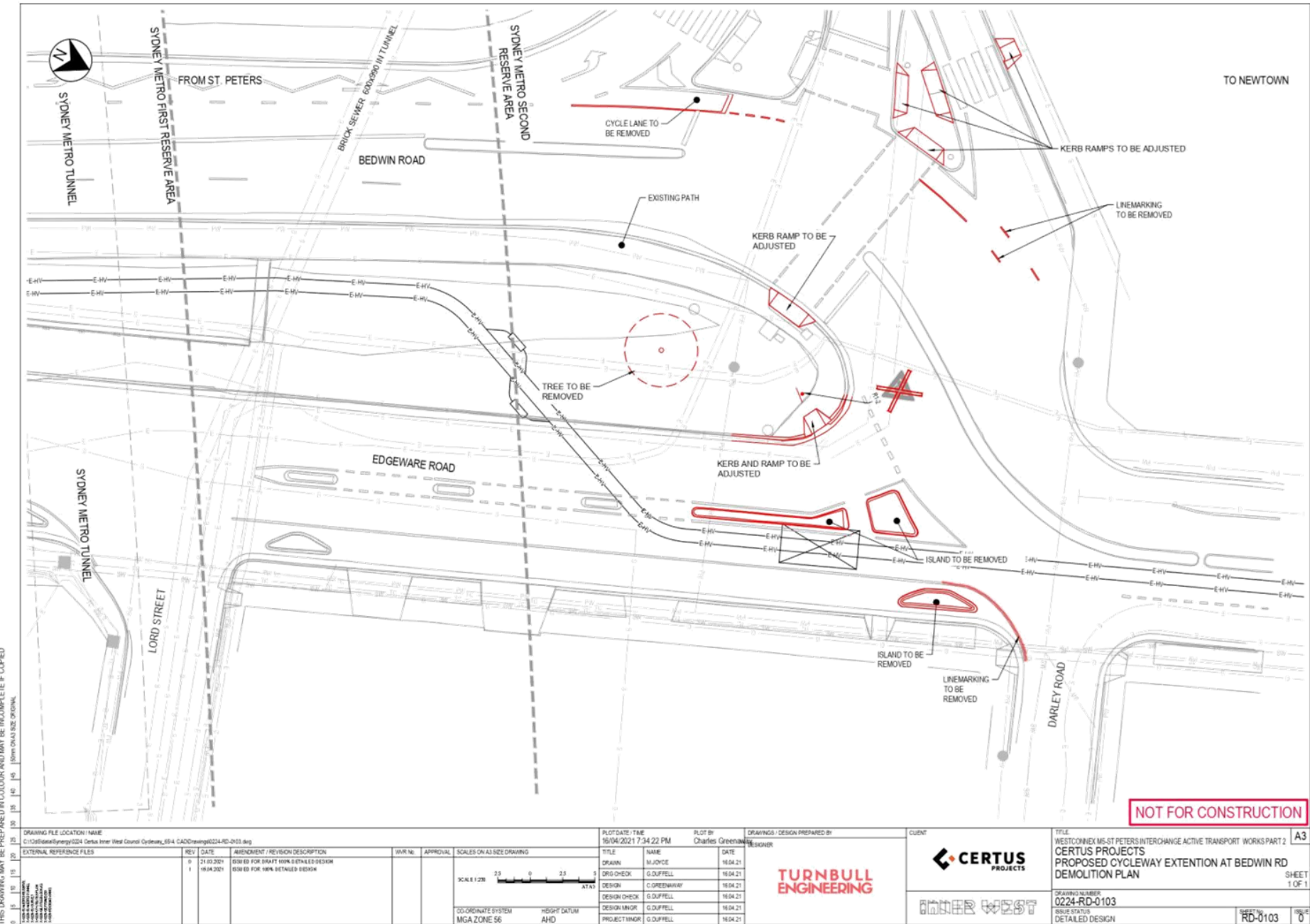
NOT FOR CONSTRUCTION

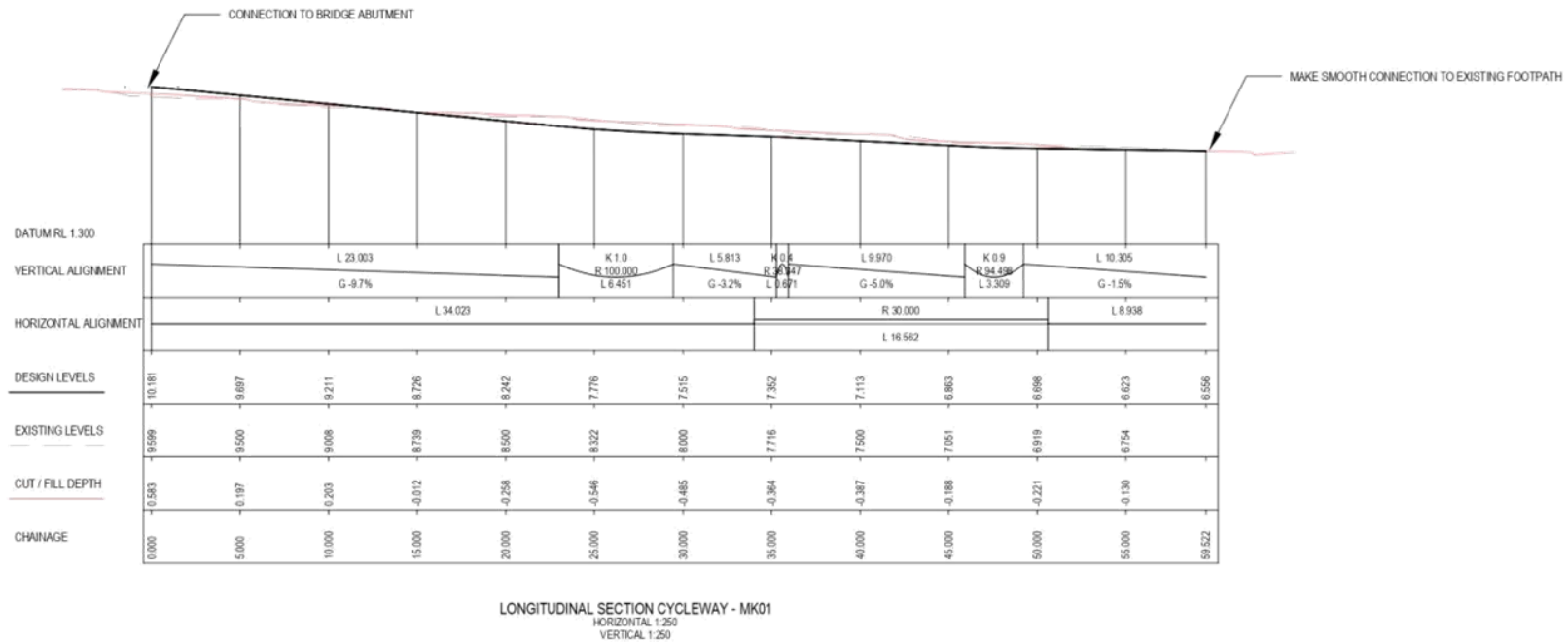
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		ISSUED FOR 100% DETAILED DESIGN

APPROVAL	DATE	NAME	POSITION
G. DUFFELL	16/04/2021	G. DUFFELL	DESIGNER
G. DUFFELL	16/04/2021	G. DUFFELL	DESIGN CHECK
G. DUFFELL	16/04/2021	G. DUFFELL	DESIGN MINOR
G. DUFFELL	16/04/2021	G. DUFFELL	PROJECT MANAGER

SCALE	ON A3 SIZE DRAWING
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HIGHEST DATUM	AHD

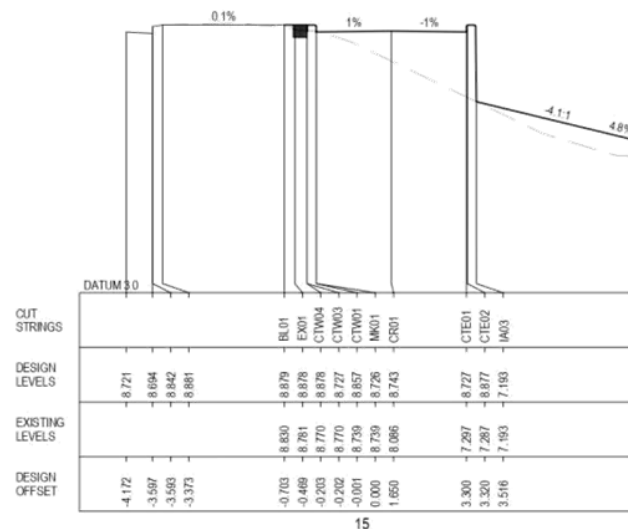
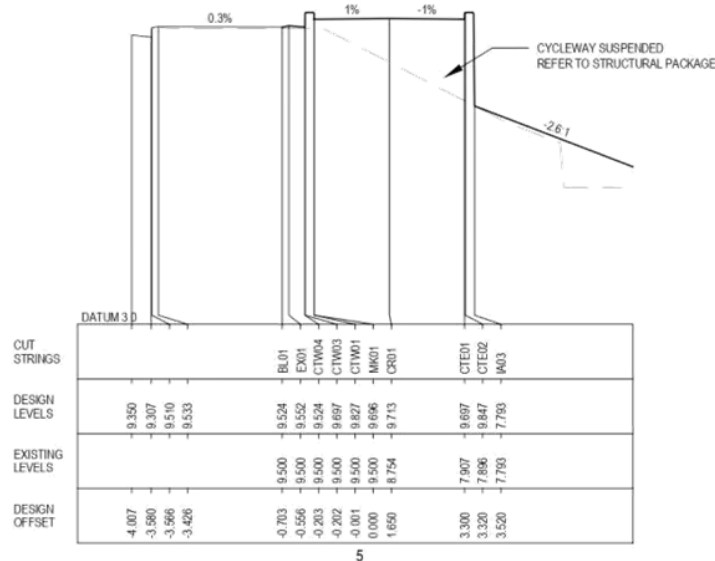
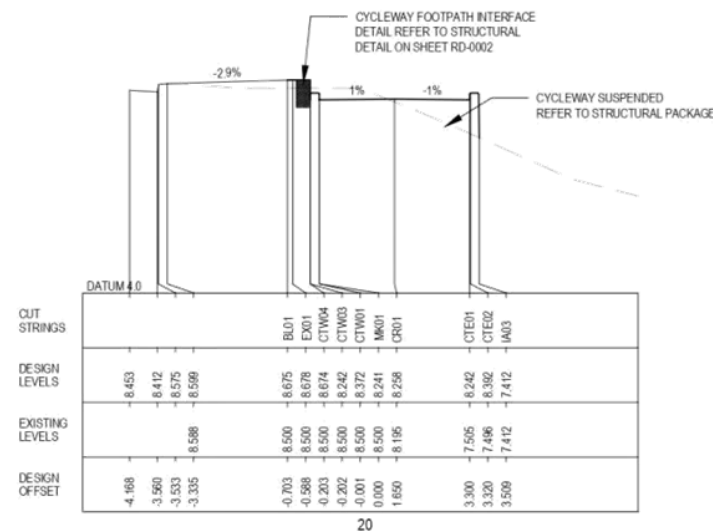
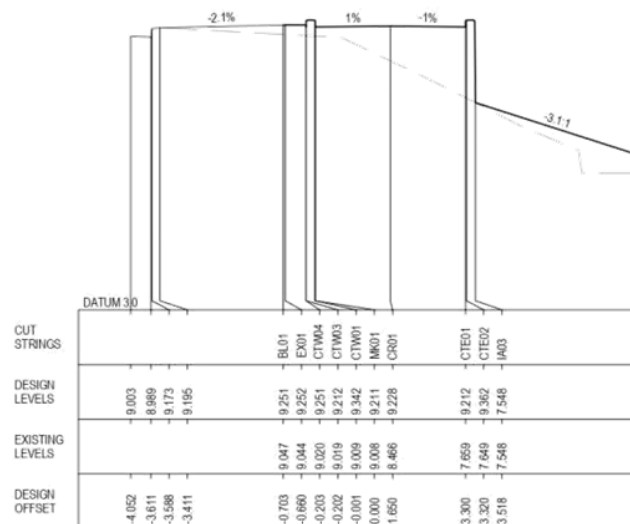
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DESIGN CHECK	G. DUFFELL	16/04/21
DESIGN MINOR	G. DUFFELL	16/04/21
PROJECT MANAGER	G. DUFFELL	16/04/21

DATE	TIME	BY	FOR
16/04/2021	7:35:26 PM	Charles Green	DESIGNER

CUSTOMER	PROJECT
CERTUS PROJECTS	PROPOSED CYCLEWAY EXTENSION AT BEDWIN RD LONGITUDINAL SECTION

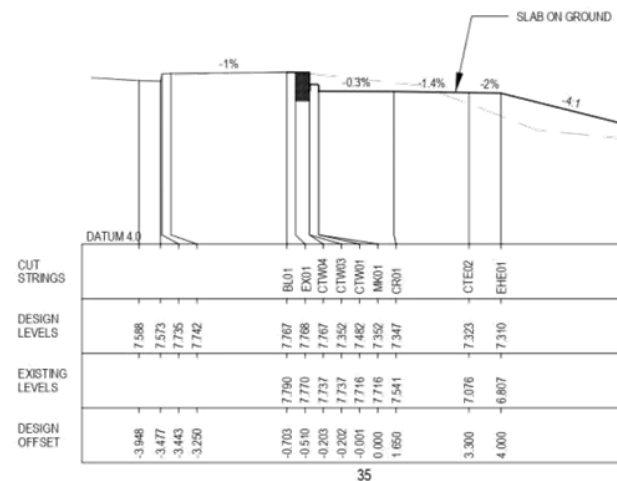
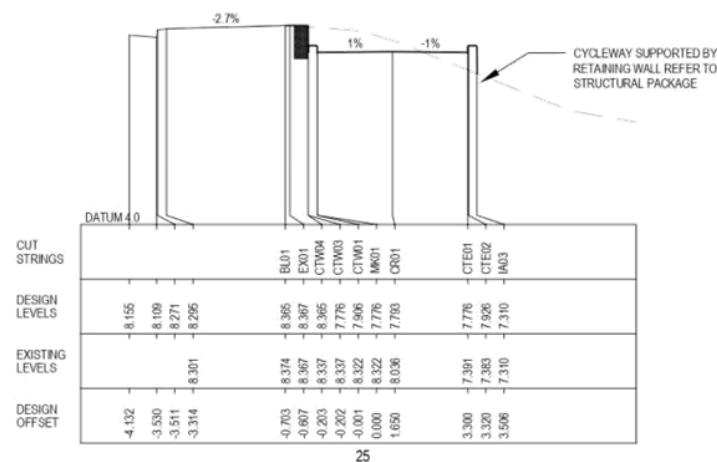
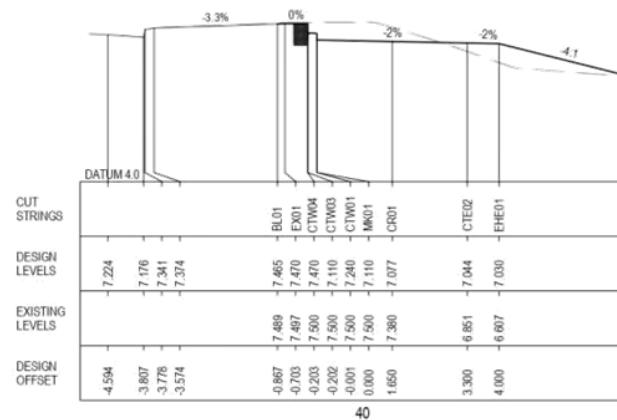
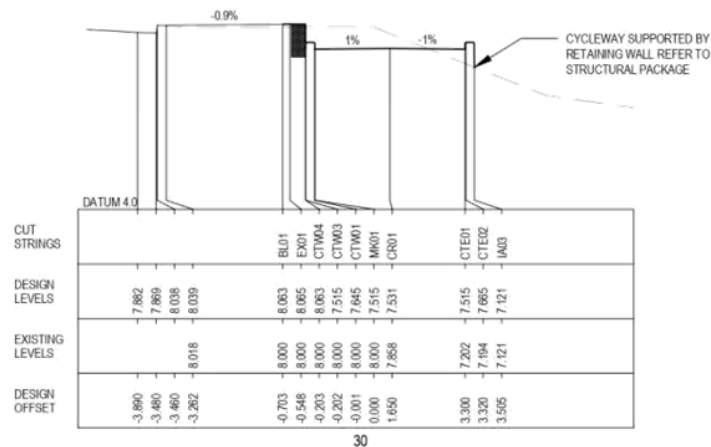
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0224-RD-0301	1	2
STATUS	DATE	BY
DETAILED DESIGN	16/04/21	G. DUFFELL

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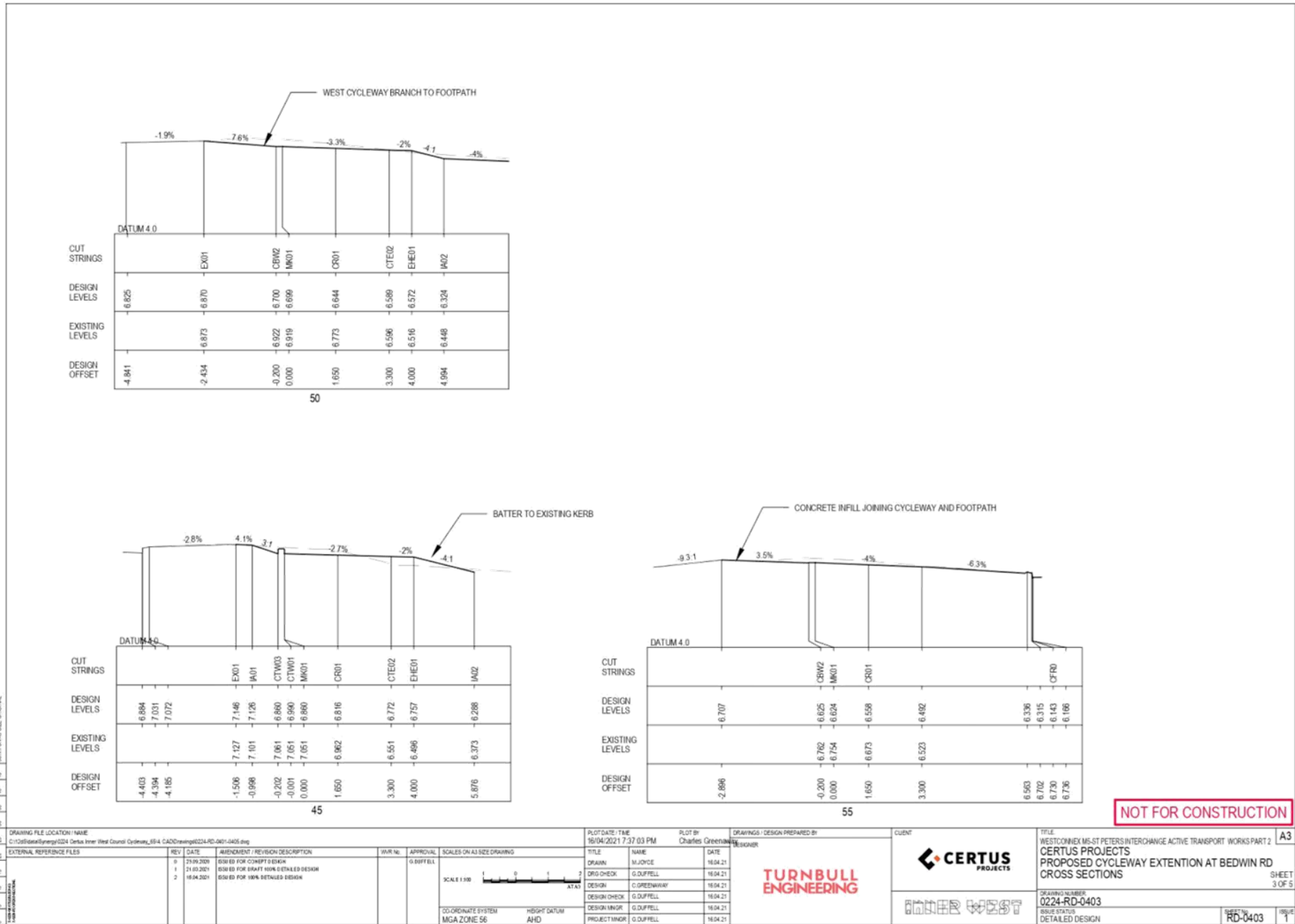
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EXTERNAL REFERENCE FILES				REV DATE AMENDMENT / REVISION DESCRIPTION 1 25.03.2020 ISSUED FOR CONCEPT DESIGN 2 21.03.2021 ISSUED FOR SHARV 10% DETAIL DESIGN 3 16.04.2021 ISSUED FOR 10% DETAIL DESIGN		SWR NO. APPROVAL 21.03.2021		SCALE ON A3 SIZE DRAWING 0.50 FT/1 BL		TITLE NAME DATE DRAWN M. JOYCE 16.04.21 DWD CHECK G. DUFFELL 16.04.21 DESIGN G. GREENHAWAY 16.04.21 DESIGN CHECK G. DUFFELL 16.04.21 DESIGN MGR G. DUFFELL 16.04.21 PROJECT MGR G. DUFFELL 16.04.21					
COORDINATE SYSTEM MGA ZONE 56				HEIGHT DATUM AHD				DRAWING NUMBER 0204-RD-0402		ISSUE STATUS DETAILED DESIGN		RD-0402		1	



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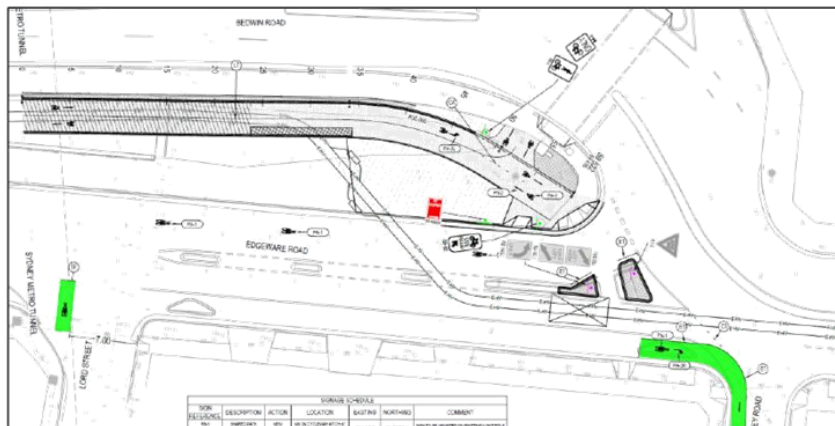
LE 22/04/2021

Item 1



## Road Safety Audit

### Bedwin Bridge Rd Cycle Path



Attachment 2

Traffic Engineering

Transport Planning

Data Analysis

Consulting Services



Headway Traffic and Transport Pty Ltd  
ABN: 86 617 026 178

45 Evans Street Balmain NSW 2041  
PO Box 3618 Waremba NSW 2046

1300 187 622  
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[info@headwaytt.com](mailto:info@headwaytt.com)





## Revision History



Issue	Date	Comments
Rev 2	22/04/2021	Issued to Client.
Prepared by:	Richard Thomas	
Reviewed by:	Michael Palamara	
<p><b>File Location:</b> <a href="https://headwaytrafficandtransport.sharepoint.com/sites/projects/shared%20documents/3.templates/road%20safety%20audit%20-%20draft.docx">HTTPS://HEADWAYTRAFFICANDTRANSPORT.SHAREPOINT.COM/SITES/PROJECTS/SHARED DOCUMENTS/3. TEMPLATES/ROAD SAFETY AUDIT - DRAFT.DOCX</a></p>		
<p><b>Date and Time Printed:</b> 22/04/2021 9:04 AM</p>		

Table 1: Revision History



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## References

Austrroads: Guide to Road Safety Audits Part 6 (February 2019)

NSW Centre for Road Safety: Guidelines for Road Safety Audit Practices (July 2011)

## 1 Project Information

**Project Name:** Bedwin Road Cycle Path

**Audit Stage:** Detailed Design Stage

**Previous Audits:** Revision 1 Audit on Draft 100% Detailed Design

**Project Location:** Bedwin Rd near Edgeware Rd, St Peters NSW



Figure 1: Project Area - Source: Six Maps

### 1.1 Project Description

The Bedwin Rd bridge is currently being upgraded as an early works activity for Sydney Metro, to provide improved accessibility for cycling. A new bridge is being constructed on the east side of Bedwin Rd rail over bridge specifically for cyclist use. On the north side of the bridge, the new cycle path will connect back into the existing road network.

Rev: 2

5  
Ref: CER2001

22/04/2021



## 1.2 Project Objectives

This design stage road safety audit is being conducted to examine the safety implications of the proposed design and to identify any feature that has the potential to be adjusted, deleted or modified in order to improve the safety of the scheme.

This audit has been undertaken in general conformance to the procedure specified in Guide to Road Safety: Part 6A: Implementing Road Safety Audits, 2019 and NSW Centre for Road Safety Guidelines for Road Safety Audit Practices.

## 2 Background Information

### 2.1 Road Safety Auditors

Name	Audit Position	RSA Number
Richard Thomas	Lead Road Safety Auditor	RSA-02-0225
Michael Palamara	Road Safety Auditor	RSA-02-1409

Table 2: Road Safety Auditors

### 2.2 Project Documents and Extent of Road Safety Audit

The following documents have been provided by the Client to be reviewed as part of the Road Safety Audit:

- WESTCONNEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2  
CERTUS PROJECTS PROPOSED CYCLEWAY EXTENTION AT BEDWIN RD – 100% DETAILED  
DESIGN Drawing package
- TCS PLAN 4898 Issue B Sheet X, prepared by B Line Drafting and dated 19/4/21.

Project documents provided for the RSA are collated in **Appendix A: Project Documentation**.

The road safety audit is limited by the extents shown on the provided plans, as well as connection points for users of the proposed scheme to the adjacent road network north of Bedwin Road bridge.



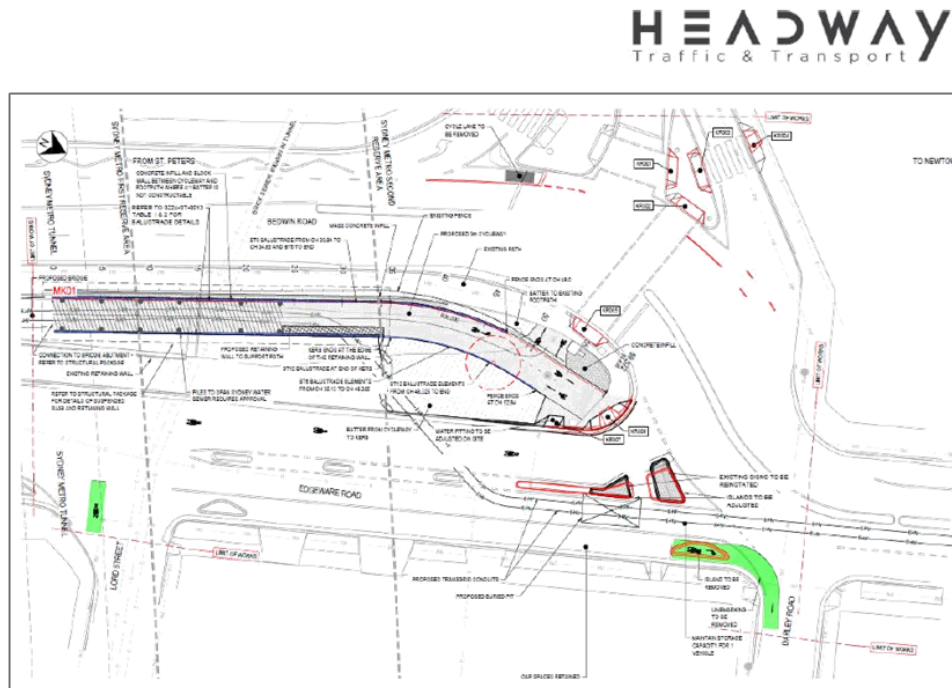


Figure 2: Audit Extents

### 2.2.1 Commencement Meeting

Commencement for the audit was undertaken via email between Michael Palamara (Headway Traffic and Transport) and Liam Elrington (Certus Projects), outlining the audit requirements, scope and timing. No specific issues were identified during the commencement meeting.

### 2.2.2 Day and Night Audit

A day and night audit inspection to inform the audit was completed on Monday 15<sup>th</sup> March 2021, in fine and dry conditions.

## 2.3 Scope of Audit

The RSA has been completed in accordance with the Austroads Guide to Road Safety Part 6 – Managing Road Safety and TfNSW Guidelines for Roads Safety Audit Practices.

The RSA is limited to the audit area and the documents provided by the client identified as identified in **0 Table 2: Road Safety Auditors**

Project Documents and Extent of Road Safety Audit.

## 3 Audit Findings

### 3.1 Identification of Road Safety Issues

While every effort has been made to identify issues associated with potential safety hazards, no guarantee can be made that all issues have been identified.

Items that do not cause a safety issue are not listed. In some circumstances, the auditors may want to include some items as a "Note" for the attention of the client.

The RSA findings are in **Appendix B: Road Safety Audit Findings**.

### 3.2 Risk Ranking

Frequency	Frequency Description
Frequent	Frequent Once or more per week
Probable	Probable Once or more per year (but less than once a week)
Occasional	Occasional Once every five or ten years
Improbable	Improbable Less often than once every ten years

Table 3: How often is the problem likely to lead to a crash?

Severity	Description	Examples
Catastrophic	Likely multiple deaths	High-speed, multi-vehicle crash on a freeway. Car runs into crowded bus stop. Bus and petrol tanker collide. Collapse of a bridge or tunnel.
Serious	Likely death or serious injury	High or medium-speed vehicle/vehicle collision. High or medium-speed collision with a fixed roadside object. Pedestrian or cyclist struck by a car
Minor	Likely minor injury	Some low-speed vehicle collisions. Cyclist falls from bicycle at low speed. Left-turn rear-end crash in a slip lane.
Limited	Likely trivial injury or property damage only	Some low-speed vehicle collisions. Pedestrian walks into object (no head injury). Car reverses into post.

Table 4: What is the likely severity of the resulting crash type?

	Frequent	Probable	Occasional	Improbable
Catastrophic	Intolerable	Intolerable	Intolerable	High
Serious	Intolerable	Intolerable	High	Medium
Minor	Intolerable	High	Medium	Low
Limited	High	Medium	Low	Low

Table 5: Resulting Level of Risk

Risk	Suggested Treatment Approach
Intolerable	Must be corrected.
High	Should be corrected or the risk significantly reduced, even if the treatment costs is high.
Medium	Should be corrected or the risk significantly reduced, if the treatment cost is moderate, but not high
Low	Should be corrected or the risk reduced if the treatment cost is low.

Table 6: Treatment Approach

### 3.2.1 Need for Subsequent Audits

While ultimately be up to the decision of the project, if it is decided to make significant changes to the design, a further audit of the revised design may be appropriate, rather than waiting for the next design stage's audit. This is a particularly important consideration if the project has reached the detailed design stage and is to be built soon.



### 3.3 Responding to the Audit Report

The RSA identifies potential road safety issues and for them to be formally considered by the project in addition to all other project considerations. Each finding or recommendation in the road safety audit report can be responded to by either:

- Accepting it completely and designing a solution to overcome or reduce the problem, in line with the audit findings or recommendation or in another equally effective way
- Accepting it in part or in principle but, due to other constraints, implementing changes which go only part of the way to resolving the safety problem
- Not accepting the finding or recommendation at all.

It is not the responsibility of the road safety auditors to approve or accept any proposed mitigation measures to be employed by the project. Audit findings must be responded to by the project (or the designer) with a written response to each audit finding or recommendation. The response document must be signed by a representative of the project.



## 4 Concluding Statement

We have examined the plans and documents provided as listed in this report. We have inspected the site. The audit has been carried out for the sole purpose of identifying any features of the design which could be altered or removed to improve the safety of the proposal. The identified issues have been noted in this report. The accompanying findings and recommendations are put forward for consideration by the Client for implementation.

### Audit Team Leader

Richard Thomas  
21/4/2021  
RSA-02-0225

Michael Palamara  
21/4/2021  
RSA-02-1409



## Appendix A: Project Documentation

Item 1

Rev: 2

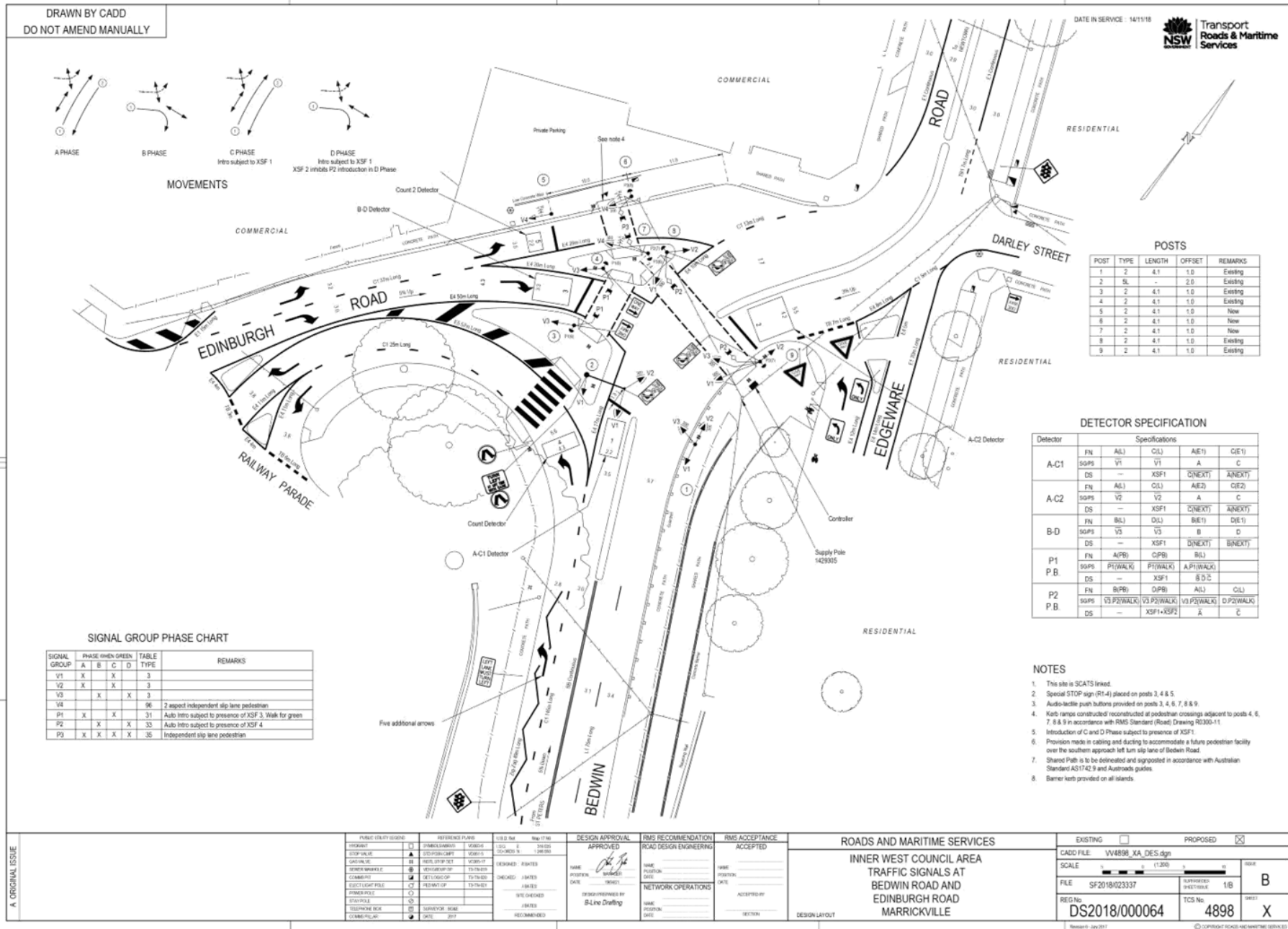
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Ref: CER2001

22/04/2021

Attachment 2



TCS 4898



## CERTUS PROJECTS

### WESTCONNEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2

### PROPOSED CYCLEWAY EXTENTION AT BEDWIN RD

### 100% DETAILED DESIGN

#### DRAWING SCHEDULE

NUMBER	DESCRIPTION
0224-RD-0001	COVER SHEET AND DRAWING SCHEDULE
0224-RD-0002	GENERAL NOTES
0224-RD-0003	DESIGN SCHEDULES AND LEGEND
0224-RD-0101	GENERAL ARRANGMENT PLAN
0224-RD-0102	SIGNAGE PLAN
0224-RD-0103	DEMOLITION PLAN
0224-RD-0301	LONGITUDINAL SECTION
0224-RD-0401	CROSS SECTIONS
0224-RD-0402	CROSS SECTIONS
0224-RD-0403	CROSS SECTIONS

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										1	16/04/2021	ISSUED FOR 100% DETAILED DESIGN									
																</					

## GENERAL NOTES

- LEVELS ARE TO AUSTRALIAN HEIGHT DATUM (AHD).
- CO-ORDINATES ARE TO MAP GRID AUSTRALIA (MGA) CO-ORDINATE SYSTEM (ZONE 56), GDA94.
- ALL DIMENSIONS ARE IN METRES U.N.O. ALL LEVELS, CHAINAGES, STATIONS AND CO-ORDINATES ARE EXPRESSED IN METRES.
- SURVEY DATA WAS OBTAINED FROM - 200236-DT-UT (2020-07-22).
- UTILITY DATA WAS OBTAINED FROM - TEA PSF CD 054.
- UTILITY INFORMATION SHOWN ON THE DRAWINGS DOES NOT DEPICT ANY MORE THAN THE PRESENCE OF A SERVICE BASED ON AVAILABLE DOCUMENTARY EVIDENCE. THE PRESENCE OF A UTILITY SERVICE, ITS SIZE AND LOCATION SHOULD BE CONFIRMED BY FIELD INSPECTION AND POTHOLING PRIOR TO THE COMMENCEMENT OF ROADWORKS, AND THE RELEVANT UTILITY PLANS SHOULD BE OBTAINED BY DIALING 1100 OR FAXING 1300 652 077 (DIAL BEFORE YOU DIG). CAUTION SHOULD BE EXERCISED WHEN WORKING IN THE VICINITY OF ALL UTILITY SERVICES.
- STOP VALVE FITTING ADJUSTMENT TO BE DONE IN LINE WITH SYDNEY WATER GUIDELINES.
- NO LIGHTING DESIGN HAS BEEN UNDERTAKEN.
- SYDNEY WATER APPROVAL IS REQUIRED FOR PROTECTION OF THEIR ASSET AND PILLING ADJACENT TO THE BRICK SEWER IN THE TUNNEL.
- TRANSGRID APPROVAL IS REQUIRED FOR WORKS OVER THE PROPOSED 330kV CONDUITS/CABLES.
- SYDNEY METRO APPROVAL IS REQUIRED FOR PROTECTION OF THEIR ASSET, WORKS AND PILES WITHIN THE 1st AND 2nd RESERVE AREAS.
- SIGNAGE AND LINEMARKING PLANS MUST BE READ IN CONJUNCTION WITH TCS PLANS.
- ALL SIGNAGE MUST BE CLEAR OF ALL VEGETATION AND OBSTRUCTIONS.
- INTERFACE COORDINATION IS REQUIRED WITH BRIDGE CONTRACTOR AND DESIGNER FOR CONNECTION OF CYCLEWAY.
- REFER TO DESIGNER FOR 12D SETOUT FILE FOR PROJECT.

## THE FOLLOWING ITEMS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE RELEVANT STANDARD DRAWINGS LISTED.

ITEM	STANDARD TO REFERENCE
KERBS AND MEDIAN ISLANDS	INNER WEST COUNCIL STANDARD ROADS DRAWING R1 (V1)
PRAM RAMP	INNER WEST COUNCIL STANDARD FOOTPATH DRAWING F4 (V1)
FOOTPATHS	INNER WEST COUNCIL STANDARD FOOTPATH DRAWING F2 (V1)
CYCLE PATHS PAVEMENT AND JOINING	INNER WEST COUNCIL STANDARD CYCLEWAY DRAWING C1 (V1)
PAVEMENT MARKING	RMS DELINEATION GUIDELINES AND RMS QA SPECIFICATIONS R142 AND R145
MESSAGES ON PAVEMENT	RMS DELINEATION GUIDELINE SECTION 9
SIGNAGE	AS1742

## CONTROL LINE MK01 - HORIZONTAL SEGMENTS

APPROACH SEGMENT	DEPARTURE SEGMENT	TANGENTIAL	CHAINAGE	COORDINATES			APPROACH BEARING	DEPARTURE BEARING	APPROACH RADIUS	DEPARTURE RADIUS
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ARC	LINE	YES	50.584	331243.107	6246529.468	6.890	4°31'27.42"	4°31'27.42"	30.000	
LINE			59.522	331243.812	6246538.378	6.556	4°31'27.42"			

## CONCRETE NOTES:

- PLACING AND CURING OF CONCRETE SHALL COMPLY WITH AS5100.5
- CONCRETE EXPOSURE CLASSIFICATION B1.
- EDGE SHALL BE CHAMFERED 20x20 AND RE-ENTRANT ANGLES FILLETED 20x20 UNO.
- CONCRETE GRADE AND NOMINAL COVER TO REINFORCEMENT (UNO):  
CAST AGAINST GROUND: N32/85mm  
ELSEWHERE: N32/55mm

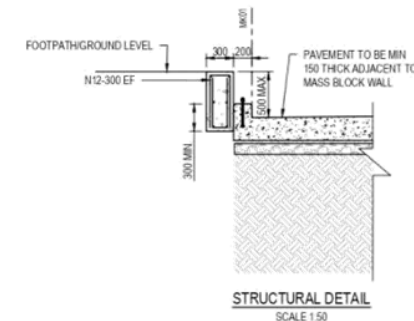
## REINFORCEMENT NOTES:

- ALL REINFORCEMENT SHALL COMPLY WITH AS 4671 AND D500N
- COGS, HOOKS, SPLICES AND PIN DIAMETERS SHALL BE IN ACCORDANCE WITH AS5100.5.
- REINFORCEMENT UNO MAY BE DISPLACED SLIGHTLY WHERE NECESSARY TO CLEAR STEEL DOWELS ANCHOR BOLTS, FORMED HOLES AND RECESSES.
- UNLESS OTHERWISE SPECIFIED, THE MINIMUM DEVELOPMENT LENGTHS AND LENGTHS OF LAPS SHALL BE AS FOLLOWS:

BARB SIZE:	N12
(A) HORIZONTAL BARS >300mm OF CONCRETE CAST BELOW THE BAR	500
(B) OTHER BARS	350

## REINFORCEMENT NOTES (CONTINUED):

- WHERE LAPPED SPLICES ARE USED, THE SEPARATION BETWEEN PORTIONS OF BARS SHALL NOT EXCEED 3 x BAR SIZE.
- LAP LENGTHS FOR UNEQUAL BAR SIZES MUST BE BASED UPON THE SMALLER BAR DIAMETER UNO 7.
- WHERE REBATES OR RECESSES ARE REQUIRED, MINIMUM CLEAR COVER TO THE REINFORCEMENT SHALL BE MAINTAINED.
- REINFORCEMENT IS SHOWN DIAGRAMMATICALLY. IT IS NOT NECESSARILY SHOWN IN TRUE PROJECTION.
- SPLICES OF REINFORCEMENT SHALL ONLY BE MADE IN POSITIONS SHOWN.
- WHERE LAPS ARE NOT SHOWN, THEY SATISFY THE REQUIREMENTS OF SPECIFICATION R53.
- DO NOT WELD REINFORCEMENT UNLESS SHOWN ON DRAWING.



NOT FOR CONSTRUCTION

THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED FROM ORIGINAL.

DRAWING FILE LOCATION: NAME C:\CellData\Project\024 Certus Inner West Council Cycleway\014 CAD\Drawings\024RD-0002.dwg										PLOT DATE: TIME 16/04/2021 7:21:14 PM		PLOT BY Charles Green		DRAWINGS - DESIGN PREPARED BY DESIGNER		CLIENT		TITLE WESTCONNEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2 CERTUS PROJECTS PROPOSED CYCLEWAY EXTENTION AT BEDWIN RD GENERAL NOTES		A3 SHEET 1 OF 1	
EXTERNAL REFERENCE FILES										REV		DATE		REVISION / REVISION DESCRIPTION		VLR No.		APPROVAL		Scales ON A3 SIZE DRAWING	
										1		21/03/2021		B30 ED FOR CONCEPT DESIGN				G. DUFFELL			
										2		16/04/2021		B30 ED FOR DRAFT 100% DETAILED DESIGN							
														B30 ED FOR 100% DETAILED DESIGN							

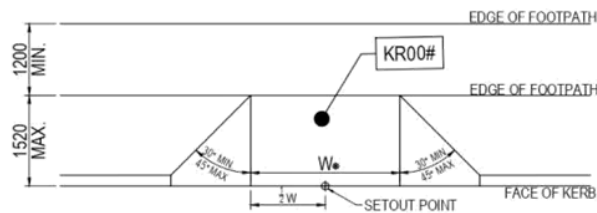
## GENERAL NOTES

KERB RAMP SET OUT TABLE			
LABEL	SETOUT POINT		WIDTH (MEASURED PERPENDICULAR) W*
	EASTING	NORTHING	
KR001	31220.056	6246532.955	2.56
KR002	331223.226	6246536.682	3.45
KR003	331216.319	6246536.698	3.48
KR004	331214.309	6246539.515	3.46
KR005	331236.381	6246533.719	3.41
KR006	331240.169	6246540.384	4.40
KR007	331249.501	6246534.917	1.50

KERB RAMP ADJUSTMENTS ON KR001 TO KR005 ARE BASED ON TCS PLAN 4898 ISSUE B DATED 16/02/2021

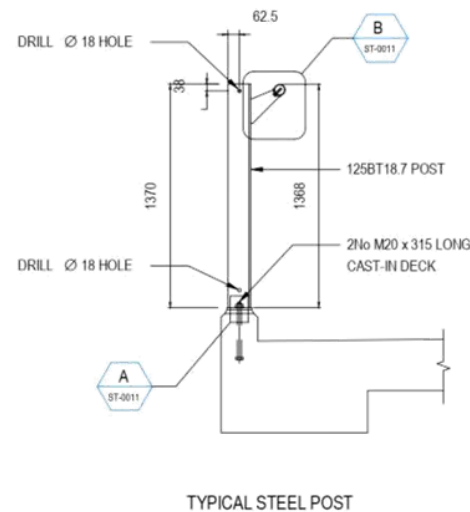
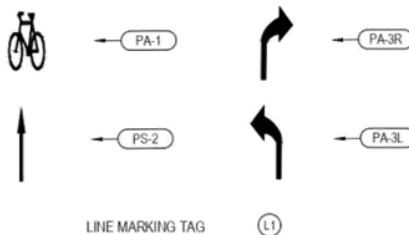
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TYPE	LENGTH (mm)	MOVE	NUMBER OF	TABLE REFERENCE
BT6	1885	2.71	14	TABLE 1
BT8	1995	1.87*	2	TABLE 1
BT12	290	1.87*	1	TABLE 2
BT13	2260	0.85*	2	TABLE 1

REFER TO STRUCTURAL DRAWING 0224-ST-0012 FOR DETAILS. NOTE THESE ARE ADDITIONAL UNITS OF BALUSTRADE ON TOP OF THOSE SPECIFIED IN THE STRUCTURAL DRAWINGS



KERB RAMP SETOUT  
N.T.S

## PAVEMENT MARKINGS



TYPICAL STEEL POST

DIMENSIONS AND ANNOTATIONS SYMMETRICAL ABOUT  
CENTRE LINE DECK / APPROACH SLAB  
N.T.S

## LEGEND

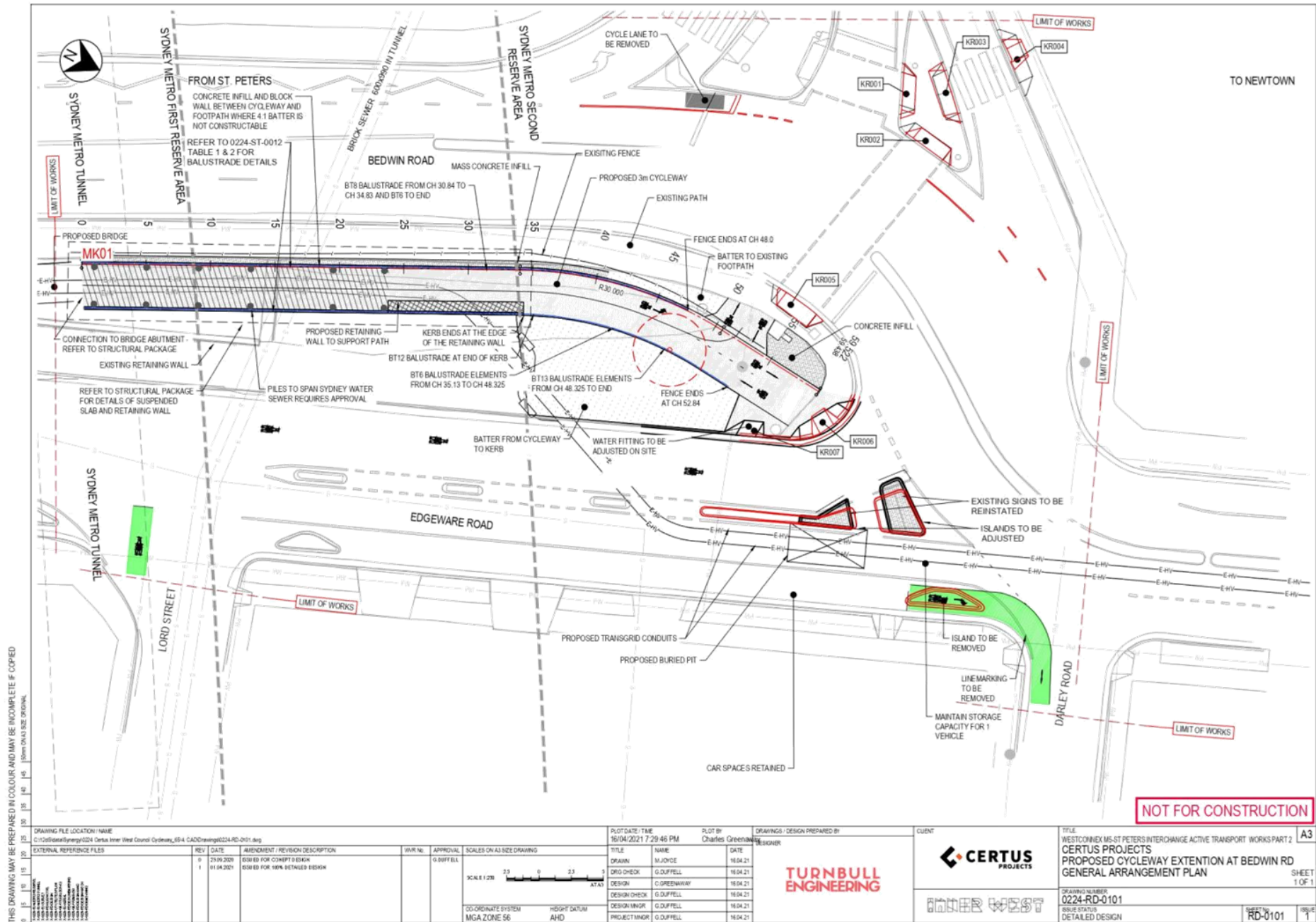
- SURVEY
- DESIGN
- SYDNEY METRO FIRST RESERVE EXTENTS
- SYDNEY METRO SECOND RESERVE EXTENTS
- EXISTING UNDERGROUND ELECTRICAL LINE
- EXISTING GAS MAIN
- EXISTING SEWER MAIN
- EXISTING TELECOMMUNICATIONS LINE
- EXISTING POTABLE WATER MAIN
- EXISTING STORM WATER MAIN
- PROPOSED TRANSGRID CONDUITS
- PROPOSED FENCE
- APPROXIMATE PILE LOCATION
- CYCLEWAY
- 4:1 BATTER
- CONCRETE
- RETAINING WALL - REFER TO STRUCTURAL PACKAGE
- SUSPENDED SLAB - REFER TO STRUCTURAL PACKAGE
- EXISTING SIGN POST TO BE REMOVED
- RELOCATED EXISTING SIGN POST
- PROPOSED SIGN POST
- PROPOSED SIGN FACE
- SIGN REFERENCE NUMBER
- RELOCATED SIGN FACE

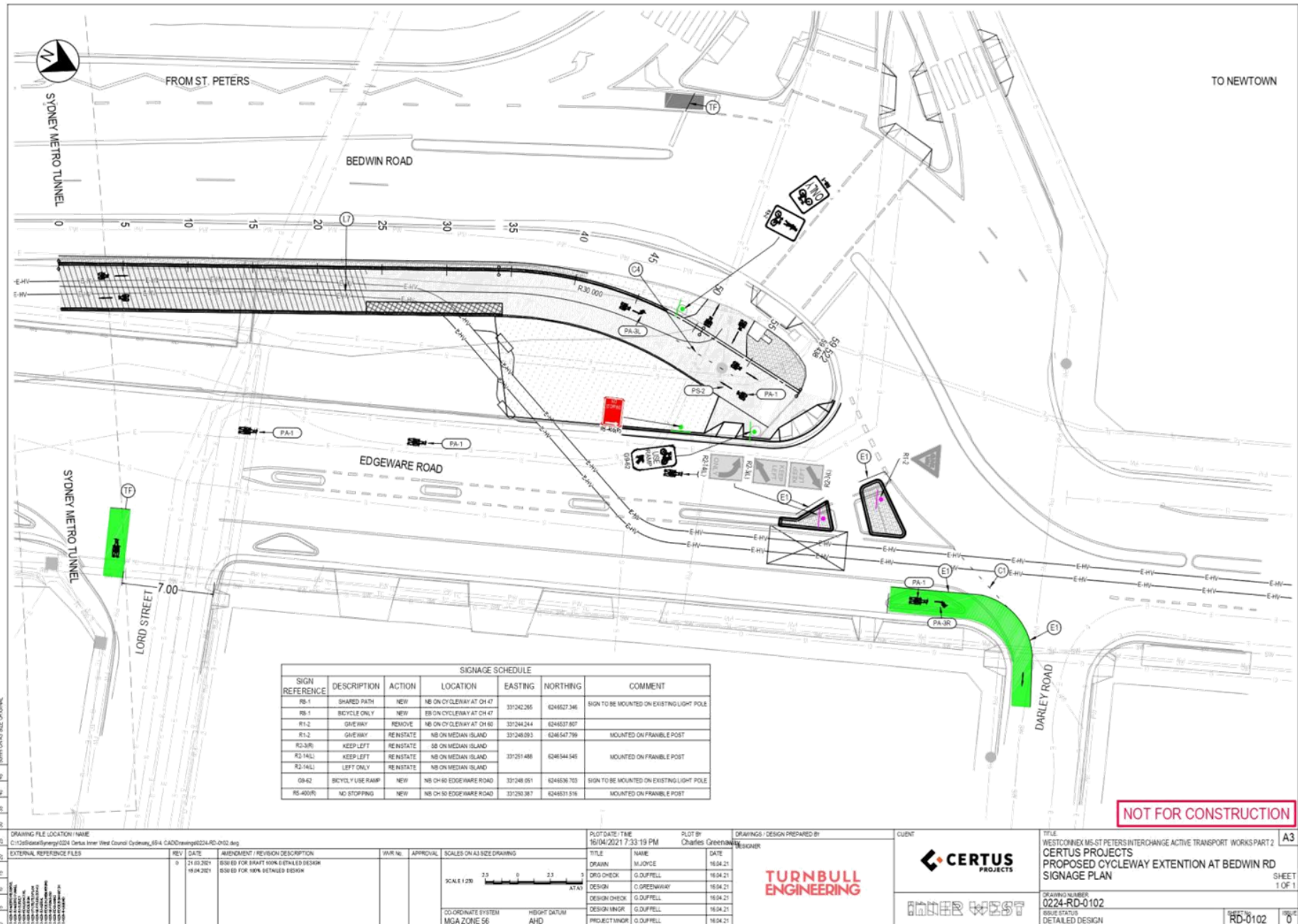
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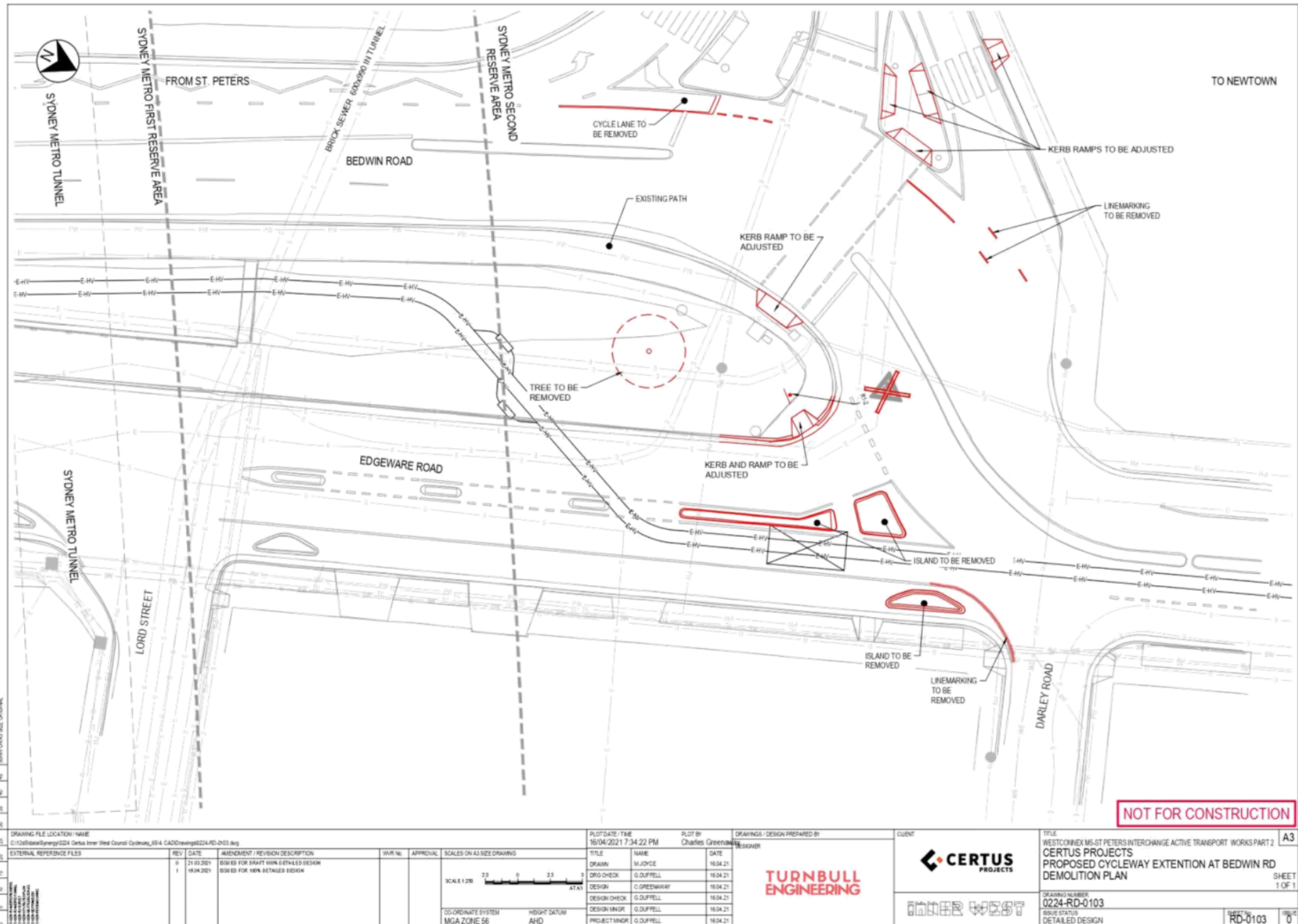
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EXTERNAL REFERENCE FILES										REV		DATE		AMENDMENT / REVISION DESCRIPTION		VLR No.		APPROVAL		SCALES ON A3 SIZE DRAWING	
										1		21.03.2021		B301 ED FOR CONCEPT DESIGN				G. DUFFELL			
										2		01.04.2021		B301 ED FOR DRAFT 100% DETAILED DESIGN				G. DUFFELL			
														B301 ED FOR 100% DETAILED DESIGN							

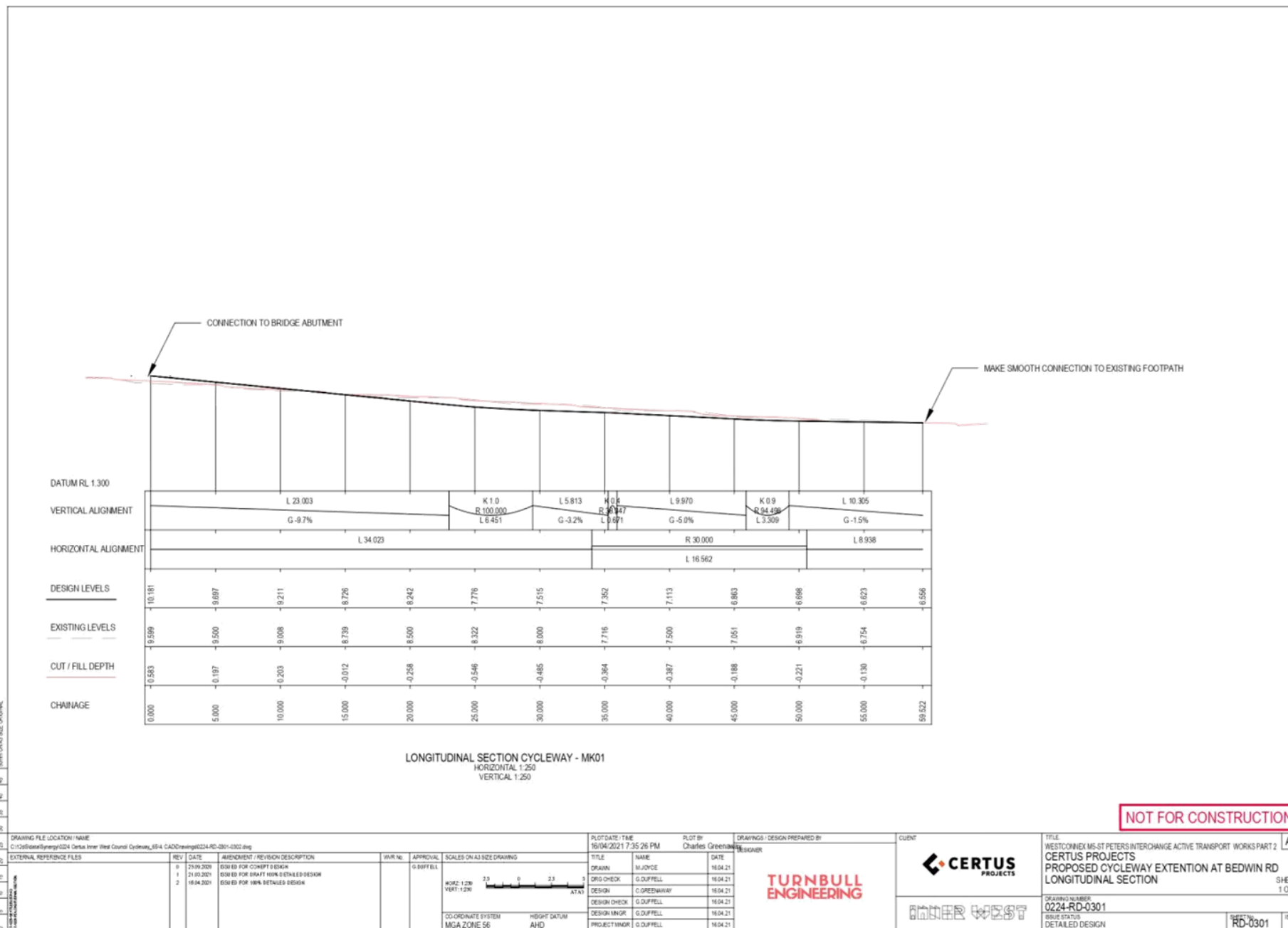


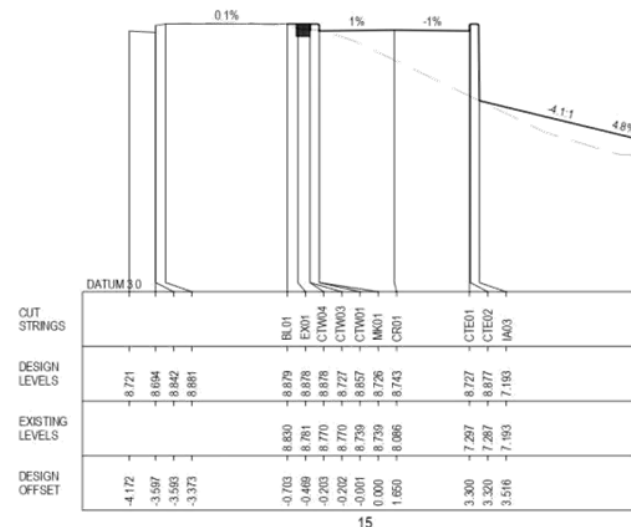
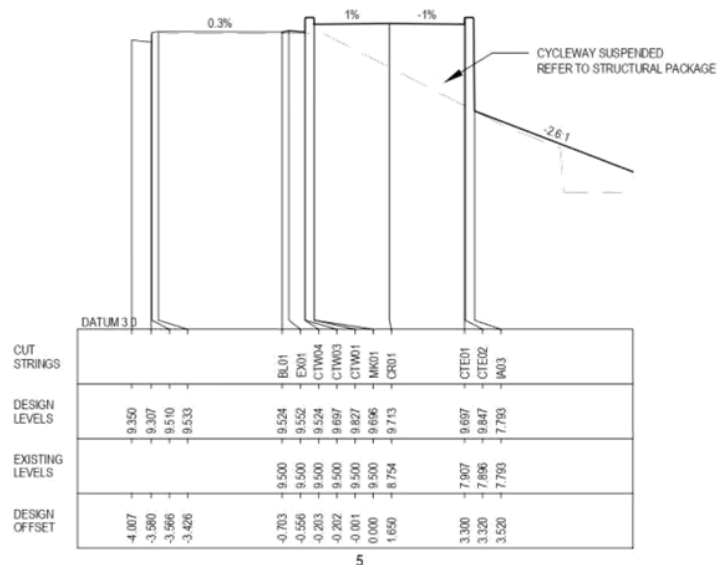
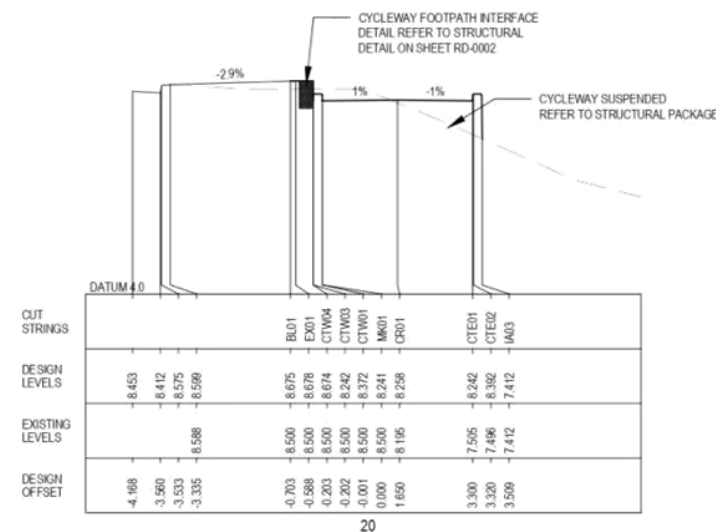
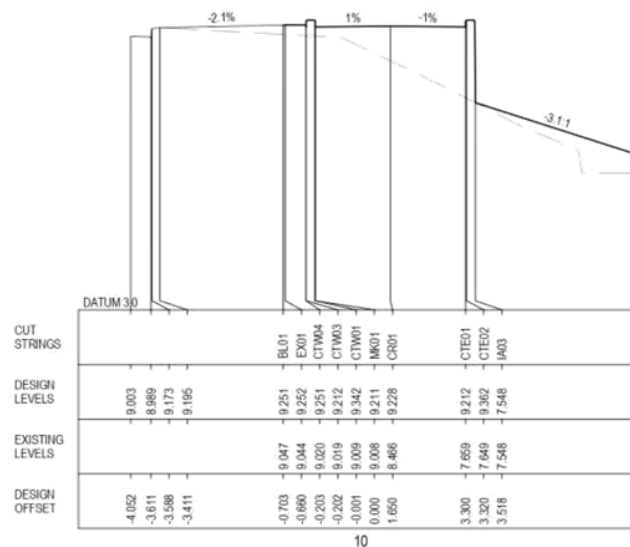










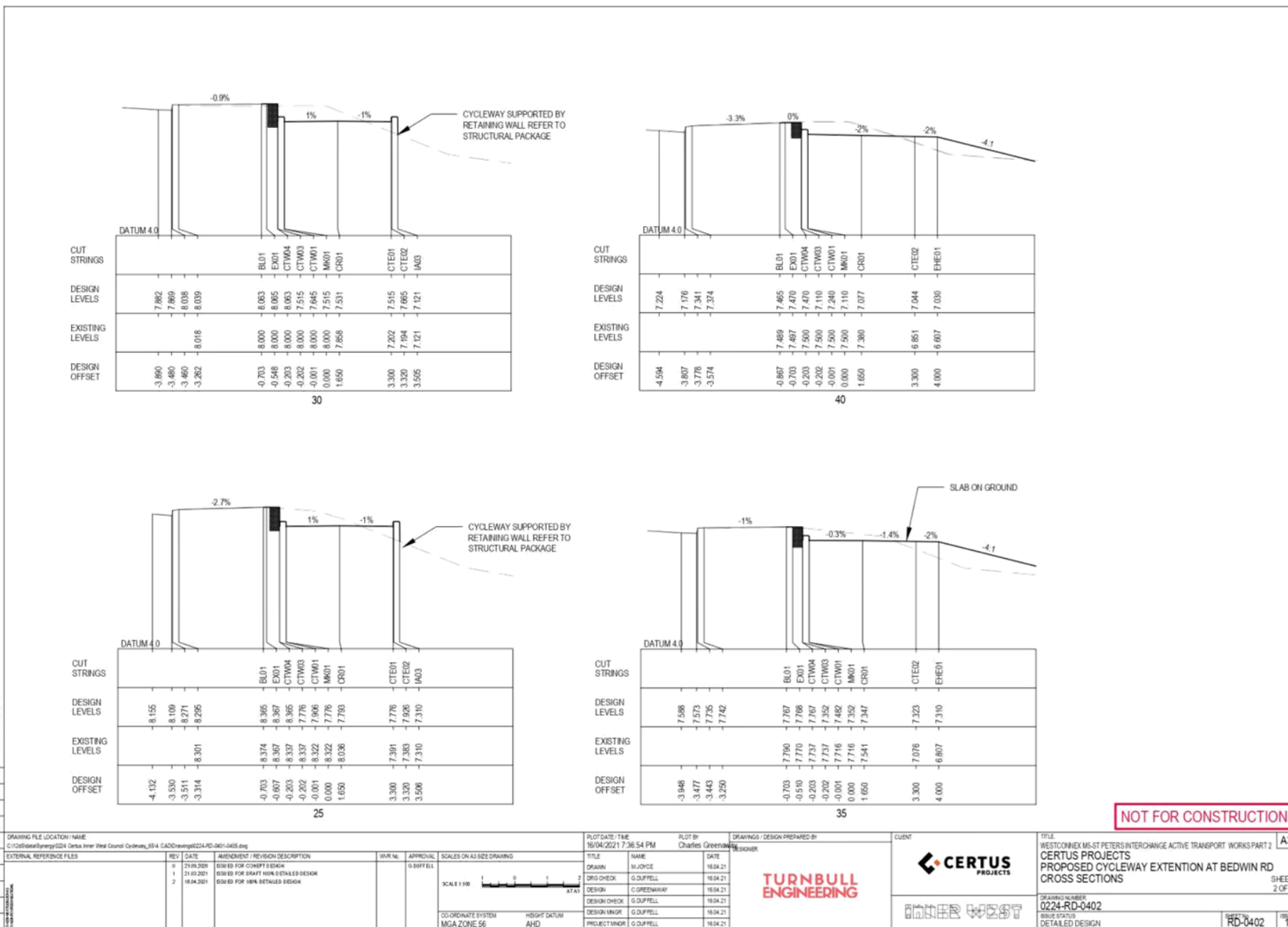


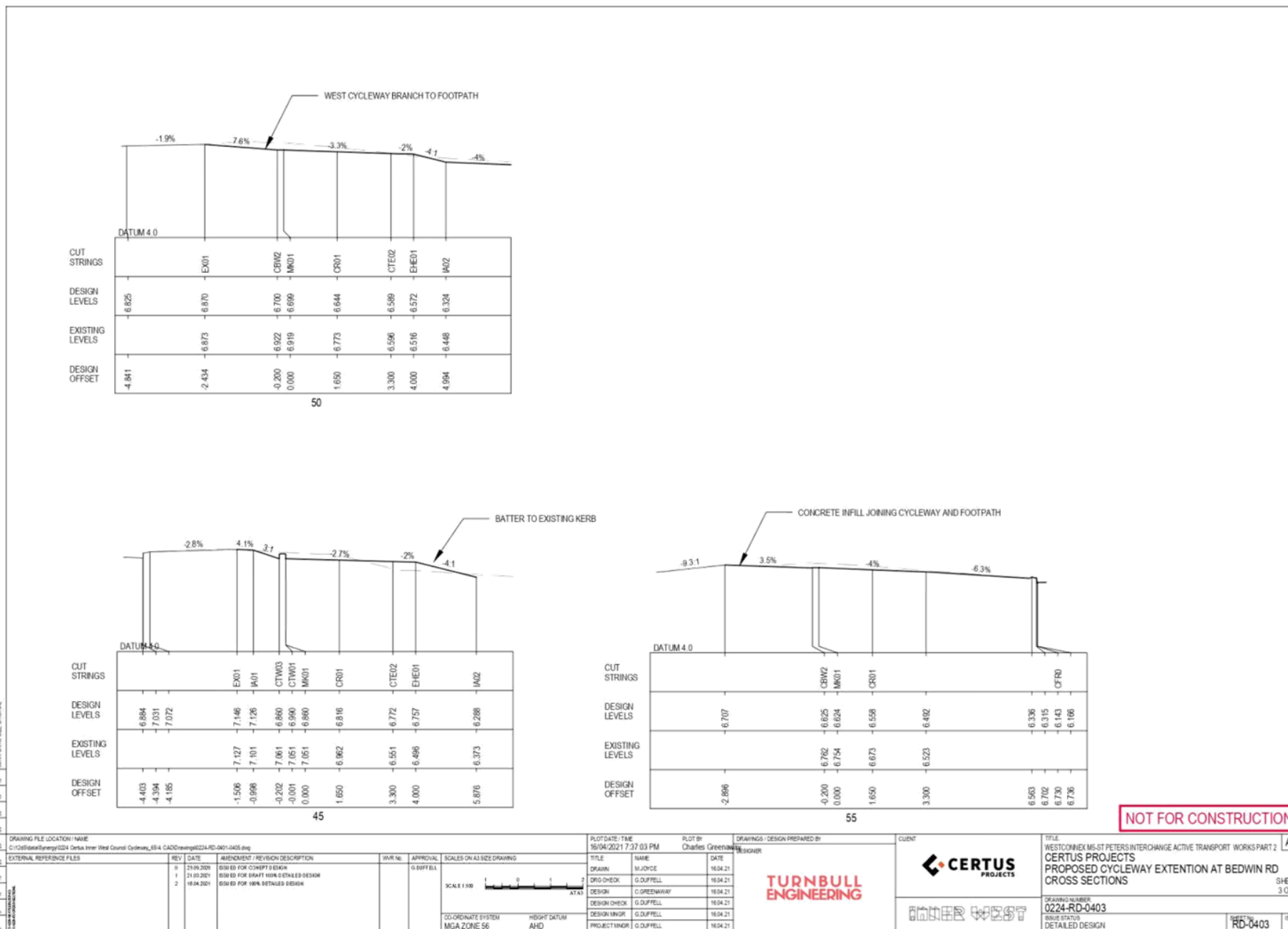
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FROM CHALK SIZE ORIGINAL.







## Appendix B: Road Safety Audit Findings

Item 1

Rev: 2

25  
Ref: CER2001

22/04/2021

Attachment 2





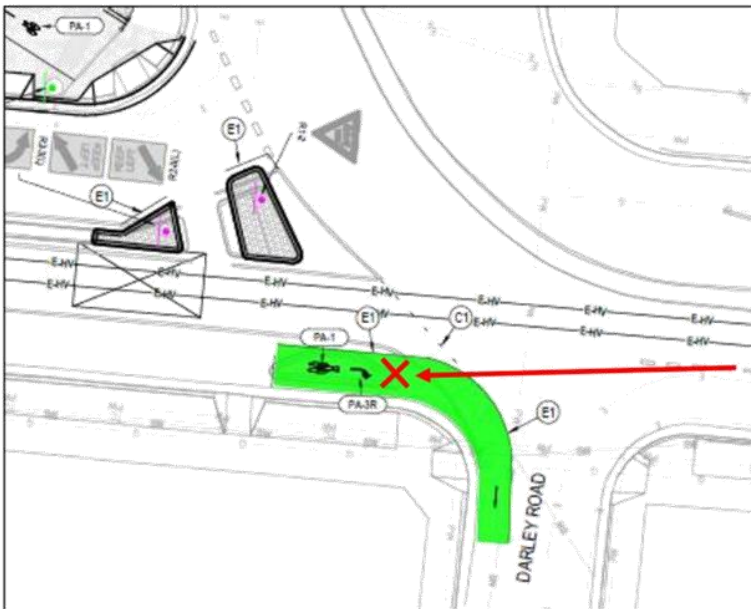
Edgware Road				
ID	Description	Treatment Option	Priority	Project Response
1`	<p>The proposed cycle lane/ advance storage area at the intersection of Lord St and Edgware Road creates a potential conflict point between cyclist and vehicles. The design intent appears to be to direct cyclists to use the lane to shelter/ store while waiting for gaps in traffic. However, a cyclist waiting at this location is at potential conflict with a vehicle waiting to turn left, the driver of which is looking right waiting for a gap in traffic and may fail to observe the cyclist waiting on the drivers left side, when commencing to pull into the intersection.</p> <p>The auditors see no advantage in providing a storage location for cyclists on approach to Edgware Road – cyclists in Lord St will ideally move to the centre of the road in Lord St on approach, and position themselves to the centre or right side of Lord St at Edgware Road, when waiting to turn right.</p>	<p>Delete the cyclist advance/ cycle lane in Lord St.</p> <p>Provide cycle pavement markings in Lord St instead.</p>	Medium	Noted. Certus Projects will consult with Council and opt for removal.



Edgeware Road				
ID	Description	Treatment Option	Priority	Project Response



Cycle path connection into Darley Road				
ID	Description	Treatment Option	Priority	Project Response
2	<p>The marked cycle lane within Edgeware Road connecting to Darley Road is contra-flow and no physical separation is proposed. There is a risk of head on crashes between northbound cyclists and southbound vehicles. Factors such as the narrow lane width in Edgeware Road southbound and the propensity for cyclists to cut the corner/ take the shortest route exacerbate the concern.</p> <p>While the southbound is not a signposted bicycle lane, cyclists were observed using this on the site visit. Cyclists travelling southbound on Edgeware Road may see this as a formalised southbound cycle lane, contributing to head on crashes between cyclists.</p>	Provide a free-standing kerb including a return into Darley Road to define the edge of the contra-flow cycle lane and provide separation between cyclists and vehicles.	High	<b>Certus Projects will adopt construction of narrow dividing strip including return into Darley St.</b>


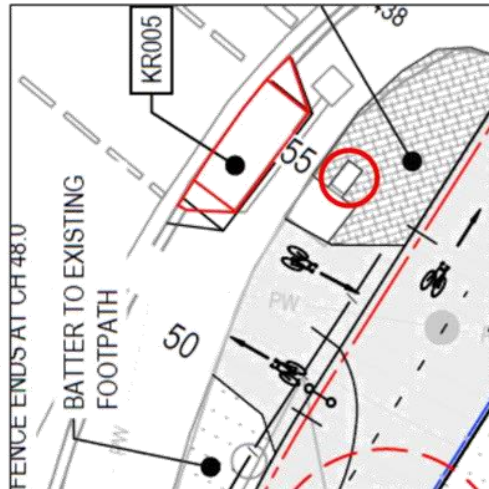
Cycle path connection into Darley Road				
ID	Description	Treatment Option	Priority	Project Response
				



Bedwin Road TCS				
ID	Description	Treatment Option	Priority	Project Response
3	<p>Shared paths/ cycle lanes are present on both sides of Bedwin Road, however TCS lanterns do not accommodate for cyclists riding across the crossing.</p> <p>Cyclists will ride between these two destinations, and the signals infrastructure should provide for this.</p>	Ensure the TCS plan lanterns for P2 and P3 include provision for cyclists.	Note	TCS Designer: Symbol shown on TCS Design is combination of Pedestrian/Cycle Lantern.



Bedwin Road TCS				
ID	Description	Treatment Option	Priority	Project Response

Bedwin Road TCS				
ID	Description	Treatment Option	Priority	Project Response
4	The interface with the new cycle path and the existing signalised crossing of Bedwin St occurs in close proximity to the existing TCS control box. The control box limits the amount of footpath available and concentrates all path user movements into a small area. There is a risk of collisions between path users at this location as different users compete for the limited space available.	The auditors note that this aspect of the design has been updated since the last Audit revision to create additional room.  The concern has been improved however not completely resolved.	Note	Certus Projects: Comments noted.
<div>   </div>				





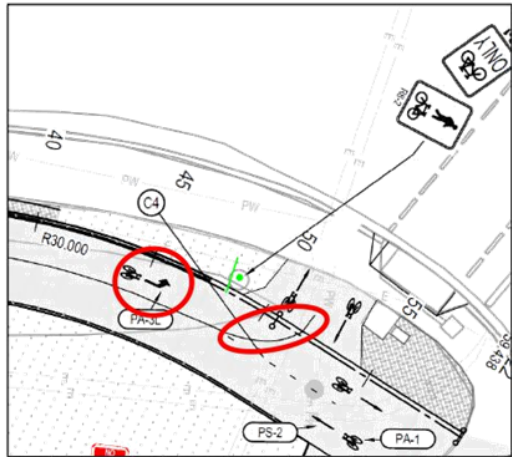


Bedwin Road				
ID	Description	Treatment Option	Priority	Project Response
5	<p>The existing on road cycle lane northbound in Bedwin St is proposed to be removed in the design drawings.</p> <p>Existing on-road cyclists are unlikely to convert to use of the off-road scheme, usually favouring the higher speeds and better connectivity of the road networks. Removal of the existing cycle facility on road reduces the safety afforded to these users.</p> <p>Removal of the marked shoulder areas gives drivers the perception of a wider carriageway which can accommodate higher speeds. Higher speeds increase the risk of all crash types.</p> <p>The TCS plan does not match either the existing site line marking inclusive of the on-road cycle lane or the design drawings which show the lane removed. The drawings are not consistent. The TCS plan always over-rides the other drawings.</p>	<p>Retain the on-road cycle facilities in their present format.</p> <p>Update the both the design drawings and the TCS plan to be consistent and retain the existing on-road facility.</p>	Medium	<p><b>Certus Projects:</b> The on road cycle lines are to be removed. Adopt the TCS Design linemarking.</p> <p>With off road shared paths being provided along Campbell Rd from Bourke Rd to Edinburgh there is no ON ROAD cycle facilities any more.</p> <p>Cyclists can ride on the road but isolated cycle facilities are not to be used unless the on road is gazetted as a cycle route. This is not the case for the Campbell Rd/ Bedwin Rd route once the shared path bridge is built.</p>

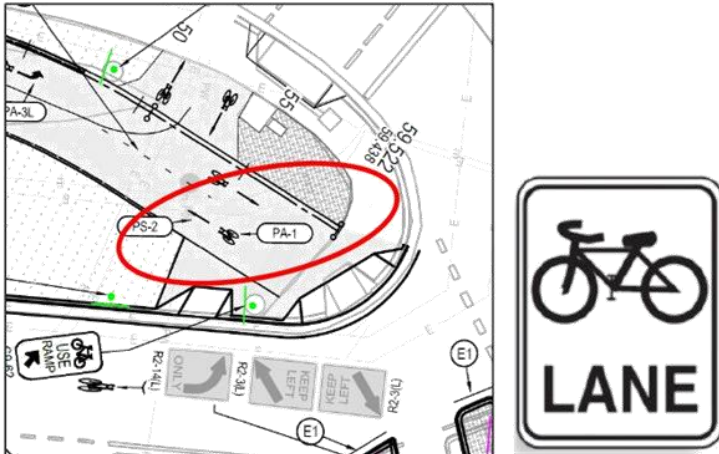


Bedwin Road				
ID	Description	Treatment Option	Priority	Project Response

Bedwin Road TCS				
ID	Description	Treatment Option	Priority	Project Response
6	The TCS drawings show the northbound left turn slip lane as signalised, however the design drawings show this as zebra crossing retained. The drawings are inconsistent.	Update the design drawings to show the signalised crossing, including adjustment of the pram ramp on the kerbside.  Check all other aspects of the plans to ensure consistency.	Note	Certus Projects: Civil Drawings will be updated.
<div>   </div>				

Cycle Path				
ID	Description	Treatment Option	Priority	Project Response
7	<p>The auditors are unclear of the purpose of providing a solid line within the new cycle lane to direct all northbound cycle path users to turn left and access the signals.</p> <p>The presence of the line may confuse some cyclists coming from the signalised crossing that they have some level of priority over northbound cyclists, encouraging them to pull into the path of a rider whom the linemarking suggests will be turning left. This may result in collisions between riders.</p>	<p>Remove the solid curved line from the plan.</p> <p>Remove the left turn pavement arrow within the cycle path, on approach to the connection between the cycle path and the TCS crossing.</p>	Low	<p><b>Certus Projects: Line marking will be updated/ removed as per RSA treatment option.</b></p>
				



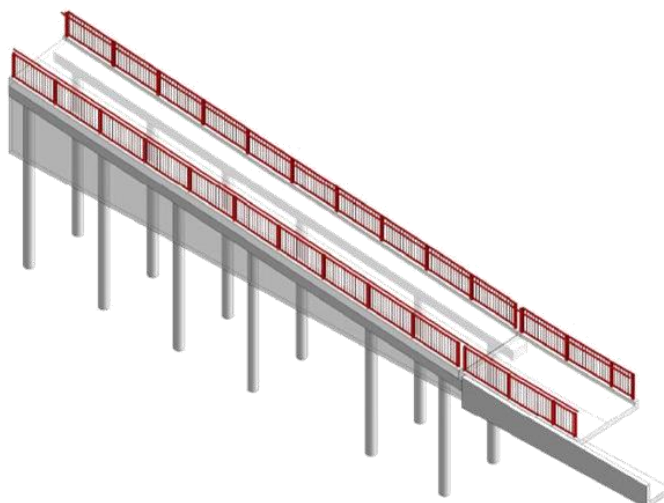
Cycle Path				
ID	Description	Treatment Option	Priority	Project Response
8	Signage to indicate the purpose and regulations governing use of the cycle path are not provided.	Add cycle lane and cycle lane end signage to the plans, at the interface with Edgware Road.	Low	Certus Projects: Signage will be updated as per RSA treatment option.
				

Edgware Road intersection				
ID	Description	Treatment Option	Priority	Project Response
9	The presence of the LEFT ONLY and GIVE WAY signs in the median, may block sight distance from northbound vehicles in Edgware Road to identify southbound vehicles on approach.	Check sight lines and adjust signage position if/as required.  Check and confirm during installation/ post implementation RSA.	Low	<b>Certus Projects: Check will be undertaken.</b>

# DETAILED DESIGN COMPLIANCE MEMO

Inner West Council – Westconnex M5-St Peters Interchange Active Transport  
Works - Part 2: Proposed Cycleway Extension at Bedwin Rd

Contract No. 0224



**TURNBULL  
ENGINEERING**

 **CERTUS**  
PROJECTS



## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

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Item 1

### Document Information

<b>Document Title</b>	Detailed Design Compliance Memo
<b>Document Number</b>	TEJ0224-MEM-0003_Detailed Design Compliance Memo_A

### Document Revisions

Revision	Description	Author	Reviewer	Approver	Date
A	100% Detail Design	C. Greenaway	G. Duffell	G. Duffell	19.04.2021

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Attachment 3

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

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## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

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### 1. Introduction

#### 1.1. Project background

Certus engaged Turnbull Engineering to undertake the detailed design of a proposed cycleway extension for the Inner West Council forming part of Westconnex M5-St Peters Interchange Active Transport Works-Part 2. The project provides an extension of the cycleway from the TransGrid cable bridge adjacent to Bedwin Road bridge.

#### 1.2. Site description

The cycleway is to be located within the Municipality of Inner West Council, approximately 6km south-west of the Sydney CBD (Refer Figure 1-1).

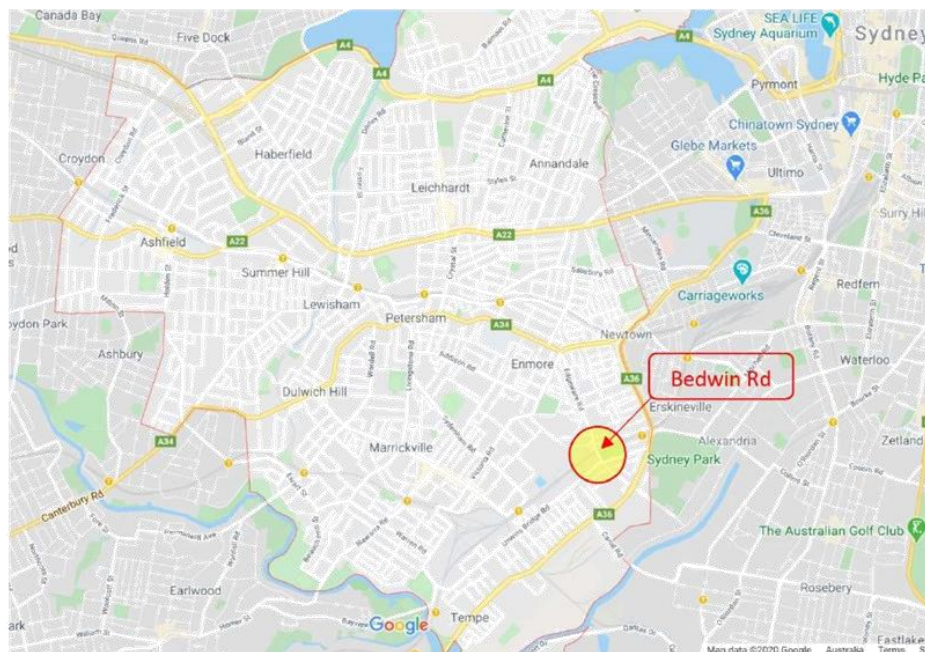


Figure 1-1 - Inner West Council Locality (Source: Google Maps)

The proposed location was subject of a previous alignment design, where a suitable alignment has been determined. The proposed cycleway extension is positioned adjacent to and directly north of the existing Bedwin Road bridge spanning over existing rail lines and the new Metro line, and forms an extension to the cycleway to the north from the TransGrid cable bridge.

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

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### 1.3. Purpose of this memorandum

The purpose of this memorandum is to provide background to the design development of the cycleway north of the Bedwin Road bridge. In particular, this report focuses on:

- Design concept
- Design criteria
- Compliance to standards
- Stakeholder requirements

### 1.4. Relationship to other disciplines

The Civil design interfaces with the following disciplines:

- Structural – predominantly to the structure tying into Bedwin Cable Bridge
- Geotechnical – substructure design

### 1.5. Stakeholders

The design development for the bridge structures component of work has included consultation with the following external stakeholders:

- Inner West Council;
- Roads and Maritime Services (Transport for NSW);
- Sydney Metro SouthWest (Transport for NSW);

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
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### 2. Scope of works

#### 2.1. Structural design

Refer to Structural design documentation for details of:

- Structural components of cycleway superstructure
- Structural components of the cycleway substructure
- Approach slabs/structure to the cycleway
- Bridge balustrades

The bridge is required to meet relevant Australian Standards and stakeholder requirements.

#### 2.2. Civil design

Design and documentation of:

- Any civil earthworks to the bridge approaches;
- Cycleway alignment, line marking requirements and
- Drainage from the bridge and to the bridge approaches.
- Adjoining road crossings specifically the delineation of cyclists movements

#### 2.3. Design Outputs

- 100% Detailed Design Civil drawings
- BoA approval
- Lighting compliance check
- Safety in design

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
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### 3. Design criteria and performance requirements

#### 3.1. Site data

##### 3.1.1. Site visits

Turnbull Engineering has undertaken a number of site visits over the course of the design and construction phases of the adjacent TransGrid cable bridge, the site is well known. Additionally, a site visit was conducted on the 11/3/21 with council to receive feedback on the design and their preferences for cyclists movements.

##### 3.1.2. Site survey and as-built information

Detailed survey of the area and the location of an existing Sydney Water brick sewer main has been provided to Turnbull Engineering and considered in the design.

##### 3.1.3. Geotechnical investigation

Geotechnical information specific to this site including interpretive results and recommendations have been provided by Construction Sciences, report no. 10791E-P-352 Rev01 dated 24 September 2020. For more information refer the structural design report.

#### 3.2. Standards, guidelines and specifications

The Standards, Guidelines and Specifications used for the design and construction are those currently in place. The design of the structural works has been undertaken using the following standards, guidelines and specifications.

Table 3-1 - Standards

REFERENCE NO.	DOCUMENT TITLE	ISSUE DATE
AGRD06A-17	Guide to Road Design Part 6A: Paths for Walking and Cycling	2017
Ap-G88-14	Cycling Aspects of Austroads Guides	2014
AGRD04-17	Guide to Road Design Part 4: Intersections and Crossings General	2017
AGRD04A-17	Guide to Road Design Part 4A: Unsignalised and Signalised Intersections	2017
AGRD05-19	Pavement Evaluation and Treatment Design	2019
	RMS Design Guide - Delineation	2015

Refer to Detailed design drawings for Inner West Council Standard drawing references.



## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
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### 4. Design compliance checks and design rational

#### 4.1. Cycleway alignment

##### 4.1.1. Horizontal geometry

The project provides an extension of the cycleway from the TransGrid cable bridge adjacent to Bedwin Road bridge. The cycleway extension was designed to follow the existing footpath whilst allowing for level differences. A 30km/h design speed was considered when developing horizontal curves and the number of curves minimised. The cycleway connects to the bridge abutment at an approximate 4.5° angle to allow for a simplified structural design and improved constructability. The cycleway path was designed to be 3.6m wide to match the cable bridge. Similarly, a crowned formation was used up to CH 35 to tie into the cable bridge. At CH 35 the superelevation changes to be a one-way cross fall to the east. This acts to improve rider comfort and aid drainage as cyclists turn the bend. At CH 48 the main cycleway branches off to provide access to the signalised crossing at Bedwin road. An additional eastern branch and new kerb ramp has been provided to better facilitate riders heading north along Edgware road negotiate the sharp hook turn on to the cycleway.

##### 4.1.2. Vertical geometry

The cycleway is vertically constrained to tie into the bridge abutment and the existing footpath at the Bedwin road and Edgware road intersection. The design attempts to lower the cycleways level to the existing surface as soon as possible whilst considering design standards. Up to CH 25 the cycleway is supported by pilling and then to CH35 a retaining wall, for more details on the substructure refer to the structural design drawings and report. After CH 35 the eastern side of the cycleway is supported by fill. The cycleway matches the gradient of the bridge abutment at 9.7% before reducing to 3.2% at CH30, a vertical curve is used to transition between grades. At approximately CH35 the cycleway steepens, and the cross fall increased to -4% to the east to tie into the existing footpath and flatten the area between the new eastern kerb ramp and the cycleway. It is noted that the gradients on the southern end of the cycleway extension are steeper than desirable however further grade reductions were not possible with the existing site constraints. See section 5 departures from design guides for more information.

##### 4.1.3. Fencing

To provide consistency the fencing has been continued from the cable bridge. On the eastern side of the cycleway the fencing continues to CH53 at which point the batter grade is minimal and does not present a risk to cyclists in line with Austroads guides. On the West side of the cycleway the fence continues to CH48 to delineate between the footpath and cycleway, prevent the fall risk whilst the cycleway is above the footpath level and provide a physical barrier to the telegraph pole at CH48. At the termination points the fence will use returns to minimise risks to cyclists, refer to the structural package for details.

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
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### 4.2. Delineation and signage

Additional line marking and signage has been added to guide cyclists along planned routes. The intersection of Bedwin road and Edgeware road are complicated and difficult for cyclists to navigate as was seen during site visits. The new cycleway will have PS-2 bike symbols and PA-1 arrows delineating it as a bike only path. Additional sections painted green will further direct cyclists and warn motorists. The existing on road bike path traveling north along Bedwin road will be removed, along with its associated line marking as cyclists will be directed across the new bridge. Signage will also be used to warn cyclists of shared pedestrian zones.

### 4.3. Median islands

The existing median islands at the crossing of Edgeware road have been redesigned to provide compliant storage of cyclists. The width of these raised concrete islands has been maximised to provide a 2.5m by 2.5m area, whilst allowing for an 8.8m service vehicle to negotiate the turn on to Bedwin road. The design vehicle used was the maximum size vehicle the existing intersection formation allows for and was determined as the initial step of the turn path assessment. Refer to Appendix C for the turn path checks.

### 4.4. Lighting compliance

A streetlighting compliance check was conducted of the project area. The lighting was assessed and confirmed to be compliant with category V3 of AS1158.1.1 on the carriageway over the intersection, category P2 of AS1158.3.1 on the footpaths and category PX2 of AS1158.4 on the pedestrian crossings. It was noted that there was no suitable table available for 250W MBF Optispec luminaires, subsequently for calculation purposes a sodium high pressure equivalent has been utilised in this review. The review also highlighted an existing section on the north bound lane that did not meet complaint lux levels, refer to the section 5 departures from design guides for more information.

### 4.5. Building Over Asset

The cycleway extension crosses over a Sydney Water 9900mm x 660mm brick sewer. Sydney Water requirements required the cycleway be supported by piles that have been spaced to avoid the sewer and transfer any loads outside of the sewers zone of influence, refer to Figure 2: Zone of influence (ZOI) section of cycleway piers & slab.

A BOA is to be submitted to Sydney water for approval refer to Appendix D Sydney Water BOA.

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
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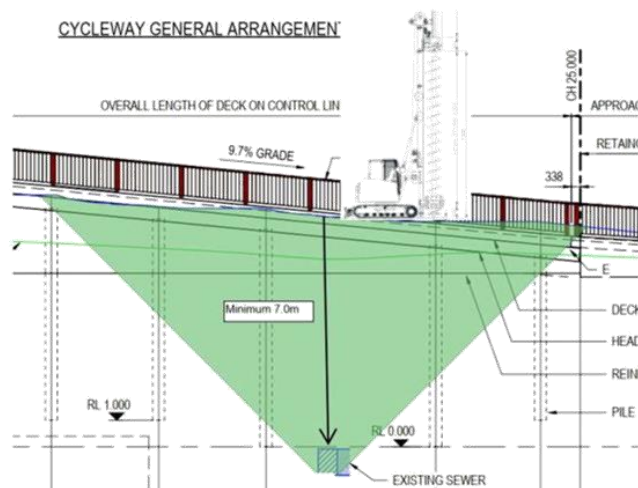


Figure 2: Zone of influence (ZOI) section of cycleway piers & slab

### 4.6. Lowering Stop Valve cover

A proposed kerb ramp along Edgeware road, KR007, will require on-site adjustment of a water stop valve cover. Initial investigation noted lowering of cover is possible. Certus projects have submitted a Tap-in for the lowering of this cover.

### 4.7. TCS design

The TCS design at the Bedwin road and Edinburgh Road intersection was designed concurrently with the cycleway design. The adjustments to existing kerb ramps on the signalised median islands and footpaths were included in the civil design plans and based on the latest TCS plan at the time TCS no. 4898 sheet 1 issue B dated 16/02/2021.

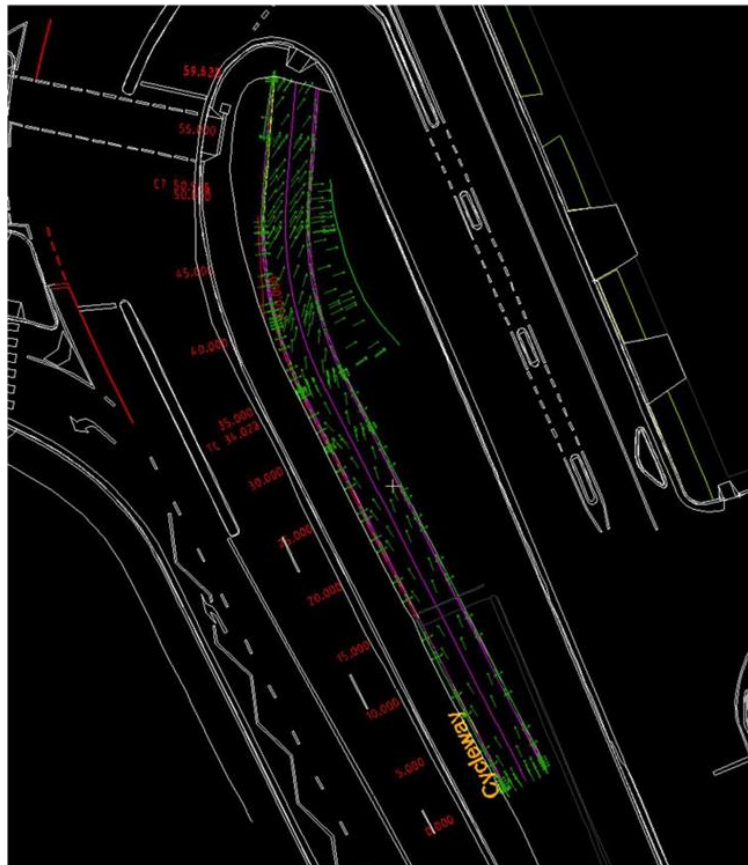
## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
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### 4.8. Drainage

The drainage design of the cycleway was assessed against the criteria in Austroads part 4 and 5. The assessment area started at the highpoint of Bedwin Cable Bridge and continued down to the crossing at Edgware road. Unbroken kerbs run along both sides of the cycleway up to CH 35 on the eastern side and CH 48 on the western side. These act to channel the water along the cycleway. To provide a safe braking zone at Edgware road sheet flows needed to be reduced. At CH35 the eastern kerb is stopped and the cross fall on the cycleway is adjusted to be one way, falling to the east. The flows are then able to shed off the paths and over the batter refer to Figure 3: Flow path diagram



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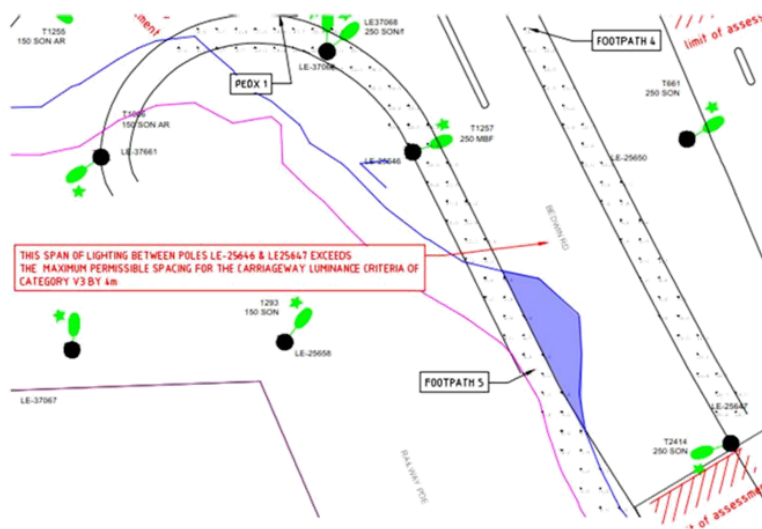
## Item 1

### 5.1. Cycle Path Grades

The design cycle grades of 9.7% on the suspended deck structure are above the acceptable uphill gradients for ease of cycling found in Figure 5.6 of Austroads 6A – Paths for walking and cycling. As the authority, Inner West Council can accept this departure. The grade has been accepted and raised in the SID report. It is noted that this requirement is a comfort criterion for uphill travel and clauses exist within the standard to allow steeper grades where it is unavoidable. After this point at approximately CH30 from the end of the bridge the grade reduces to 3.2%.

## 5.2. Lighting compliance

A lighting compliance check was undertaken across the project scope area, refer appendix E. An existing section of the north bound lane of Bedwin road was identified as non-compliant by not meeting 7.5Lux highlighted blue below. The span of lighting between poles LE25646 and LE25647 exceeds the maximum permissible spacing for the carriage way luminance criteria of category V3. This non-compliance occurs due to existing conditions. The construction of the proposed cycleway is not within this existing area, so no further actions were taken.





## MEMORANDUM

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### 6. Environment and Heritage Considerations

Environment and Heritage requirements were excluded from this report.



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### 7. Approvals

Item	Status 0/50/100/*IFC	Comment	Approver
Civil set	100%	Submitted	Council
Structural set	IFC	Submitted	Council
Lighting	100%	Info only	Info Only
Building over asset	100%	Pending submission	Sydney Water
Syd Metro Protection	100%	Submitted, under review	Sydney Metro

\*IFC – Issue For Construction

Other approvals are also required for the project were not included as part of this design report.

Two approvals are outstanding;

- Sydney Water BoA acceptance
- Sydney Metro Corridor Protection acceptance

## MEMORANDUM

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### Appendix A Drawing List

DRAWING NUMBER	REV	TITLE
TE0224-RD-0001	1	COVER SHEET AND DRAWING SCHEDULE
TE0224-RD-0002	2	GENERAL NOTES
TE0224-RD-0003	2	DESIGN SCHEDULES AND LEGEND
TE0224-RD-0101	1	GENERAL ARRANGEMENT PLAN
TE0224-RD-0102	1	SIGNAGE PLAN
TE0224-RD-0103	1	DEMOLITION PLAN
TE0224-RD-0301	2	LONGITUDINAL SECTION
TE0224-RD-0401	2	CROSS SECTIONS
TE0224-RD-0402	2	CROSS SECTIONS
TE0224-RD-0403	2	CROSS SECTIONS

## MEMORANDUM

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### Appendix B Detailed Design Drawings

Item 1

Attachment 3

## CERTUS PROJECTS

### WESTCONNEX M5-ST PETERS INTERCHANGE ACTIVE TRANSPORT WORKS PART 2

### PROPOSED CYCLEWAY EXTENTION AT BEDWIN RD

### 100% DETAILED DESIGN

#### DRAWING SCHEDULE

NUMBER	DESCRIPTION
0224-RD-0001	COVER SHEET AND DRAWING SCHEDULE
0224-RD-0002	GENERAL NOTES
0224-RD-0003	DESIGN SCHEDULES AND LEGEND
0224-RD-0101	GENERAL ARRANGMENT PLAN
0224-RD-0102	SIGNAGE PLAN
0224-RD-0103	DEMOLITION PLAN
0224-RD-0301	LONGITUDINAL SECTION
0224-RD-0401	CROSS SECTIONS
0224-RD-0402	CROSS SECTIONS
0224-RD-0403	CROSS SECTIONS

NOT FOR CONSTRUCTION

THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED  
FROM A SMALL SIZE ORIGINAL.

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EXTERNAL REFERENCE FILES										REV	DATE	AMENDMENT / REVISION DESCRIPTION				APP. NO.	APPROVAL	SCALES ON A3 SIZE DRAWING				TITLE		DATE	
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										1	16/04/2021	ISSUED FOR 100% DETAILED DESIGN										DRG CHECK		16/04/21	
																						DESIGN		16/04/21	
																						DESIGN CHECK		16/04/21	
																						DESIGN MGR		16/04/21	
																						PROJECT MGR		16/04/21	

## GENERAL NOTES

- LEVELS ARE TO AUSTRALIAN HEIGHT DATUM (AHD).
- CO-ORDINATES ARE TO MAP GRID AUSTRALIA (MGA) CO-ORDINATE SYSTEM (ZONE 56), GDA94.
- ALL DIMENSIONS ARE IN METRES U.N.O. ALL LEVELS, CHAINAGES, STATIONS AND CO-ORDINATES ARE EXPRESSED IN METRES.
- SURVEY DATA WAS OBTAINED FROM - 200236-DT-UT (2020-07-22).
- UTILITY DATA WAS OBTAINED FROM - TEA PSF CD 054.
- UTILITY INFORMATION SHOWN ON THE DRAWINGS DOES NOT DEPICT ANY MORE THAN THE PRESENCE OF A SERVICE BASED ON AVAILABLE DOCUMENTARY EVIDENCE. THE PRESENCE OF A UTILITY SERVICE, ITS SIZE AND LOCATION SHOULD BE CONFIRMED BY FIELD INSPECTION AND POTHOLING PRIOR TO THE COMMENCEMENT OF ROADWORKS, AND THE RELEVANT UTILITY PLANS SHOULD BE OBTAINED BY DIALING 1100 OR FAXING 1300 652 077 (DIAL BEFORE YOU DIG). CAUTION SHOULD BE EXERCISED WHEN WORKING IN THE VICINITY OF ALL UTILITY SERVICES.
- STOP VALVE FITTING ADJUSTMENT TO BE DONE IN LINE WITH SYDNEY WATER GUIDELINES.
- NO LIGHTING DESIGN HAS BEEN UNDERTAKEN.
- SYDNEY WATER APPROVAL IS REQUIRED FOR PROTECTION OF THEIR ASSET AND PILLING ADJACENT TO THE BRICK SEWER IN THE TUNNEL.
- TRANSGRID APPROVAL IS REQUIRED FOR WORKS OVER THE PROPOSED 330kV CONDUITS/CABLES.
- SYDNEY METRO APPROVAL IS REQUIRED FOR PROTECTION OF THEIR ASSET, WORKS AND PILES WITHIN THE 1st AND 2nd RESERVE AREAS.
- SIGNAGE AND LINEMARKING PLANS MUST BE READ IN CONJUNCTION WITH TCS PLANS.
- ALL SIGNAGE MUST BE CLEAR OF ALL VEGETATION AND OBSTRUCTIONS.
- INTERFACE COORDINATION IS REQUIRED WITH BRIDGE CONTRACTOR AND DESIGNER FOR CONNECTION OF CYCLEWAY.
- REFER TO DESIGNER FOR 12D SETOUT FILE FOR PROJECT.

## THE FOLLOWING ITEMS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH THE RELEVANT STANDARD DRAWINGS LISTED.

ITEM	STANDARD TO REFERENCE
KERBS AND MEDIAN ISLANDS	INNER WEST COUNCIL STANDARD ROADS DRAWING R1 (V1)
PRAM RAMP	INNER WEST COUNCIL STANDARD FOOTPATH DRAWING F4 (V1)
FOOTPATHS	INNER WEST COUNCIL STANDARD FOOTPATH DRAWING F2 (V1)
CYCLE PATHS PAVEMENT AND JOINING	INNER WEST COUNCIL STANDARD CYCLEWAY DRAWING C1 (V1)
PAVEMENT MARKING	RMS DELINEATION GUIDELINES AND RMS QA SPECIFICATIONS R142 AND R145
MESSAGES ON PAVEMENT	RMS DELINEATION GUIDELINE SECTION 9
SIGNAGE	AS1742

## CONTROL LINE MK01 - HORIZONTAL SEGMENTS

APPROACH SEGMENT	DEPARTURE SEGMENT	TANGENTIAL	CHAINAGE	COORDINATES			APPROACH BEARING	DEPARTURE BEARING	APPROACH RADIUS	DEPARTURE RADIUS
				X	Y	Z				
	LINE		0.000	331261.811	6246483.147	10.182		332°53'37.00"		
LINE	ARC	YES	34.023	331246.309	6246513.433	7.384	332°53'37.00"	332°53'37.00"		30.000
ARC	LINE	YES	50.584	331243.107	6246529.468	6.890	4°31'27.42"	4°31'27.42"	30.000	
LINE			59.522	331243.812	6246538.378	6.556	4°31'27.42"			

## CONCRETE NOTES:

- PLACING AND CURING OF CONCRETE SHALL COMPLY WITH AS5100.5
- CONCRETE EXPOSURE CLASSIFICATION B1.
- EDGE SHALL BE CHAMFERED 20x20 AND RE-ENTRANT ANGLES FILLETED 20x20 UNO.
- CONCRETE GRADE AND NOMINAL COVER TO REINFORCEMENT (UNO):  
CAST AGAINST GROUND: N32/85mm  
ELSEWHERE: N32/55mm

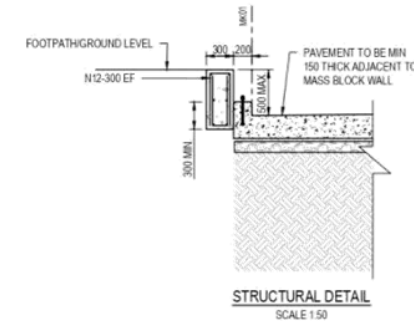
## REINFORCEMENT NOTES:

- ALL REINFORCEMENT SHALL COMPLY WITH AS 4671 AND D500N
- COGS, HOOKS, SPLICES AND PIN DIAMETERS SHALL BE IN ACCORDANCE WITH AS5100.5.
- REINFORCEMENT UNO MAY BE DISPLACED SLIGHTLY WHERE NECESSARY TO CLEAR STEEL DOWELS ANCHOR BOLTS, FORMED HOLES AND RECESSES.
- UNLESS OTHERWISE SPECIFIED, THE MINIMUM DEVELOPMENT LENGTHS AND LENGTHS OF LAPS SHALL BE AS FOLLOWS:

BARB SIZE:	N12
(A) HORIZONTAL BARS >300mm OF CONCRETE CAST BELOW THE BAR	500
(B) OTHER BARS	350

## REINFORCEMENT NOTES (CONTINUED):

- WHERE LAPPED SPLICES ARE USED, THE SEPARATION BETWEEN PORTIONS OF BARS SHALL NOT EXCEED 3 x BAR SIZE.
- LAP LENGTHS FOR UNEQUAL BAR SIZES MUST BE BASED UPON THE SMALLER BAR DIAMETER UNO 7.
- WHERE REBATES OR RECESSES ARE REQUIRED, MINIMUM CLEAR COVER TO THE REINFORCEMENT SHALL BE MAINTAINED.
- REINFORCEMENT IS SHOWN DIAGRAMMATICALLY. IT IS NOT NECESSARILY SHOWN IN TRUE PROJECTION.
- SPLICES OF REINFORCEMENT SHALL ONLY BE MADE IN POSITIONS SHOWN.
- WHERE LAPS ARE NOT SHOWN, THEY SATISFY THE REQUIREMENTS OF SPECIFICATION R53.
- DO NOT WELD REINFORCEMENT UNLESS SHOWN ON DRAWING.



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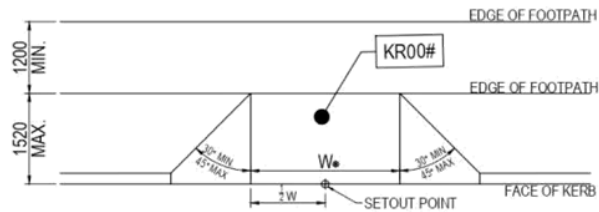
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	EASTING	NORTHING	
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KR002	331223.226	6246536.682	3.45
KR003	331216.319	6246536.698	3.48
KR004	331214.309	6246539.515	3.46
KR005	331236.381	6246533.719	3.41
KR006	331240.169	6246540.384	4.40
KR007	331249.501	6246534.917	1.50

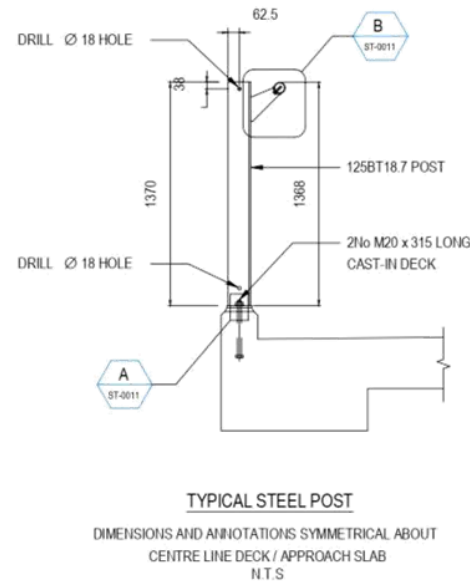
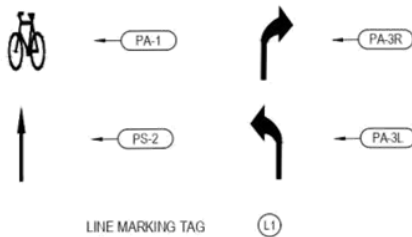
KERB RAMP ADJUSTMENTS ON KR001 TO KR005 ARE BASED ON TCS PLAN 4898 ISSUE B DATED 16/02/2021

BALUSTRADE TABLE				
TYPE	LENGTH (mm)	MOVE	NUMBER OF	TABLE REFERENCE
BT6	1885	2.71	14	TABLE 1
BT8	1995	1.87*	2	TABLE 1
BT12	290	1.87*	1	TABLE 2
BT13	2260	0.85*	2	TABLE 1

REFER TO STRUCTURAL DRAWING 0224-ST-0012 FOR DETAILS. NOTE THESE ARE ADDITIONAL UNITS OF BALUSTRADE ON TOP OF THOSE SPECIFIED IN THE STRUCTURAL DRAWINGS



## PAVEMENT MARKINGS



## LEGEND

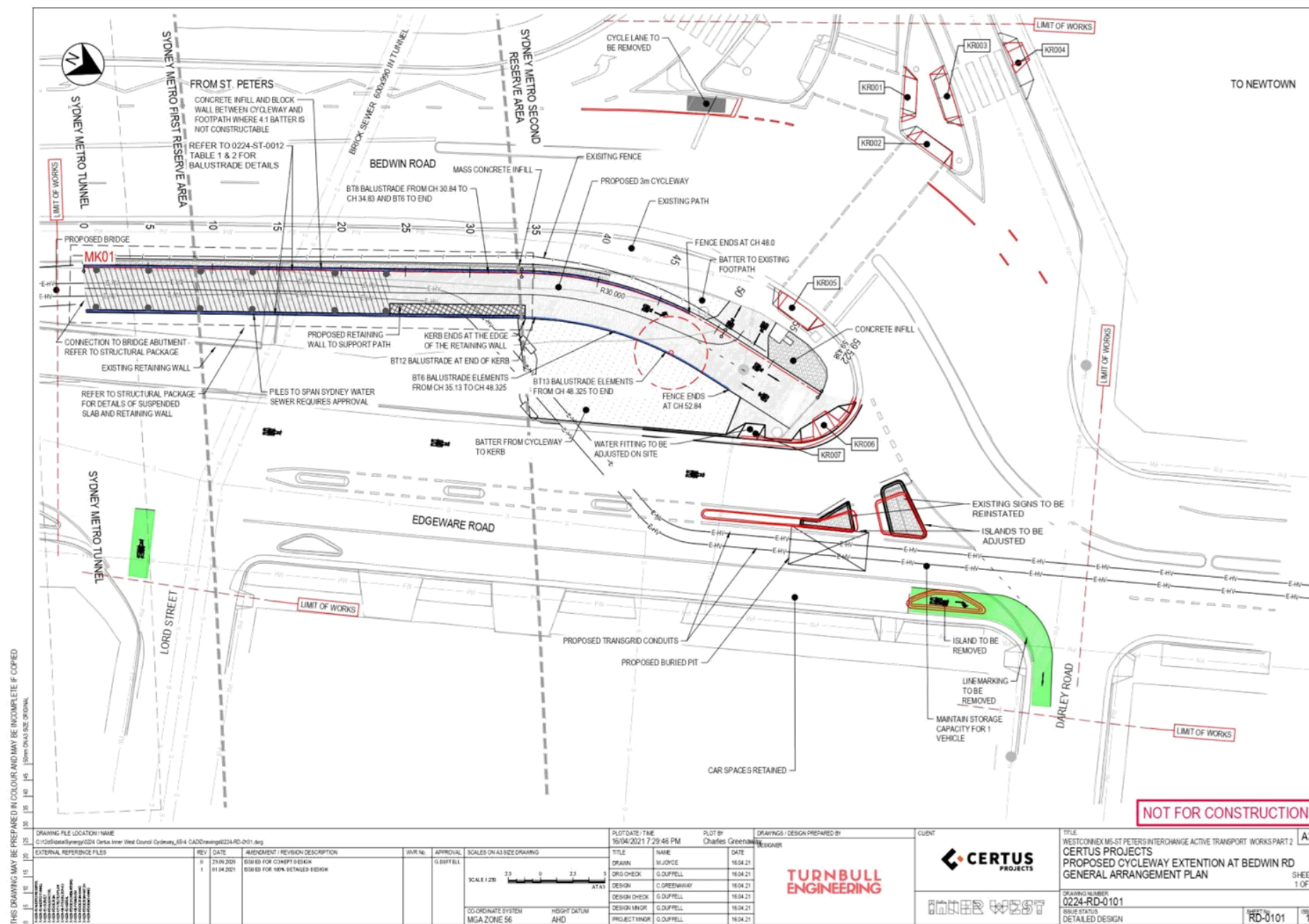
- SURVEY
- DESIGN
- SYDNEY METRO FIRST RESERVE EXTENTS
- SYDNEY METRO SECOND RESERVE EXTENTS
- EXISTING UNDERGROUND ELECTRICAL LINE
- EXISTING GAS MAIN
- EXISTING SEWER MAIN
- EXISTING TELECOMMUNICATIONS LINE
- EXISTING POTABLE WATER MAIN
- EXISTING STORM WATER MAIN
- PROPOSED TRANSGRID CONDUITS
- PROPOSED FENCE
- APPROXIMATE PILE LOCATION
- CYCLEWAY
- 4:1 BATTER
- CONCRETE
- RETAINING WALL - REFER TO STRUCTURAL PACKAGE
- SUSPENDED SLAB - REFER TO STRUCTURAL PACKAGE
- EXISTING SIGN POST TO BE REMOVED
- RELOCATED EXISTING SIGN POST
- PROPOSED SIGN POST
- PROPOSED SIGN FACE
- SIGN REFERENCE NUMBER
- RELOCATED SIGN FACE

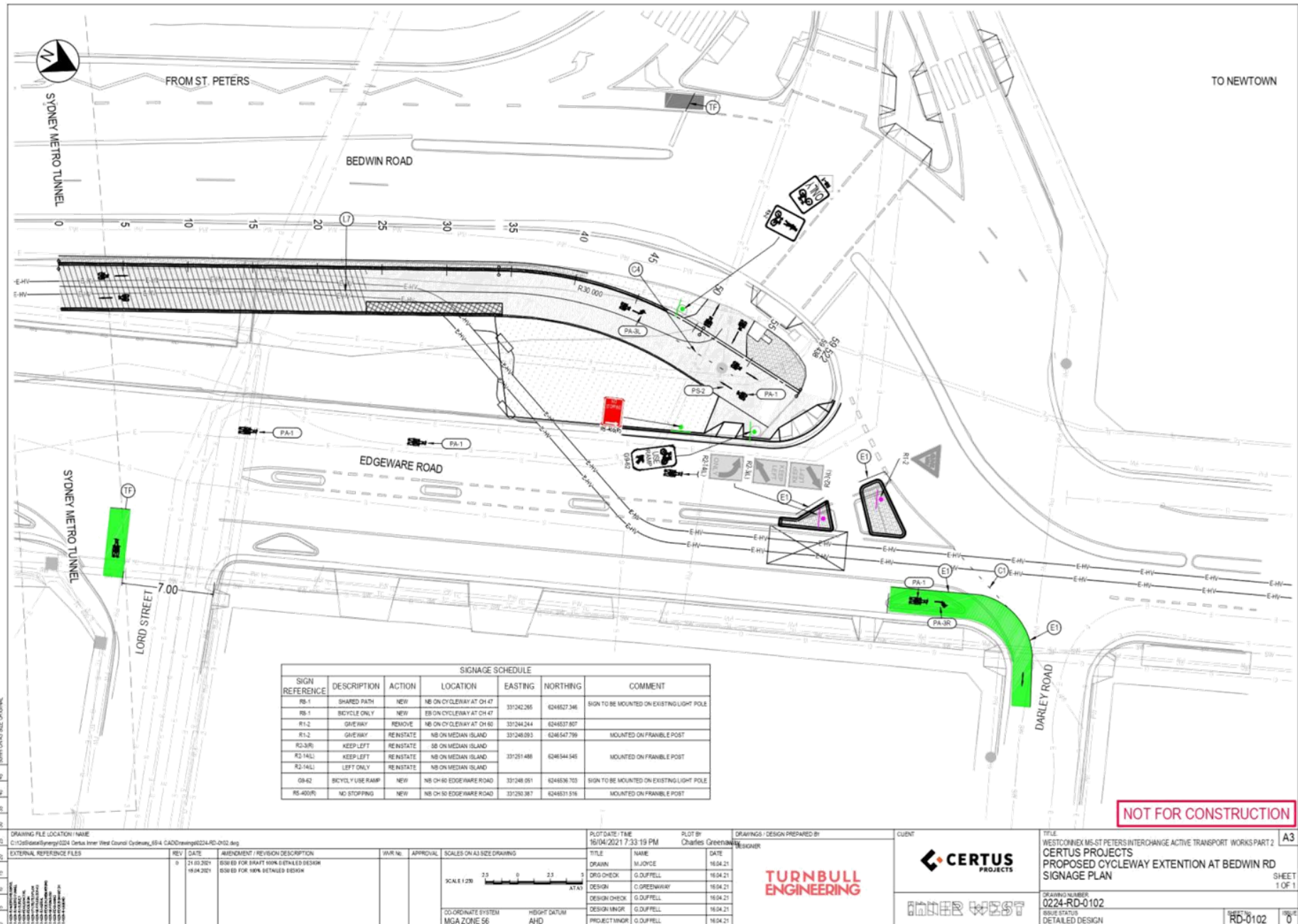
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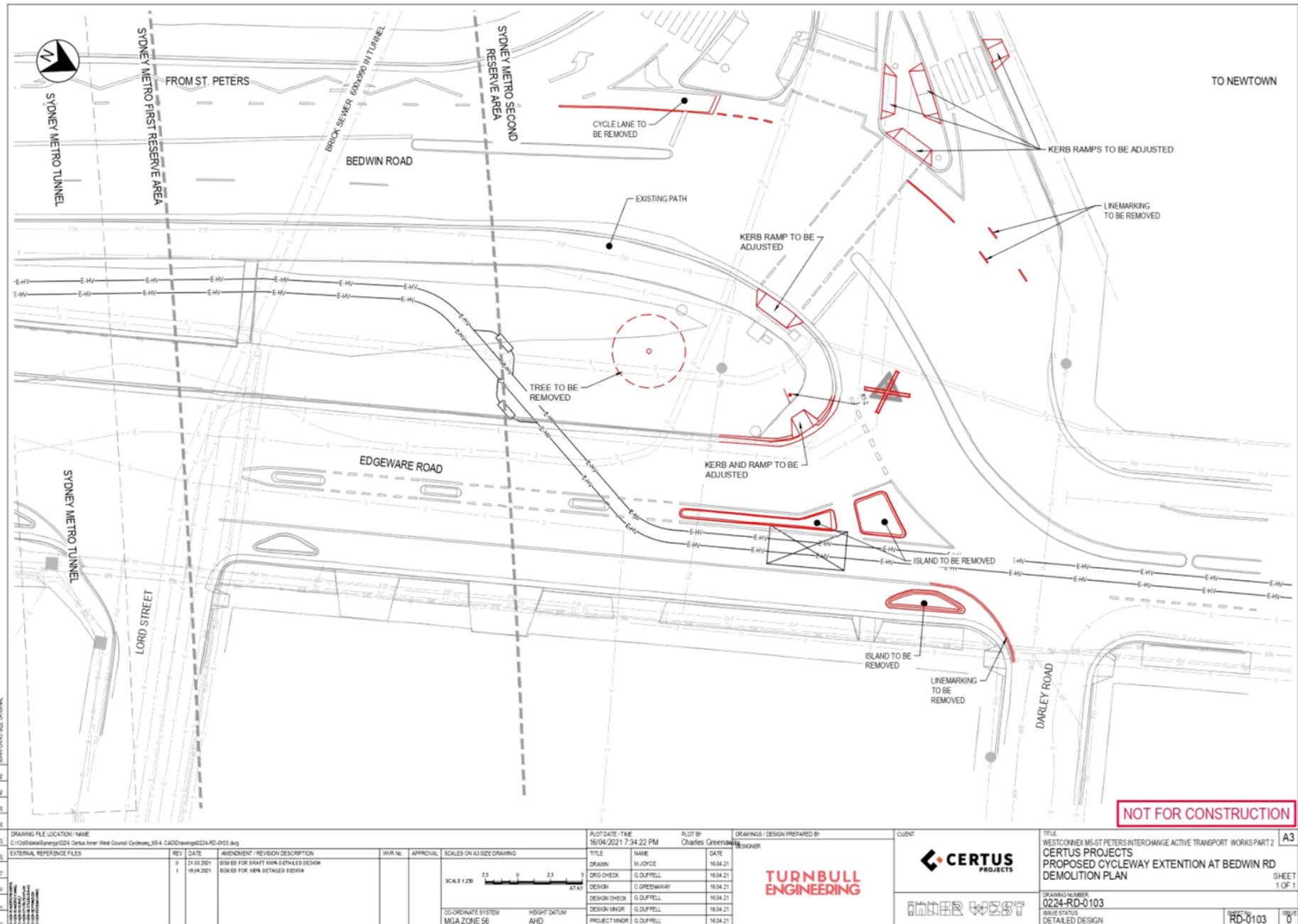
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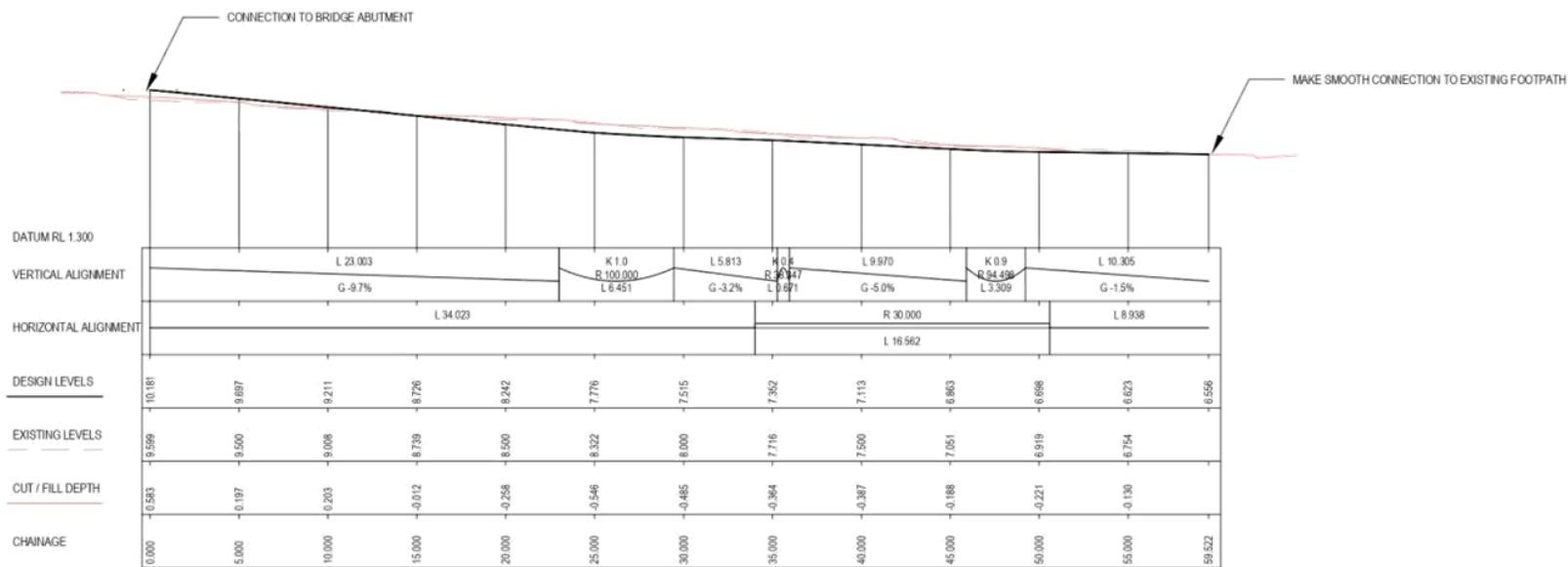
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										2		01.04.2021		DESIGNED FOR DRAFT 100% DETAILED DESIGN							
														DESIGNED FOR 100% DETAILED DESIGN							









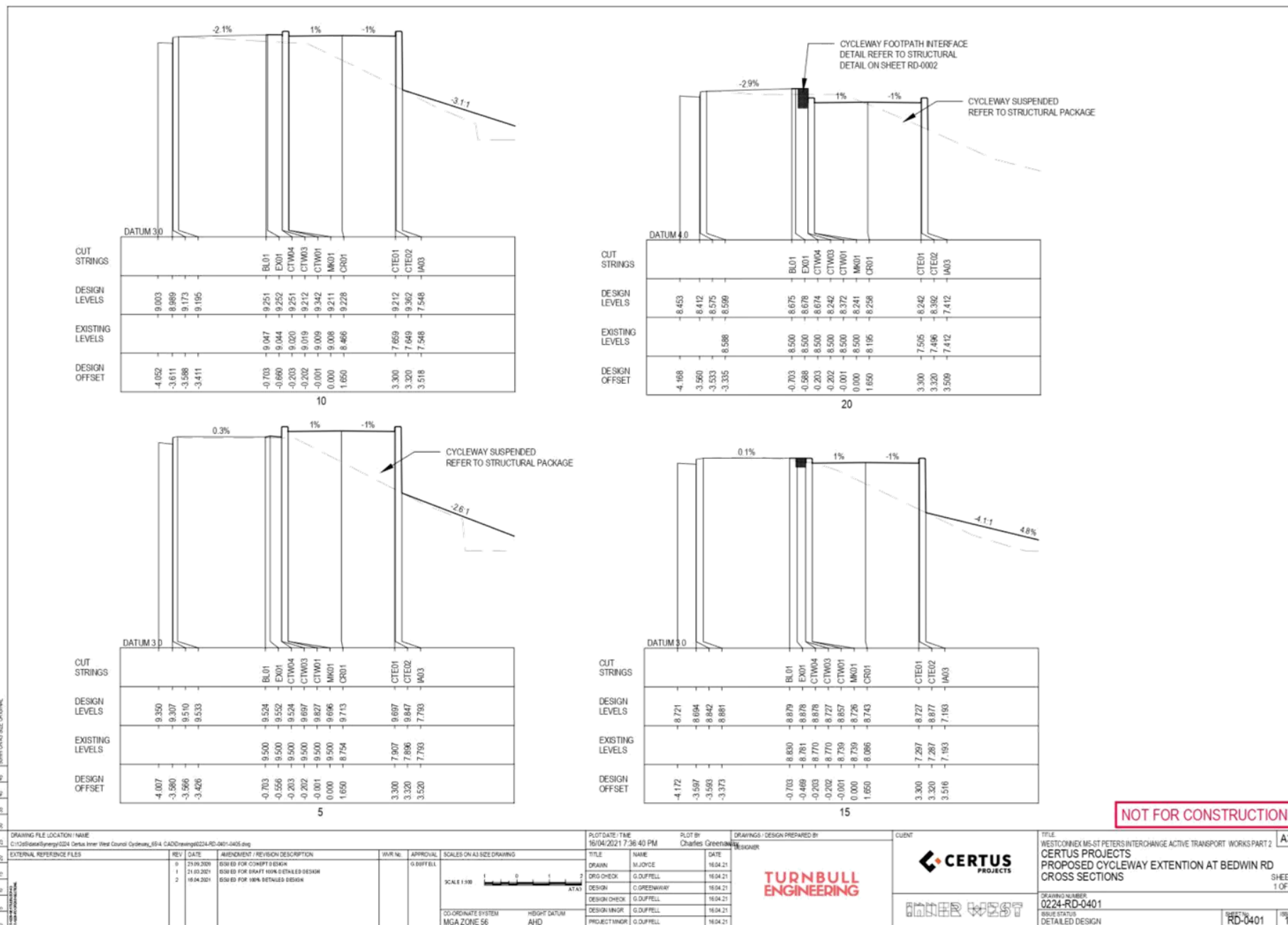


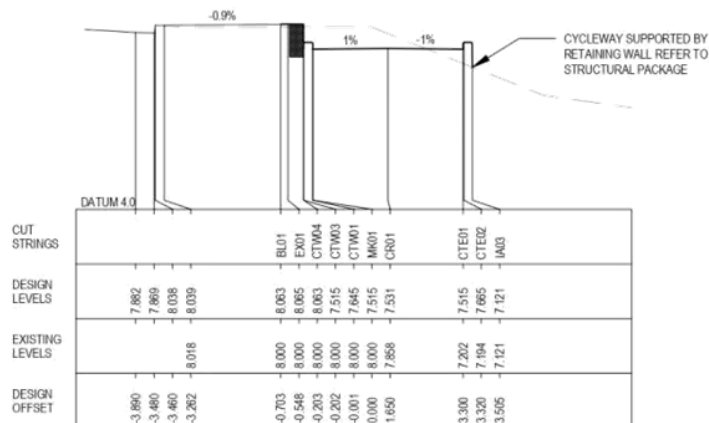
LONGITUDINAL SECTION CYCLEWAY - MK01  
HORIZONTAL 1:250  
VERTICAL 1:250

NOT FOR CONSTRUCTION

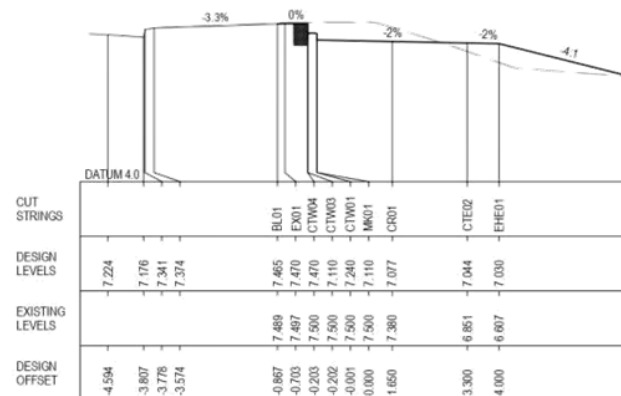
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		2 21/03/2021 ISSUED FOR SHARP HORN ESTIMATED DESIGN						DRG CHECK		G.DUFFELL		16/04/21		16/04/21	
		3 16/04/2021 ISSUED FOR 100% DETAILED DESIGN						DESIGN		G.DUFFELL		16/04/21		16/04/21	
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								PROJECT MGR		G.DUFFELL		16/04/21		16/04/21	
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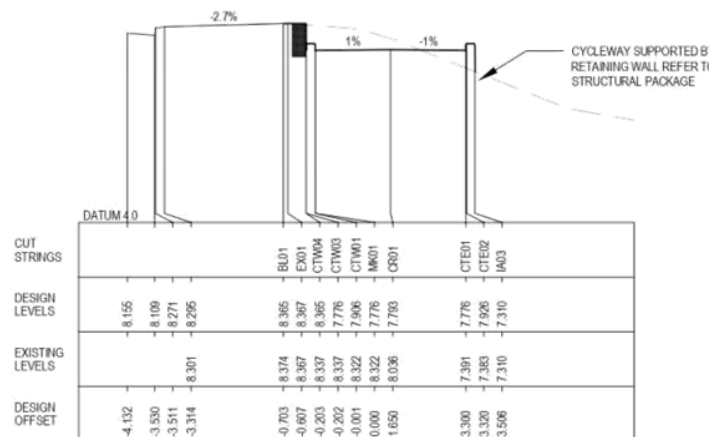




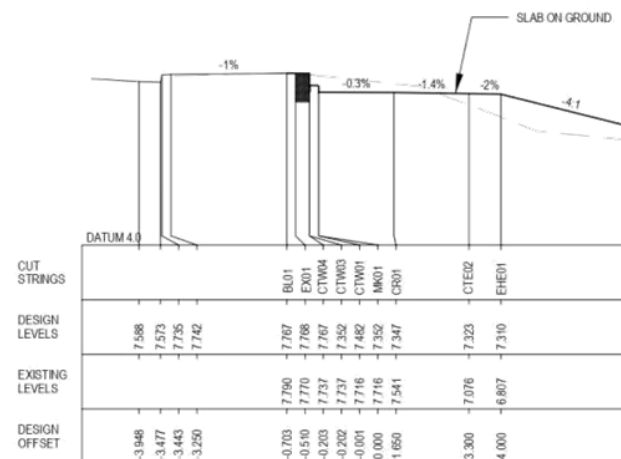
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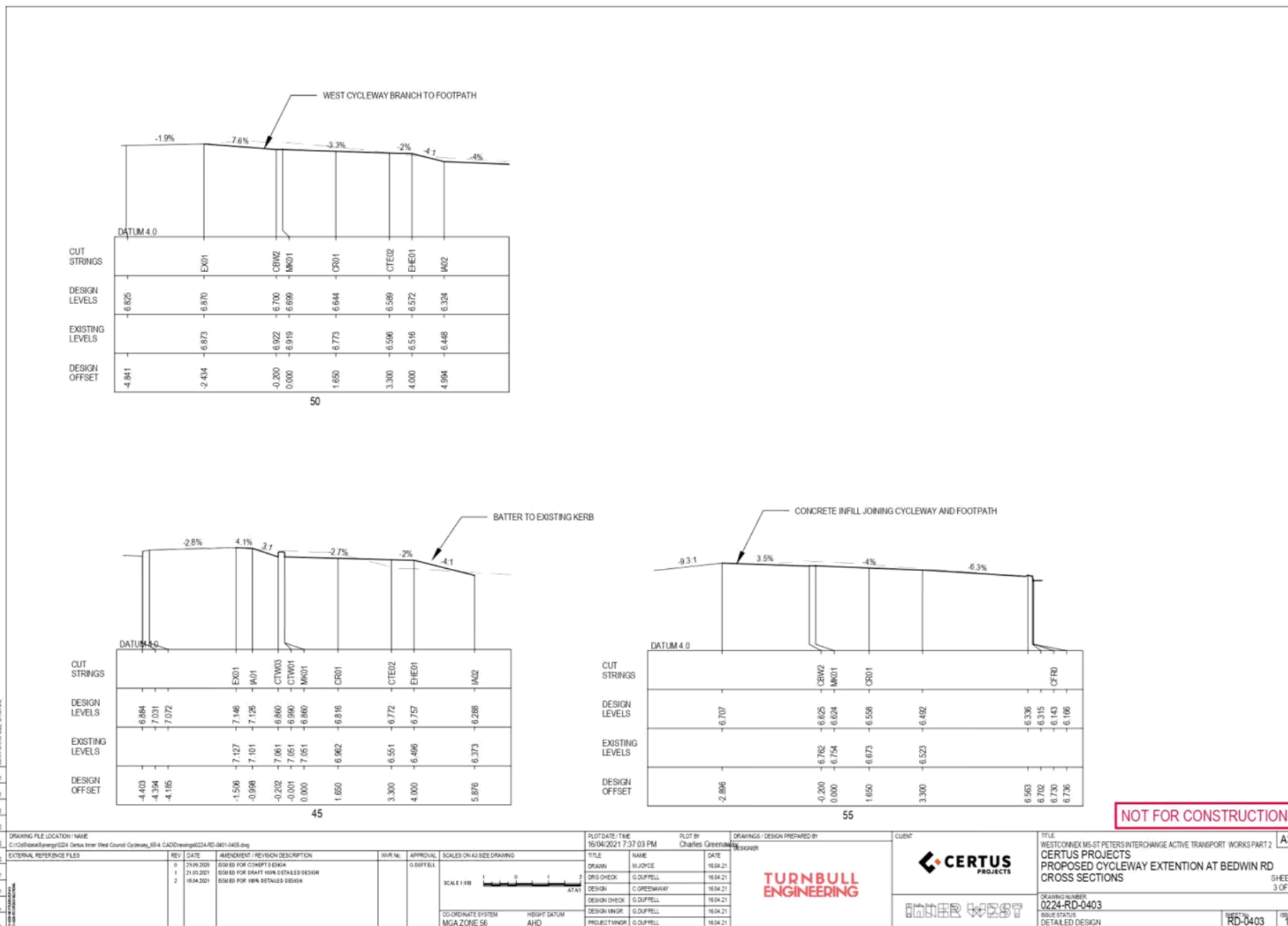
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EXTERNAL REFERENCE FILES				SCALE 1:100		CO-ORDINATE SYSTEM MGA ZONE 56		HIGHEST DATUM AHD		DRAWING NUMBER 0224-RD-0402		SHEET 2 OF 5		
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## MEMORANDUM

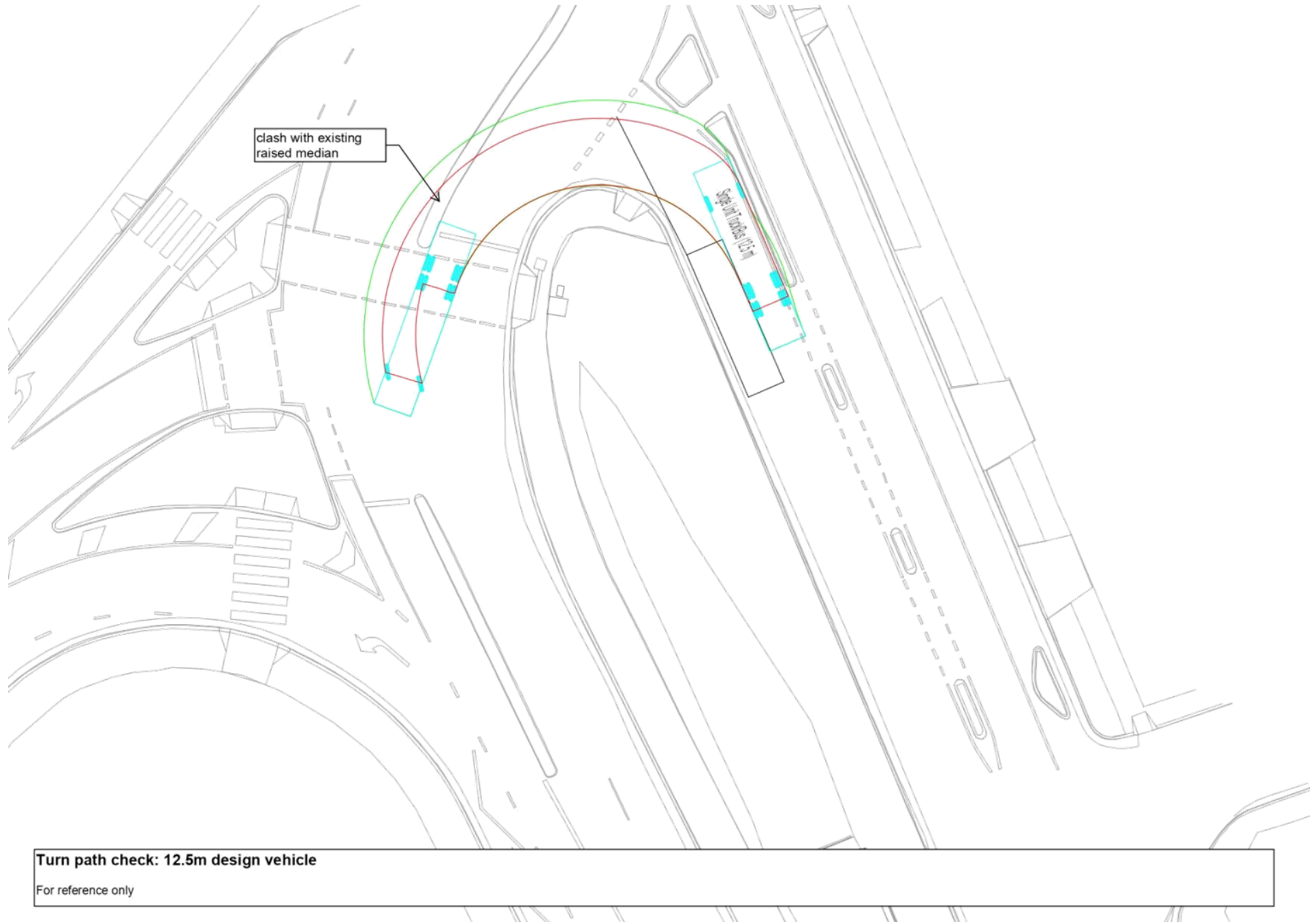
Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

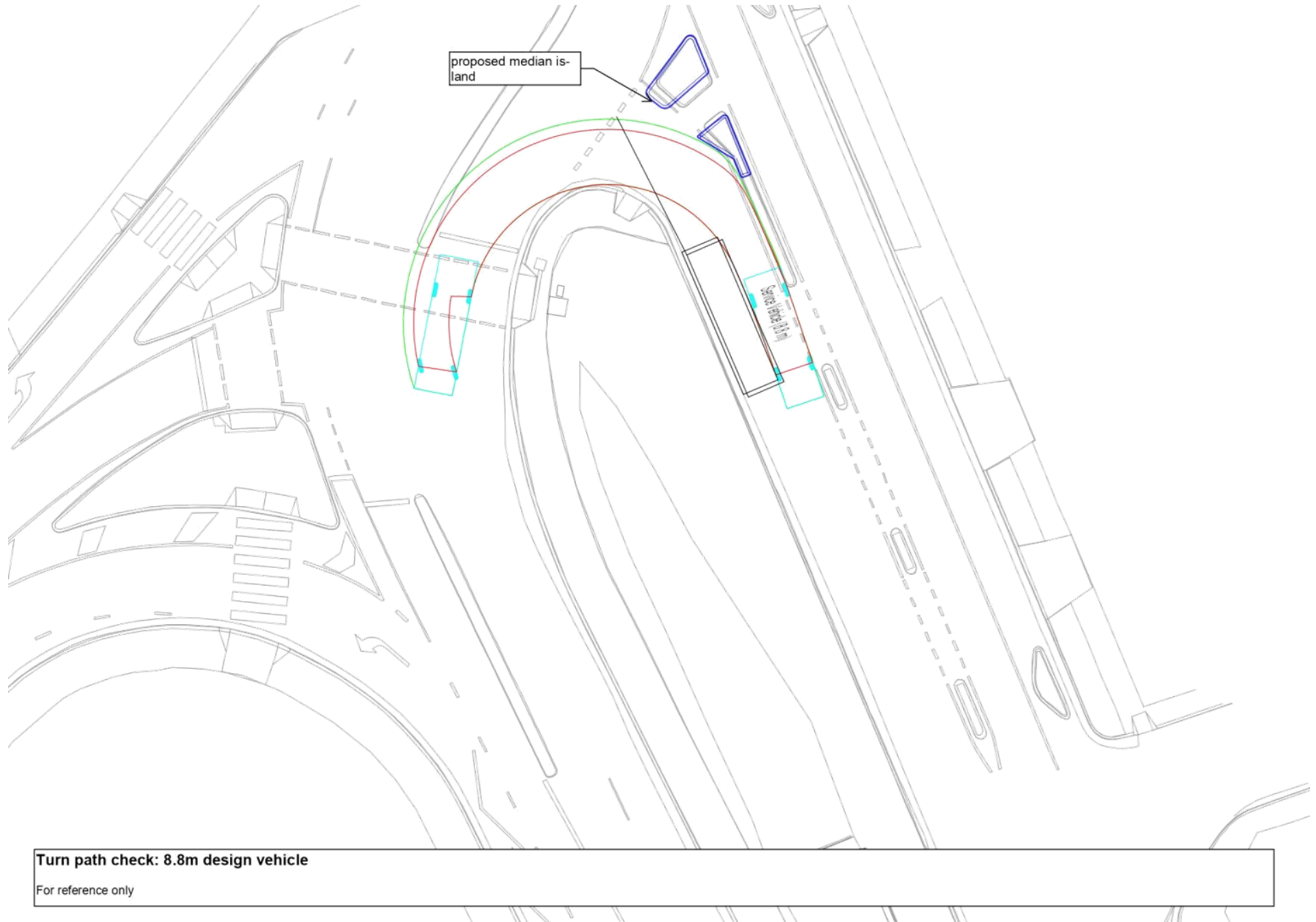
**TURNBULL  
ENGINEERING**

### Appendix C Vehicle Turn Path Checks

Item 1

Attachment 3





**Turn path check: 8.8m design vehicle**

For reference only

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

**TURNBULL  
ENGINEERING**

Appendix D Sydney Water BOA

Item 1

Attachment 3

## MEMORANDUM

Inner West Council – Proposed Cycleway Extension at Bedwin Rd  
Certus Projects

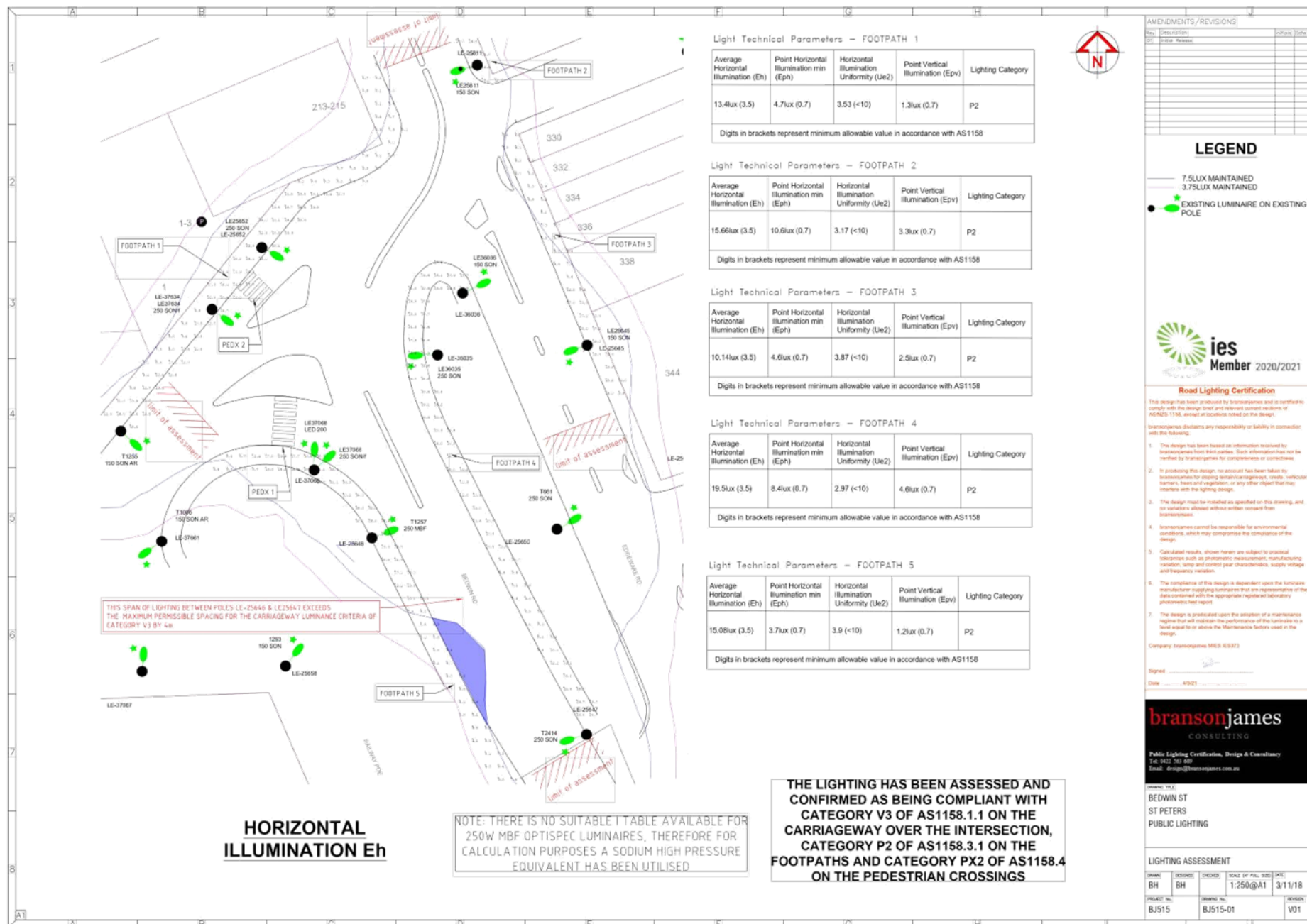
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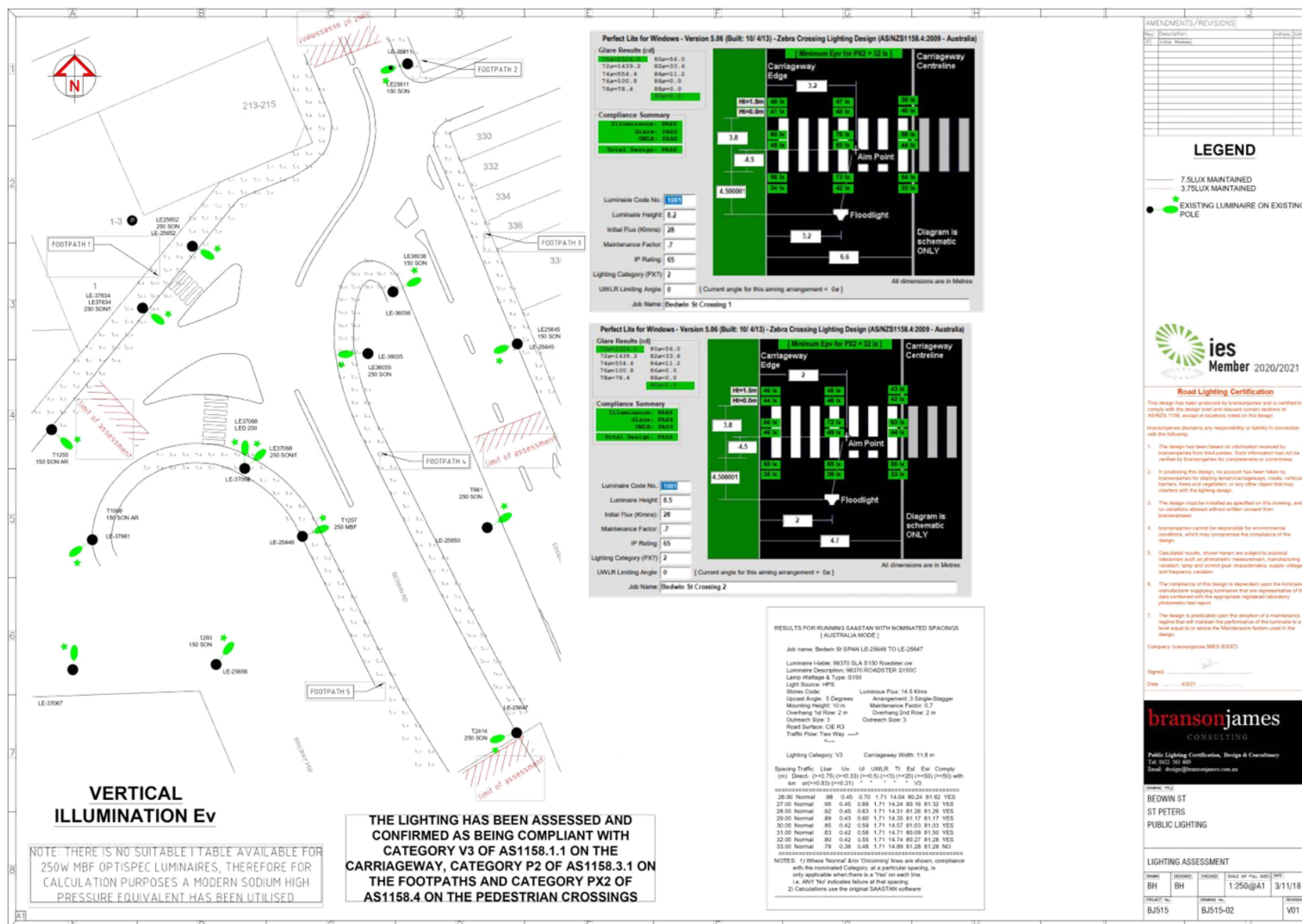
### Appendix E Lighting Compliance Check Drawings

Item 1

Attachment 3







**Item No:** LTC0521(1) Item 2

**Subject:** GARNET STREET AND DUDLEY STREET, DULWICH HILL; ILLAWARRA ROAD AND MARRICKVILLE ROAD, MARRICKVILLE; AND (LOWER) RAILWAY PARADE, SYDENHAM - BUS REPLACEMENTS DURING MAJOR RAIL SHUTDOWN - TEMPORARY PARKING CHANGES DURING T3 LINE UPGRADE FOR SYDNEY METRO - (MIDJUBURI -MARRICKVILLE WARD / SUMMER HILL ELECTORATE / INNER WEST PAC)

**Prepared By:** Jennifer Adams - Engineer – Traffic and Parking Services

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

Council has been notified by Transport for NSW (TfNSW) that Sydney Metro works will involve a major rail shutdown of the Sydenham to Bankstown rail line (3) between 28 June and 11 July 2021. During the shutdown buses will replace train services along the T3 line and to accommodate the increased bus movements and necessary holding areas some short-term parking changes are required at a number of locations.

Specifically, TfNSW is requesting approval for the temporary conversion of multiple parking spaces at the following locations: Garnet Street and Dudley Street, Dulwich Hill; Illawarra Road and Marrickville Road, Marrickville; and Railway Parade (lower section), Sydenham. It is recommended that no objections be raised, and Council approves the temporary short-term parking changes at the identified locations during the rail shutdowns.

## RECOMMENDATION

**THAT this report be received and noted and the following temporary short-term parking changes from Friday 25 June 2021 to Monday 12 July 2021 be approved and implemented by TfNSW:**

### Garnet Street - Hurlstone Park Station Precinct (4 parking spaces)

1. The short-term conversion of 24m (4 parking spaces) 'unrestricted parking' on the eastern side of Garnet Street (between The Parade and Ewart Street) to a 'Bus Zone M-F 6 am – 9:30 am and 3 pm – 11 pm'; be **APPROVED**, in order to provide a temporary bus layover area with adequate draw-in and out length;

### Dudley Street - Dulwich Hill Station Precinct (6 parking spaces)

2. The short-term conversion of 7m (1 parking space) 'Loading Zone 8:30 am – 6 pm Mon – Fri & 8:30 am – 12:30 pm Sat' on the northwestern side of Dudley Street (between Wardell Road and School Parade) to a 'Bus Zone'; be **APPROVED** in order to provide an additional bus bay with adequate draw-in length;
3. The short-term conversion of 18m (3 parking spaces) 'P30 min 8:30 am – 6 pm Mon – Fri & 8:30 am – 12:30 pm Sat' on the northwestern side of Dudley Street (between Wardell Road and School parade) to a 'Bus Zone'; be **APPROVED** in order to provide an additional bus bay with adequate draw-in length;
4. The short-term conversion of 18m (2 parking spaces) 'Works Zone 7 am – 5:30 pm Mon – Sat' on the southeastern side of Dudley Street (between School Parade and Wardell Road) to a 'Bus Zone'; be **APPROVED** in order to provide an additional bus bay with adequate draw-in length;

### Illawarra Road - Marrickville Station Precinct (4 parking spaces)

5. The short-term conversion of 15m (2 parking spaces) '1P 8:30 am – 6 pm' on the northwestern side of Illawarra Road (between Warburton Street and Greenbank Street) to a 'Bus Zone Mon-Fri only'; be APPROVED in order to provide an additional bus bay with adequate draw-in length;
6. The short-term conversion of 10m (2 parking spaces) 'No Parking 8:30 am – 5 pm Mon - Fri' on the northwestern side of Illawarra Road (between Warburton Street and Greenbank Street) to a 'Bus Zone Mon-Fri only'; be APPROVED in order to provide an additional bus bay with adequate draw-in length;

**Marrickville Road, Marrickville NSW 2204 (4 parking spaces)**

7. The short-term conversion of 24m (4 parking spaces) '1P 8:30 am – 6 pm' on the southeast kerb of Marrickville Road (between Frampton Avenue and Gladstone Street) to a 'Bus Zone 3pm – 9pm Mon-Fri only'; be APPROVED in order to provide an additional bus bay with adequate draw-in length;

**Lower Railway Parade - Sydenham Station Precinct (50 parking spaces)**

8. The short-term conversion of 69m (11 parking spaces) 'unrestricted parking' on the southeast kerb of Railway Parade (between Gleeson Avenue and Marrickville Road) to a 'Bus Zone'; be APPROVED in order to provide additional layover bus bays with adequate draw-in length;
9. The short-term conversion of 78m (28 parking spaces) 'unrestricted parking' on the northwest kerb of Railway Parade (Lower) (between Sydenham Road and Marrickville Road) to a 'Bus Zone'; be APPROVED in order to provide additional layover bus bays with adequate draw-in length;
10. The short-term conversion of 32m (11 parking spaces) '4P 8:30 am – 6 pm Mon - Fri' on the northwest kerb of Railway Parade (Lower) (between Gleeson Avenue and Marrickville Road) to a 'Bus Zone'; be APPROVED in order to provide additional layover bus bays with adequate draw-in length; and
11. The applicant and Council Rangers be advised in terms of this report.

## BACKGROUND

Sydney Metro City & Southwest - Sydenham to Bankstown project will upgrade all 10 stations between Marrickville and Bankstown to meet metro standards before converting the T3 Bankstown Line to Metro operations.

During this possession, rail services on the T3 Bankstown Line will not operate and Temporary Transport Plan (TTP) buses will operate instead necessitating some short-term changes in parking at a number of locations.

All changes to street signage will be made by TfNSW contractor(s) from 10pm the night before the closures and will be reinstated at the completion of the planned shutdowns.

## FINANCIAL IMPLICATIONS

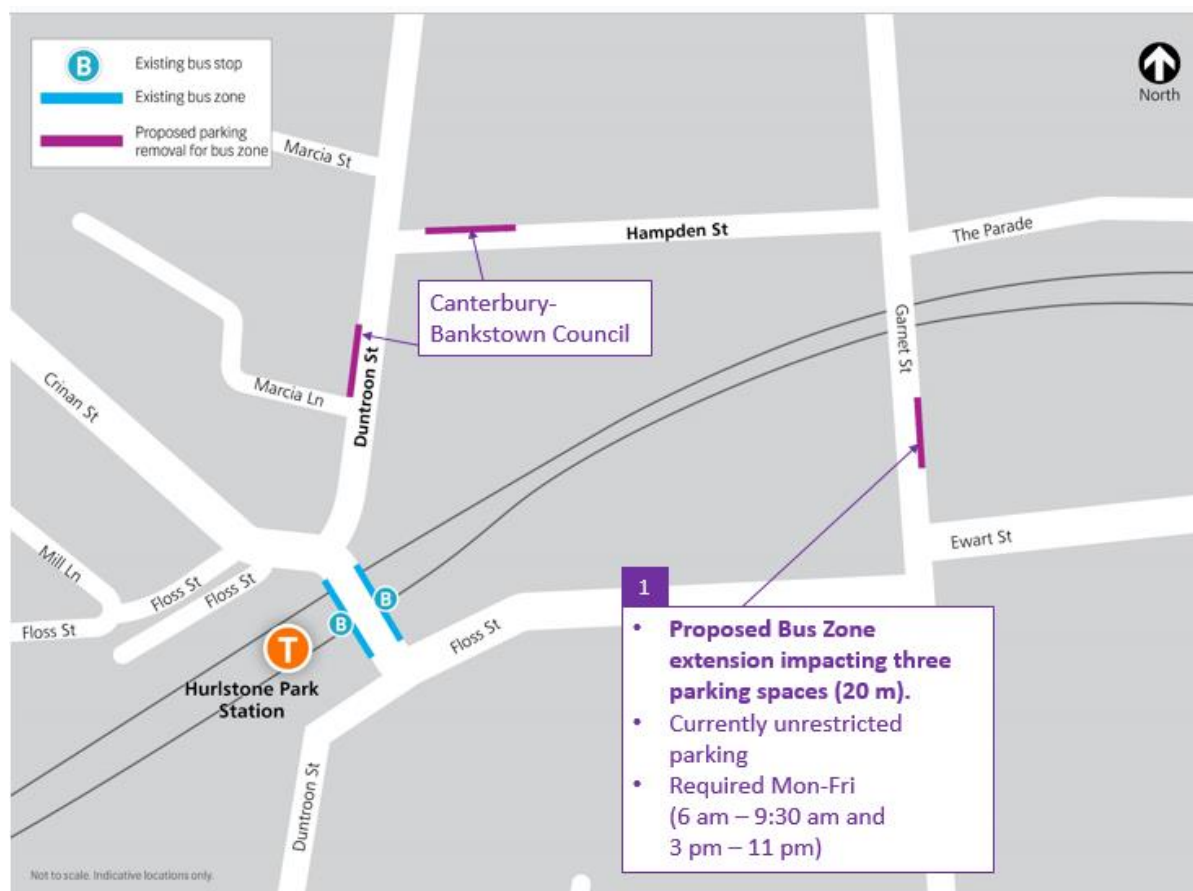
There are no financial implications for Council associated with this matter. The cost of the work will be borne by Transport for NSW.

## OFFICER COMMENTS

Each of the locations will be detailed separately.



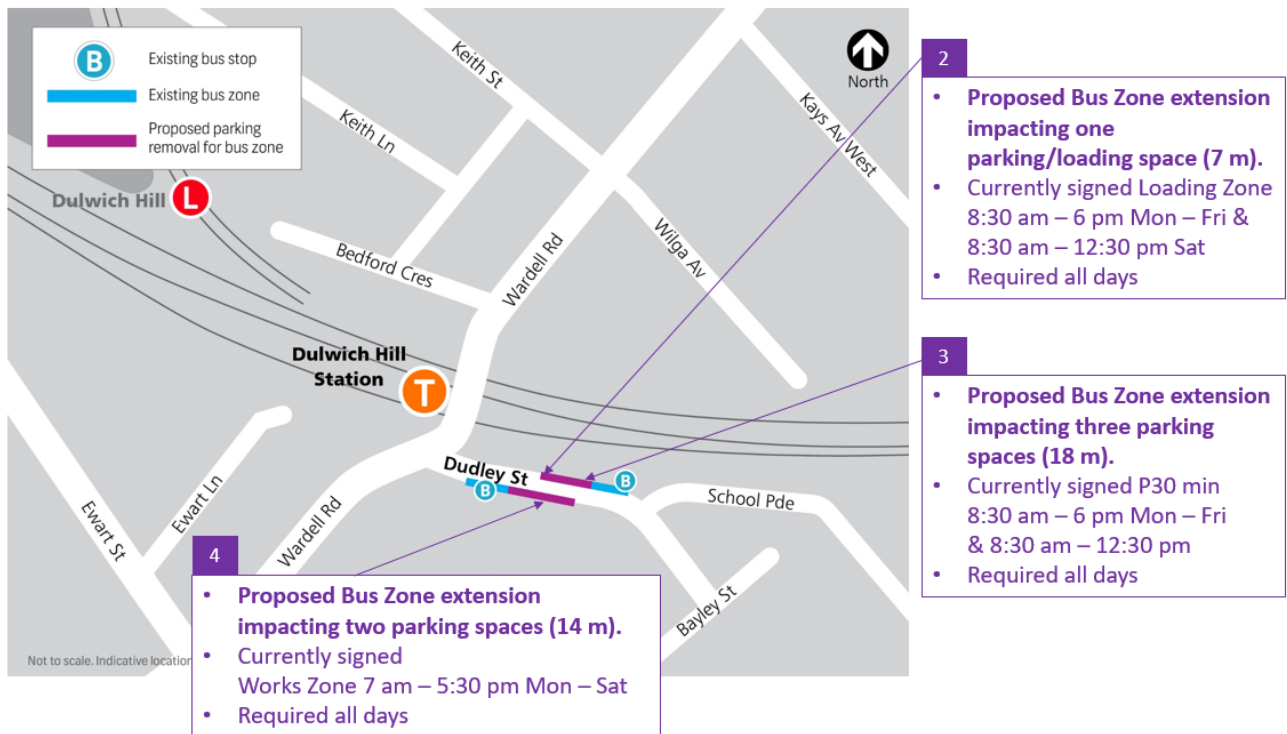
## Garnet Street - Hurlstone Park Station Precinct (4 parking spaces)



Item 2

- Garnet Street at Ewart Street, Hurlstone Park (Stop ID: 220370) – Bus Stop extension for peak-hour bus layover (3 parking spaces)
  - Garnet Street (east kerb between The Parade and Ewart Street)
  - Currently signed unrestricted parking
  - Proposed Temporary Bus Zone Mon-Fri (6 am – 9:30 am & 3 pm – 11 pm)
  - This proposed zone is to be used as a layover.
  - Proposed temporary change will impact approximately four parking spaces (24 m).
  - The proposed layover areas for Hurlstone Park Station have been separated into two locations to minimise local impacts but are still: (i) reasonably close to the active bus stops; and (ii) located along the route path used between the last and first stops.
- The bus stops listed below will also be used by TTP buses but require no regulatory signage changes.
  - Hurlstone Park Station, Floss Street (Stop ID: 219311)
  - Floss Street opposite Hurlstone Park Station (Stop ID: 219312)

## Dulwich Hill Station Precinct (6 parking spaces)



### 2. Section A of Dulwich Hill Station, Dudley Street (Stop ID: 220433) - Bus stop extension (1 parking space)

- Dudley Street (northwest kerb between Wardell Road and School Parade)
- Currently signed Loading Zone 8:30 am – 6 pm Mon – Fri & 8:30 am – 12:30 pm Sat
- Proposed Bus Zone extension is required for all days of the TTP bus operation
- Proposed temporary change will impact approximately one parking space (7m).
- This change is proposed to create the extra space required for the additional TTP buses using the stop

### 3. Section B of Dulwich Hill Station, Dudley Street (Stop ID: 220433) - Bus stop extension (3 parking spaces)

- Dudley Street (northwest kerb between Wardell Road and School Parade)
- Currently signed P30 min 8:30 am – 6 pm Mon – Fri & 8:30 am – 12:30 pm Sat
- Proposed Bus Zone extension is required for all days of the TTP bus operation
- Proposed temporary change will impact approximately three parking spaces (18m).
- This change is proposed to create the extra space required for the additional TTP buses using the stop

### 4. Dudley Street opposite Dulwich Hill Station (Stop ID: 220432) - Bus stop extension (2 parking spaces)

- Dudley Street (southeast kerb between School Parade and Wardell Road)
- Currently signed Works Zone 7 am – 5:30 pm Mon – Sat
- Proposed Bus Zone extension is required for all days of the TTP bus operation
- This proposed change is to create the extra space required for the additional TTP buses using the stop
- Proposed temporary change will impact approximately two parking spaces (14 m).

## Marrickville Station Precinct (4 parking spaces)





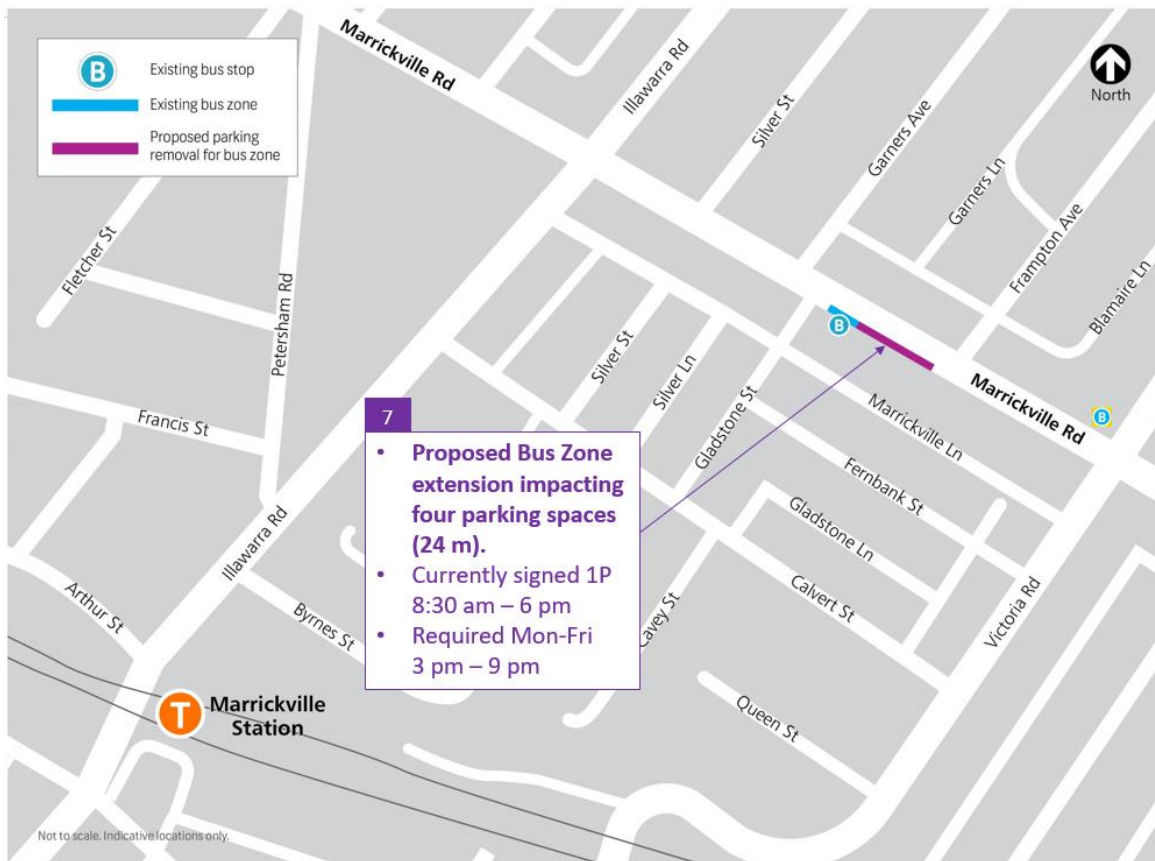
**5. Section A of Illawarra Road opposite Marrickville Station (Stop ID: 2204102) - Bus stop extension (2 parking spaces)**

- Illawarra Road (northwest kerb between Warburton Street and Greenbank Street)
- Currently signed 1P 8:30 am – 6 pm
- Proposed Bus Zone extension is required Mon-Fri only.
- Proposed temporary change will impact approximately two parking spaces (15 m).
- This change is proposed to create the extra space required for the additional TTP buses using the stop

**6. Section B of Illawarra Road opposite Marrickville Station (Stop ID: 2204102) - Bus stop extension (2 parking spaces)**

- Illawarra Road (northwest kerb between Warburton Street and Greenbank Street)
  - Currently signed No Parking 8:30 am – 5 pm Mon - Fri
  - Proposed Bus Zone extension is required Mon-Fri only.
  - Proposed temporary change will impact approximately two parking spaces (10m).
  - This change is proposed to create the extra space required for the additional TTP buses using the stop
- Marrickville Station, Illawarra Road (Stop ID: 2204101) will also be used by TTP buses but no regulatory signage changes are required.

Marrickville Road, Marrickville NSW 2204 (4 parking spaces)



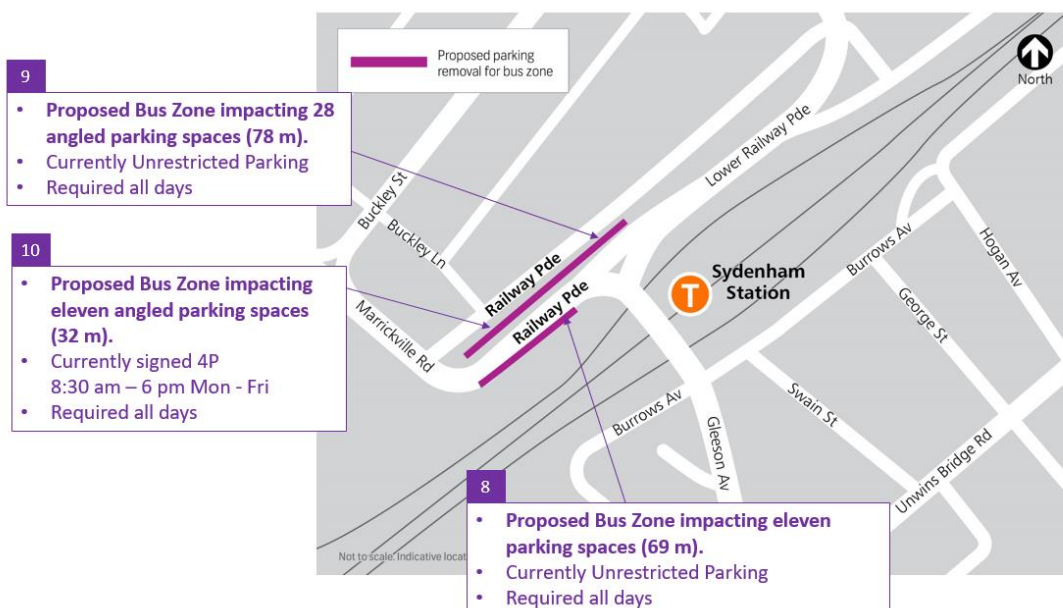
## 7. Marrickville Road at Gladstone Street (Stop ID: 220419) - Bus stop extension (4 parking spaces)

- Marrickville Rd (southeast kerb between Frampton Ave and Gladstone St)
- Currently signed 1P 8:30 am – 6 pm
- Proposed Bus Zone extension is required Mon-Fri 3 pm – 9 pm
- This proposed change is to create the extra space required for the additional TTP buses using the stop during the PM peak period.
- Proposed temporary change will impact approximately four parking spaces (24 m).
- Marrickville Road at Victoria Road (Stop ID: 220466) will also be used by TTP buses but no regulatory signage changes are required.

## Sydenham Station Precinct (50 parking spaces)

### 8. Railway Parade, Marrickville NSW 2204 – Layover Bus Zone (11 parking spaces)

- Railway Parade (southeast kerb between Gleeson Avenue and Marrickville Road)
- Currently Unrestricted parking
- Proposed Bus Zone is required for all days of the TTP bus operation
- This proposed change is to create the space required for buses to layover. This location is proposed as it is (i) reasonably close to the active bus stop; and (ii) located along the route path used between the last and first stops.
- Proposed temporary change will impact approximately eleven parking spaces (69 m).



9. Section A of Railway Parade (Lower) angled parking, Marrickville NSW 2204 – Layover Bus Zone (28 parking spaces)
  - Railway Parade Parallel Parking (northwest kerb between Sydenham Road and Marrickville Road)
  - Currently Unrestricted parking
  - Proposed Bus Zone is required for all days of the TTP bus operation
  - Proposed temporary change will impact approximately 28 angled parking spaces (78 m).
  - This proposed change is to create the space required for buses to layover. This location is proposed as it is (i) reasonably close to the active bus stop; and (ii) located along the route path used between the last and first stops.
10. Section B of Railway Parade (Lower) angled parking, Marrickville NSW 2204 – Layover Bus Zone (11 parking spaces)
  - Railway Parade Parallel Parking (northwest kerb between Gleeson Avenue and Marrickville Road)
  - Currently 4P 8:30 am – 6 pm Mon - Fri
  - Proposed Bus Zone is required for all days of the TTP bus operation
  - Proposed temporary change will impact approximately eleven angled parking spaces (32 m).
  - This proposed change is to create the space required for buses to layover. This location is proposed as it is (i) reasonably close to the active bus stop; and (ii) located along the route path used between the last and first stops.

## PUBLIC CONSULTATION

A Consultation Outcomes Report will be prepared collating feedback received as part of TfNSW consultation process. The report will be submitted prior to the May 2021 Traffic Committee meeting.

## ATTACHMENTS

Nil.

**Item No:** LTC0521(1) Item 3

**Subject:** ELIZA STREET, NEWTOWN - PROPOSED 'MOTOR BIKE ONLY' RESTRICTIONS (DAMUM-STANMORE WARD / NEWTOWN ELECTORATE / INNER WEST PAC)

**Prepared By:** Jennifer Adams - Engineer – Traffic and Parking Services

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

Council has received concerns regarding inadequate motorbike parking in the Newtown area in the vicinity of Lennox Street and/or Eliza Street, Newtown. A proposal to install a 4.0 metre 'Motor Bike Only' parking zone on the eastern side of Eliza Street south of its intersection with Lennox Street, Newtown was distributed to local residents and subsequent to the feedback and observation it is now recommended that the location of the dedicated motorbike parking be located around the corner on Lennox Street outside No.80 Lennox Street. This report provides the results of the motorbike parking investigation.

## RECOMMENDATION

**THAT a length of 4.0m of 'Motor Bike Only' parking be installed on the southern side of Lennox Street, outside No.80 Lennox Street, Newtown starting from the existing 'No stopping' restrictions and replacing the existing 2P restrictions.**

## BACKGROUND

At the February 2021 LTC meeting in 'General Business' an item was listed regarding motorbike parking in Lennox Street, Newtown (LTC0221 Item 15). A resident had requested up to one car space between 38-84 Lennox Street, Newtown be converted to motorbike parking, preferably for M3 permit holders. The resident stated that dedicated motorbike parking in the street would allow for the available parking space to be used more efficiently, improve road safety and encourage the use of more sustainable transport. Council officers have now reviewed this request and have now prepared this report in response.

## OTHER STAFF COMMENTS

Council generally only seeks to install motorbike parking in locations where there is redundant kerb space that could otherwise only accommodate small vehicles such as a motorcycle (e.g. in short spaces between driveways). Thus, in response to the request for "up to one car space to be converted to motorbike parking", Council Officers surveyed the locality for redundant kerb space and the most viable location nearest Lennox Street was around the corner in Eliza Street. A reduction in statutory 10m 'No Stopping' was also sought.

A community engagement letter of a proposal to install a 4.0 metre "Motor Bike Only" parking zone on the eastern side of Eliza Street south of its intersection with Lennox Street, Newtown was distributed to local residents. Refer to the diagram below.





On further examination of the proposed site for the dedicated motorbike parking in Eliza Street revealed that the slope of the road is not conducive to parking motorcycles due to a concern that motorbikes could topple over. Motorbike parking areas generally should have limited gradients in order to facilitate manoeuvrability and to ensure the motorcycle is unlikely to topple over. Other safety considerations should include the actual process of manoeuvring a motorbike whilst parking and personal safety at or around the location. Thus, along with the feedback from residents it became apparent Eliza Street was not the optimum location for the proposed dedicated Motorbike only parking in the locality.

Thus, a more suitable alternative location is now recommended in Lennox Street outside No.80. Refer to the diagram below.



## FINANCIAL IMPLICATIONS

The costs of installation of the parking restrictions as recommended can be funded within Council's signs and line marking budget.

## PUBLIC CONSULTATION

A letter outlining the initial proposal to provide a length of dedicated Motorbike only parking in Eliza Street, Newtown was hand delivered to affected properties (32 properties) in Eliza Street and Lennox Street, Newtown requesting residents' and businesses' views regarding the proposal.

Five (5) responses were received. Generally, all responses were positive that dedicated motorbike parking was being considered in Newtown. One business in Eliza Street requested that the location be revised to be adjacent to their driveway helping with access to their property often hindered by vehicles parking over the start of their driveway. It is noted that there are alternative measures available to remedy such issues, like widening a driveway and/or installing driveway lines (which are already present).

One resident preferred a location behind the Denby Theatre or any location not adjacent to resident premises. Other locations were also suggested however the most preferred suggested location was cited as Lennox Street between Mary Street and Eliza Street, Newtown. Accordingly, it is considered viable and recommended that a section of dedicated motorbike parking be located on Lennox Street outside No.80 Lennox Street, Newtown.

## CONCLUSION

This recommendation will add to the provision of dedicated 'Motor Bikes only' parking in the Newtown/Enmore area as a result of Council's strategic processes. This includes 6 metres of 'Motorcycle Parking Only' installed in Simmons Street, Enmore in 2010 as a result of an adopted recommendation in 2010 the Newtown-Enmore Parking Study. In addition, in 2018, 6 motor bike spaces were provided as part of Shared Zone project in Clara Street, Newtown and with the recent upgrade of the Lennox Street car park, corner of Church Street and Lennox Street, Newtown there are now 3 marked out dedicated 'Motor Bike Only' spaces. These spaces were provided as part of a parking study or as part of a design and community engagement process.

## ATTACHMENTS

Nil.



**Item No:** LTC0521(1) Item 4

**Subject:** HOLLANDS AVENUE, MARRICKVILLEWAY STREET, TEMPE – REQUEST FOR 'NO PARKING' RESTRICTIONS (MIDJUBURI – MARRICKVILLE WARD / SUMMER HILL ELECTORATE / INNER WEST PAC)

**Prepared By:** Jennifer Adams - Engineer – Traffic and Parking Services

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

Currently vehicles, including Council Waste Services vehicles, are having difficulty in manoeuvring around the bend in Hollands Avenue, Marrickville due to vehicles being parked near the bend therefore Council is proposing to install a 20 metre length of 'No Parking' restrictions on the inner side of the bend in Hollands Avenue, Marrickville to improve access in the street.

## RECOMMENDATION

**THAT a 20 metre length 'No Parking' zone be installed on the inner bend in Hollands Avenue, Marrickville (northern end starting opposite No.52 extending eastern side to the midpoint of property No. 17) be approved in order to improve access in the one-way street.**

## BACKGROUND

It has been reported to Council that vehicles, at times, are having difficulty in manoeuvring around the bend in Hollands Avenue, Marrickville due to vehicles being parked near/on the bend. Hollands Avenue is a local street with one-way restrictions in a southern direction and is approximately 7.3 metres in width with unrestricted parking permitted on both sides.

A consultation letter was sent out to adjoining properties informing them that Council is proposing to install a 24 metre length of 'No Parking' restrictions on the inner side of the bend in Hollands Avenue, Marrickville to improve access in the street (refer to the plan below). The length of 'No Parking' zone was reduced to 20m upon further review by Council Officers.

### Proposed 'No Parking' restrictions on the inner bend in Hollands Avenue, Marrickville



## FINANCIAL IMPLICATIONS

The costs of installation of the parking restrictions as recommended can be funded within Council's signs and line marking budget.

## PUBLIC CONSULTATION

21 letters were sent to owners / occupiers of surrounding properties in Hollands Avenue, Marrickville. One (1) response was received. It was in general agreement that vehicles have difficulty negotiating the bend in the road when other vehicles are parked near the bend. It is noted that parking too close to an intersection can impede a vehicle from turning safely and thus, it is recommended that the 'No Parking' restrictions be approved and installed.

## ATTACHMENTS

Nil.

**Item No:** LTC0521(1) Item 5

**Subject:** LOCAL BICYCLE ROUTE 3 (LR03) - FRAZER PARK TO MARRICKVILLE ROAD – AMENDMENT TO DETAILED DESIGN PLANS AT TWO INTERSECTIONS (MARRICKVILLE WARD / SUMMER HILL ELECTORATE / INNER WEST LAC)

**Prepared By:** Jennifer Adams - Engineer – Traffic and Parking Services

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

It is proposed to make a minor amendment to the approved detailed design plans for Local Route 3 (LR03) bicycle route at two separate intersections. The current approved construction plans only specify resurfacing treatments with AC10 at George Street/Livingstone Road and Pile Street/Livingstone Road intersections and it is proposed, for consistency, to match those intersections with a similar treatment to that of the existing stamped AC “Duratherm” finish entry treatment at Enfield Street/Livingstone Road, Marrickville. It is recommended that the proposed detailed design plan amendments be approved.

## RECOMMENDATION

**THAT an amendment to the detailed design plans for LR03 at the intersections of George Street/Livingstone Road and Pile Street/Livingstone Road to incorporate a stamped AC “Duratherm” finish entry treatment be approved.**

## BACKGROUND

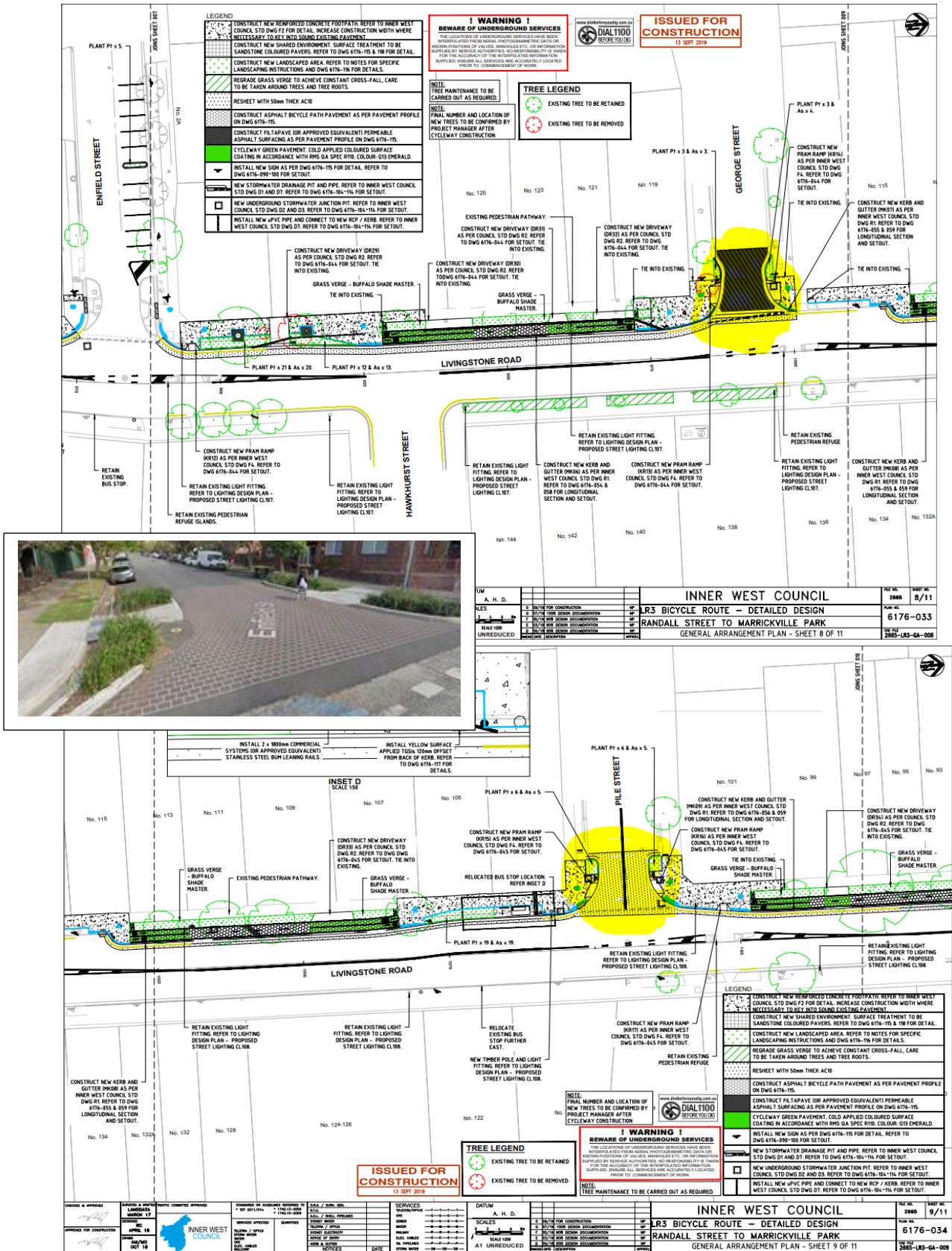
In November 2018, Council approved final detailed design plans for Local Route 3 (LR03) bicycle route and works have now commenced. LR3 is located along Livingstone Road, from Randall Street to Marrickville Park. For consistency along the cycle route it is proposed to amend approved design plans to provide a more consistent entry treatment along three consecutive intersections. The approved design plans will be amended accordingly, and the concept of the proposed entry changes is now presented to the Committee for endorsement.

## FINANCIAL IMPLICATIONS

The additional cost of the two proposed entry treatments is \$50,000 which can be funded within the existing budget.

## PUBLIC CONSULTATION

The project has already been out to consultation during the concept stage and detailed design stage.



ATTACHMENTS

Nil.

**Item No:** LTC0521(1) Item 6

**Subject:** BAY RUN UPGRADE AND DIVERSION ROUTE (GULGADYA-LEICHHARDT AND BALUDARRI-BALMAIN/SUMMER HILL AND BALMAIN ELECTORATES/BURWOOD AND LEICHHARDT PAC)

**Prepared By:** Ryan Hawken - Project Manager Greenway Delivery

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

Council is proposing upgrade of the Bay Run between UTS Haberfield Rowers Club and Lilyfield Road, including the Lilyfield Road bridge. Due to the limited width between the City West Link and Iron Cove, a diversion route for all pedestrians and cyclists is proposed for the duration of the works to facilitate construction.

## RECOMMENDATION

### THAT:

1. The plans for the proposed Bay Run upgrade be approved for construction.
2. The proposed diversion plan for pedestrian and cyclists for works along the Bay Run be approved.
3. A request be made to Transport for NSW to consider the introduction of an eastern pedestrian leg at the Intersection of City West Link, Timbrell Drive and Mortley Ave as part of future upgrades planned for the intersection

## BACKGROUND

In August 2018 Council adopted the Cooks to Cove Greenway Master Plan. Based on the adopted Master Plan concept designs were developed and endorsed by Council in May 2019. Council is now developing detailed design plans to facilitate construction.

Part of the works include upgrade of the Bay Run between UTS Haberfield Rowers Club and Lilyfield Road, including the Lilyfield Road bridge and the link from the Bay Run into Richard Murden Reserve.

Due to the limited width between the city west link and Iron Cove, to facilitate construction, a diversion route for all pedestrians and cyclists is proposed for the duration of the works.

## FINANCIAL IMPLICATIONS

All works are funded from the Greenway budgets. Grant funding is planned to be sought from Transport for NSW in mid-2021 under the Transport for NSW Walking and Cycling Program. Construction is planned from mid-2022.

## OFFICER COMMENTS

### *Bay Run Upgrade Plans*



Refer **Attachment 1** for proposed Bay Run Upgrade Plans. The main features of the Bay Run upgrade are:

- Modifications to the UTS Haberfield club forecourt and carpark to reduce conflict in this area between various users.
- Widening of the path from UTS Haberfield club to Lilyfield Road bridge to a 4.8m wide, visually separated asphalt path, with 2.4m wide pedestrian path and 2.4m wide cycle path, including new lighting
- A new 4.5m wide ramp up to the Lilyfield Road Bridge and new 3.5m ramp down to the Greenway path and Richard Murden Reserve
- Upgrade of the Lilyfield Road bridge including a separate 2.5m pedestrian path, separate 3.0m cycle path (relocated to southern side of bridge), plus landscaping, exercise equipment, furniture, shelter and lighting.
- Note the 3.0m wide cycle path will provide intermittent service vehicle access to Jemena regulator station on the western side of the bridge, generally as per existing access.
- Conversion of the end of Lilyfield Road into a shared zone with a turning circle adjacent to the access to 91 Canal Road
- Conversion of Canal Road south of 91 Canal Road into a shared path.
- Note the shared path will provide intermittent access to the existing public boat ramp for authorised SES, TfNSW and Sydney Water vehicles only. The existing public boat ramp is proposed to be closed to the public as it is rarely used due to limited space to manoeuvre and only being usable a few hours either side of high tide.
- Upgrade of the path from the Bay Run into Richard Murden Reserve to a 3.5m wide shared concrete path with lighting to match recent upgrades in Richard Murden Reserve.

TfNSW is the owner of the Lilyfield Road Bridge and Council provides maintenance of the bridge surface which is a public reserve. TfNSW have previously advised that any works to the bridge must be load neutral, i.e. not increase loads on the bridge, and the design has been prepared accordingly.

UTS Haberfield club has previously provided advice that they are supportive of the upgrade and emphasised the need to retain the existing car parking. The proposed design results in the loss of one car parking space in the existing car park, but provides one additional loading zone to facilitate deliveries to the club as well as to load/unload boat trailers.

A number of informal parking spaces on the road verge will be removed on Canal Road.

## **Bay Run Diversion Route Plans**

Upgrade of the area between UTS Haberfield club and the Lilyfield Road bridge means there will be no room for pedestrians and cyclists to travel adjacent to the proposed works area due to the proximity of the City West Link and Iron Cove.

Whilst pedestrian and cyclist access will be maintained along the Bay Run as much as possible, for significant periods of the construction it will be necessary to divert all pedestrians and cyclists around the site.

Refer **Attachment 2** for proposed diversion route plans and **Attachment 3** for an overview of the diversion route. The main features of the diversion route are:

- Minor improvements to the intersection of City West Link, Timbrell Drive and Mortley Avenue (refer discussion below).
- Marked up-hill shoulder lane and mixed traffic down-hill lane on Mortley Avenue. This incorporates relocation of the bus stop and modifications to the Robson Park entrance



at Mortley Avenue. Bus stop relocation has been reviewed and approved by the operator.

- Extension of Robson Park path network from the existing paths to Boomerang Street by way of a new permanent 2m wide concrete path.
- Provision of a new crossing point at Boomerang Street.
- Conversion of the path link from Boomerang Street to Dobroyd Parade to a shared path by way of new permanent shared path signage.
- A temporary shared path along Dobroyd Parade.
- Temporary closure of the one-way link between Dobroyd Parade and Hawthorne Parade.
- A temporary shared path along Hawthorne Parade linking into Richard Murden Reserve.
- To allow users to better plan their route, advanced warning signs will be installed to provide early notice that the diversion will be in place.

Access to the UTS Haberfield club would be retained from the café and along the existing path west of the club and from the carpark only. There would be no pedestrian access from the east.

From Timbrell Drive, pedestrians would be diverted through Robson Park, along part existing and part new path, and then along an existing path through the park lands adjacent to the City West Link. From here pedestrians would travel along a temporary shared path in Dobroyd Parade and Hawthorne Parade, through Richard Murden and along Canal Road to re-join the Bay Run at Maliyawul Street. The total length of the diversion would be 1.6km. This compares to the regular route of 1.0km. Having to cross three pedestrian legs at the City West Link intersection will also result in approximately a 5 minutes delay for Bay Run users. (refer discussion below)

The path through Robson park is only 2.1m wide and would not be suitable for the high numbers of cyclists using the Bay Run. Instead experienced cyclists would be encouraged up Mortley Avenue, down Boomerang Street and along Crescent Street. From here cyclists would travel along Dobroyd Parade, Hawthorne Parade, and Canal Road to re-join the Bay Run at Maliyawul Street. Commuter cyclists may also wish to utilise alternate routes such as Waratah Street.

The low traffic numbers are generally suitable for an on-road route although it is noted that less confident riders may ride through Robson Park and along the shared path following the same route as pedestrians.

Based on historic traffic counts, traffic volumes along the proposed diversion route are as follows:

- Mortley Avenue: 5,500 to 6,000 AADT
- Boomerang Street (north of Mortley Avenue): 1,200 to 2,000 AADT
- Crescent Street: 500 to 600 AADT
- Tillock Street: 500 to 600 AADT
- Hawthorne Parade to Dobroyd Parade one-way link: 255 AADT

All roads except Mortley Avenue have low traffic volumes suitable for an on-road cycle route. Mortley Avenue forms part of an existing cycle route, but existing conditions are suboptimal due to the mixed traffic and high traffic volumes combined with a significant incline.

To improve the cycling environment an uphill shoulder lane and downhill mixed traffic arrangement is proposed.

## Improvements to the intersection of City West Link, Timbrell Drive and Mortley Ave

The Road Safety Audit identified the potential to short cut across the eastern leg of the intersection as a risk. Given the high number of Bay Run users likely to cross at the intersection, this remains a risk. Having to cross three pedestrian legs at the City West Link intersection will also result in approximately a 4 to 5 minute delay for Bay Run users, which will in turn encourage short cutting.

Introduction of an eastern pedestrian leg on the signalised intersection would remove the above risks and this is Council's preference. Transport for NSW have indicated that they do not currently support the introduction of a marked foot crossing on the eastern side of the intersection due to impact this would have on traffic efficiency.

Consultation with Transport for NSW has indicated that they plan to upgrade the intersection of City West Link, Mortley Avenue and Timbrell Drive in 2023 and, as such have advised that any works to the intersection should be kept to a minimum.

This being the case, no upgrade of the signals are proposed except for the adjustment of the crossing point, stop line and sensor loop in Mortley Avenue to provide a wider pedestrian crossing as well as minor kerb realignment and widening of the existing pedestrian ramps on each pedestrian leg. TfNSW has provided Agreement in Principle for the TCS design.

The limited storage room for pedestrians and cyclists on the north west corner is also a risk given the expected significant increase in pedestrian and cyclist volumes. The current footpath is around 1m wide and with proposed upgrades, to accommodate turn movements, this maximum width that this can be increased to is around 1.5m.

To provide sufficient storage for pedestrians and cyclists on the north west corner of the intersection while the diversion route is in place, the left turn lane from the City West Link into Timbrell Drive would need to be closed for the duration of the works.

## PUBLIC CONSULTATION

Previous consultation on the *Cooks to Cove Greenway Master Plan* and *Cooks to Cove Concept Design* was undertaken from 2017 to 2019.

The detailed design plans for the Bay Run Upgrade and Diversion Route were public exhibited from 8 to 28 February 2021 on Council's Your Say website.

A notification letter was delivered to owners of properties and businesses within 100m of the proposed Bay Run upgrade works and also to properties immediately adjacent to the diversion route on 8 Feb 2021. A total of 217 letters were distributed. The closing date for submissions was 28 February 2020. Eight (8) responses were received.

UTS Haberfield Rowing Club, Le Montage, Inner West Bike Coalition, Greenway Steering Committee, Transport for NSW and Canada Bay Council were also contacted directly for comment.

Direct consultation was also held prior to the public consultation with Transport for NSW, Canada Bay Council, Jemena and UTS Haberfield Rowing Club.

A summary of the issues raised during and prior to the public exhibition is provided in the table below.

### **Bay Run Upgrade**

Issue	Number of Responses	Officer Comment
The pedestrian path width needs to be increased to allow for future increases	2	<p>The Bay Run path is proposed to be widened from a 3m shared path to a 4.8m visually separated path (2.4m + 2.4m) between UTS Haberfield Club and Lilyfield Road Bridge and a separated path (2.5m + 3.0m) across the Lilyfield Road Bridge.</p> <p>The Greenway path is proposed to be upgraded to a 3.5m wide shared path, consistent with previous upgrades in Richard Murden Reserve.</p>
Consider separated paths past the Rowers.	1	Due to pedestrians crossing from the carpark to the club and café in this area, a shared path is considered the most suitable solution to avoid conflict. The natural tendency will be for users to stay on their side of the shared path as they traverse from one separated path to the other. The existing pinch point will be widened to reduce conflict. Bespoke go-slow pavement markings are also proposed in this location.
<p>Need to retain parking in carpark servicing UTS Haberfield Rowers.</p> <p>Potential to modify existing landscaped areas at eastern end of carpark for more car spaces/trailer space etc.</p>	1	<p>UTS Haberfield club has previously provided advice that they are supportive of the upgrade and emphasised the need to retain the existing car parking. The proposed design results in the loss of one car parking space in the existing car park, but provides one additional loading zone space to facilitate deliveries to the club as well as to load/unload boat trailers. Under the road rules the loading zone would enable loading/unloading for up to 30mins</p> <p>The modification of the carpark at the eastern end would require removal of a number of trees which is not supported.</p>
Landscaping on the bridge deck needs to be planted in deeper soil, deep planter boxes or be subject to a regular watering program as droughts have often killed the bridge's current grass cover.	1	Structural considerations limit the amount of soil that can be used on the bridge. Planter boxes are proposed were feasible and an irrigation system is also proposed.
Consider access for Jemena maintenance vehicles to the pressure regulator at the western end of the Lilyfield Road.	2	Jemena has been consulted regarding the design. Access for maintenance vehicles will be retained along the relocated cycle path across the bridge. The existing arrangement around the pressure regulator will not change.
Bollards will be needed between the trees adjacent to the boat ramp if boat trailers are not to be parked on the grassed areas.	1	Access to the boat ramp will be restricted to authorised SES, TfNSW and Sydney Water vehicles only. No parking signs are proposed in the shared zone at the end of Lilyfield Road.
Reduced boat trailer parking and maneuvering area adjacent to the boat ramp, suggests that the boat ramp is not envisioned as a harbour access point.	1	Access to the boat ramp will be restricted to authorised SES, TfNSW and Sydney Water vehicles only.

Issue	Number of Responses	Officer Comment
It would better suit the UTS Club if the works were to be undertaken over the winter months.	1	Based on feedback from UTS the proposed works will be undertaken during the winter months.

## Diversion Route

Issue	Number of Responses	Officer Comment
A signalised pedestrian crossing on the eastern approach of the City West Link, Timbrell Dr, Mortley Ave intersection needs to be installed. Without this a three leg crossing for pedestrians and cyclists will be required and will add approx. 7 mins to a Bay run pedestrian trip and 5 mins to a cycle trip. That impact is far too severe for a sustained period.	3	Refer discussion in report. Introduction of an eastern pedestrian leg on the signalised intersection would remove the above risks and this is Councils preference. Transport for NSW were reluctant to provide the modelling for the proposed intersection upgrade or for Council to revisit the design of the intersection and as such no analysis of the intersection has been completed.  Consultation with Transport for NSW has indicated that they plan to upgrade the intersection of City West Link, Mortley Avenue and Timbrell Drive in 2021/22 and, as such have advised that any works to the intersection should be kept to a minimum.
The north west corner of the City West Link-Timbrell Drive intersection is dangerous for both cyclists and pedestrians due to the lack of space and the non-standard grade of the tram ramps	4	Refer discussion in report. Failing introduction of an eastern pedestrian leg, the left turn lane from the City West Link into Timbrell Drive would need to be closed for the duration of the works to provide sufficient storage for pedestrians and cyclists on this corner. Detailed traffic control plans including diversion signage will be undertaken as part of the construction Traffic Management Plan.
The signal crossings should be adapted to include crossing for cyclists. Installing a Cyclist Dismount sign is inconsistent with the principles of promoting active transport and (most likely) going to be ignored.	2	Consultation with Transport for NSW has indicated that they plan to upgrade the intersection of City West Link, Mortley Avenue and Timbrell Drive in 2021/22 and, as such have advised that any works to the intersection should be kept to a minimum.
Suggestion to use Dobroyd Lane as the cycling route diversion	2	Cyclists would be encouraged up Mortley Avenue, down Boomerang Street and along Crescent Street. It is acknowledged Dobroyd Lane provides somewhat flatter grades.  Detailed traffic control plans including diversion signage will be undertaken as part of the construction Traffic Management Plan and can consider use of Dobroyd Lane as suggested.

Issue	Number of Responses	Officer Comment
The width of the path through the 'park' in front of the along Dobroyd parade residences is insufficient. Consider some plastic/rubber matting to place on the grass to prevent slips, trips and falls in what is a poorly lit area.	1	The existing path is 2m wide which is considered sufficient as a temporary path for Bay Run pedestrians. Cyclists would be encouraged up Mortley Avenue, down Boomerang Street and along Crescent Street or Dobroyd Lane. It is acknowledged that some cyclists will prefer to use to the shared path route.  Commuter cyclists may also wish to utilise alternate routes such as Waratah Street.
Suggestion to make Dobroyd Pde, and Tillock Street north of Crescent Street closed to traffic with the exception of local residence. Making safe provisions for bikes and pedestrians to gain access inside the bollards is critical.	1	Temporary closure of the one-way link between Hawthorne and Dobroyd Parade is proposed for the duration of the works.  Detailed traffic control plans including diversion signage will be undertaken as part of the construction Traffic Management Plan and can consider local traffic only access signs as suggested.
Clear signage is required	1	Detailed traffic control plans including diversion signage will be undertaken as part of the construction Traffic Management Plan.

## CONCLUSION

With consideration of the above analysis and the outcomes of the community consultation, it is recommended that the detailed design plan be approved for construction.

## ATTACHMENTS

1. [Bay Run Upgrade Plans](#)
2. [Bay Run Upgrade Diversion Plans](#)
3. [Bay Run Diversion Overview](#)



## BAY RUN AND LILYFIELD BRIDGE UPGRADE

LILYFIELD RD, LILYFIELD, NSW, 2040

### CONSULTATION PLANS



--- SCOPE OF WORKS  
--- REFER DIVERSION ROUTE PACKAGE

#### DRAWING LIST

L_0000	COVER SHEET
L_2001	GA PLAN
L_2002	GA PLAN
L_2003	GA PLAN
L_2004	GA PLAN
L_2005	GA PLAN
L_2006	GA PLAN
L_2007	GA PLAN
L_2008	GA PLAN
L_2009	GA PLAN
L_2010	GA PLAN
L_2011	GA PLAN

Drawings  
A  
SCALE FOR INFORMATION  
DATE  
20/11/2020

Client  
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Litchfield

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architectural interior design urban design infrastructure  
new residential 3d rendering 360°

Project Title

BAY RUN AND LILYFIELD  
BRIDGE UPGRADE  
COVER SHEET

PRELIMINARY

Drawings/Revisions  
Drawing/Revision  
Date  
Author  
Checked  
Approved  
Date

Project No.  
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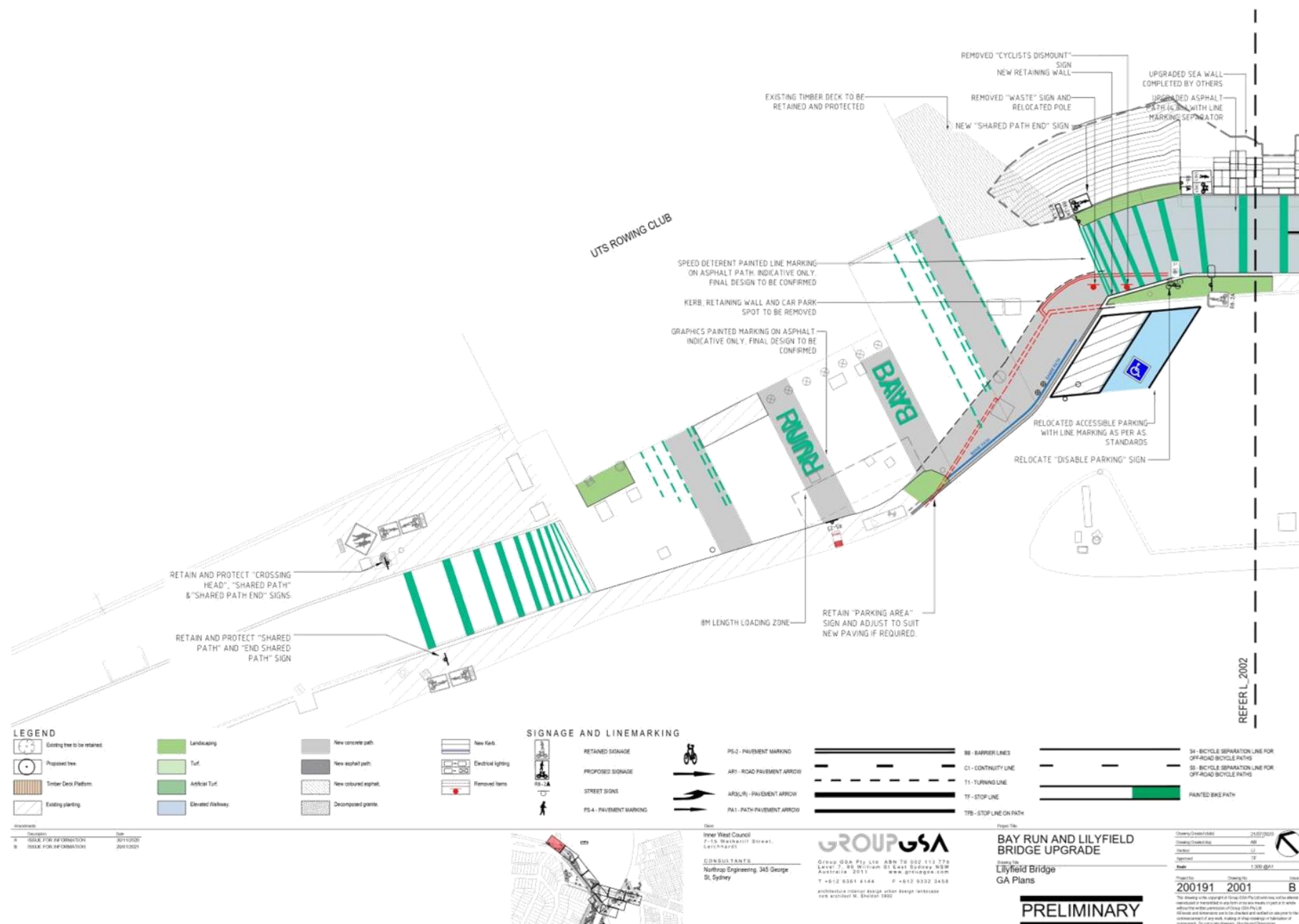
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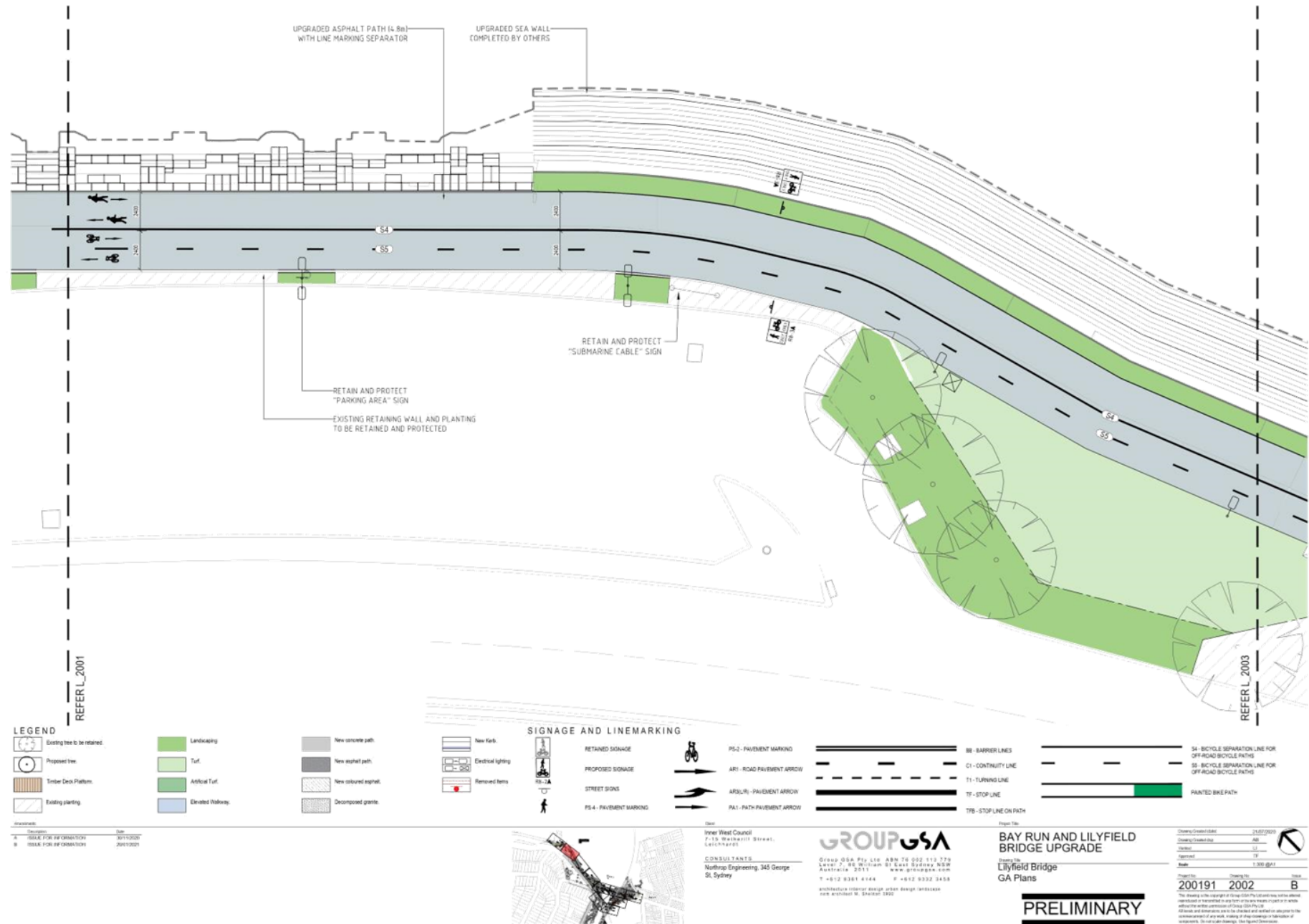
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GA Plans

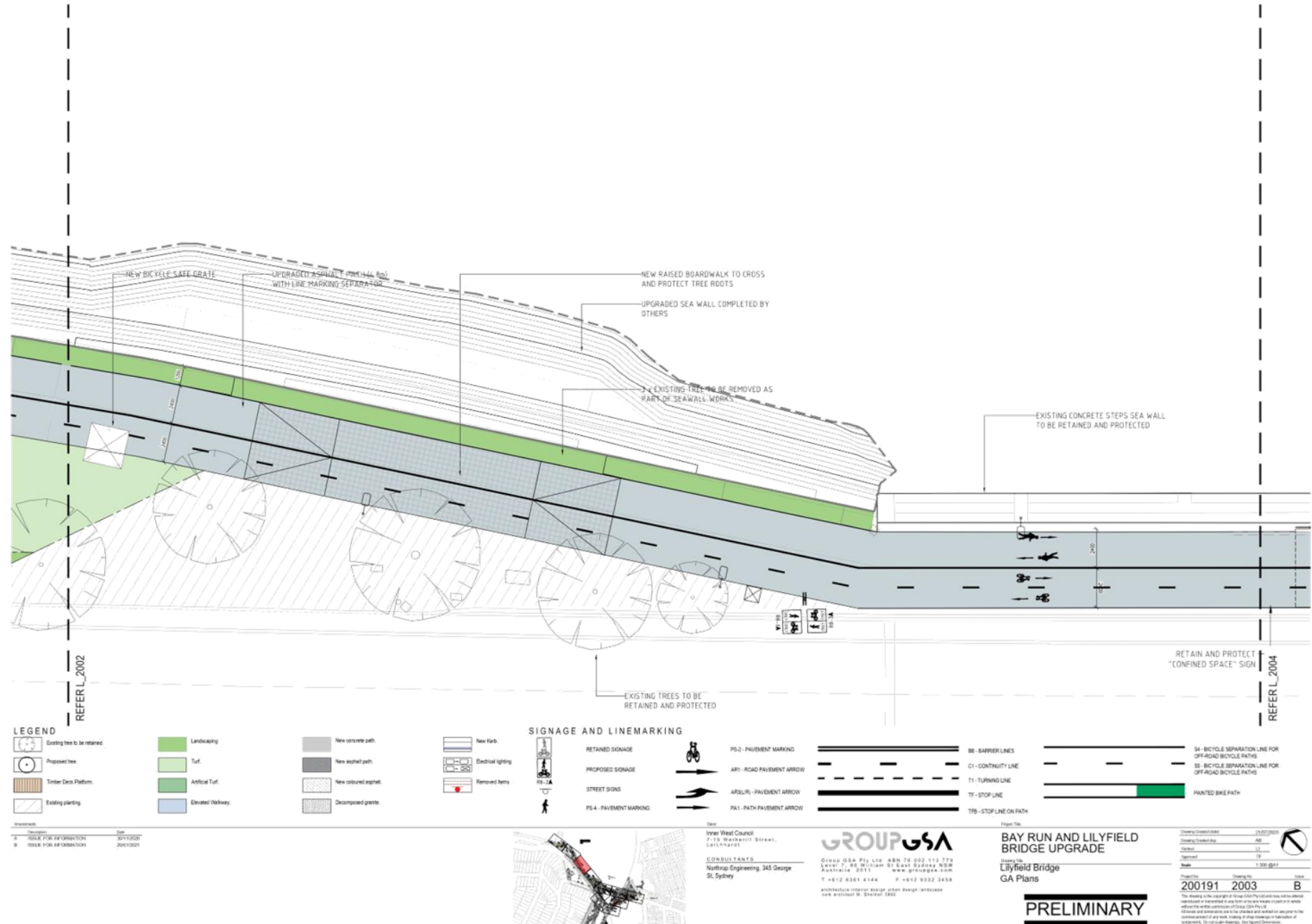
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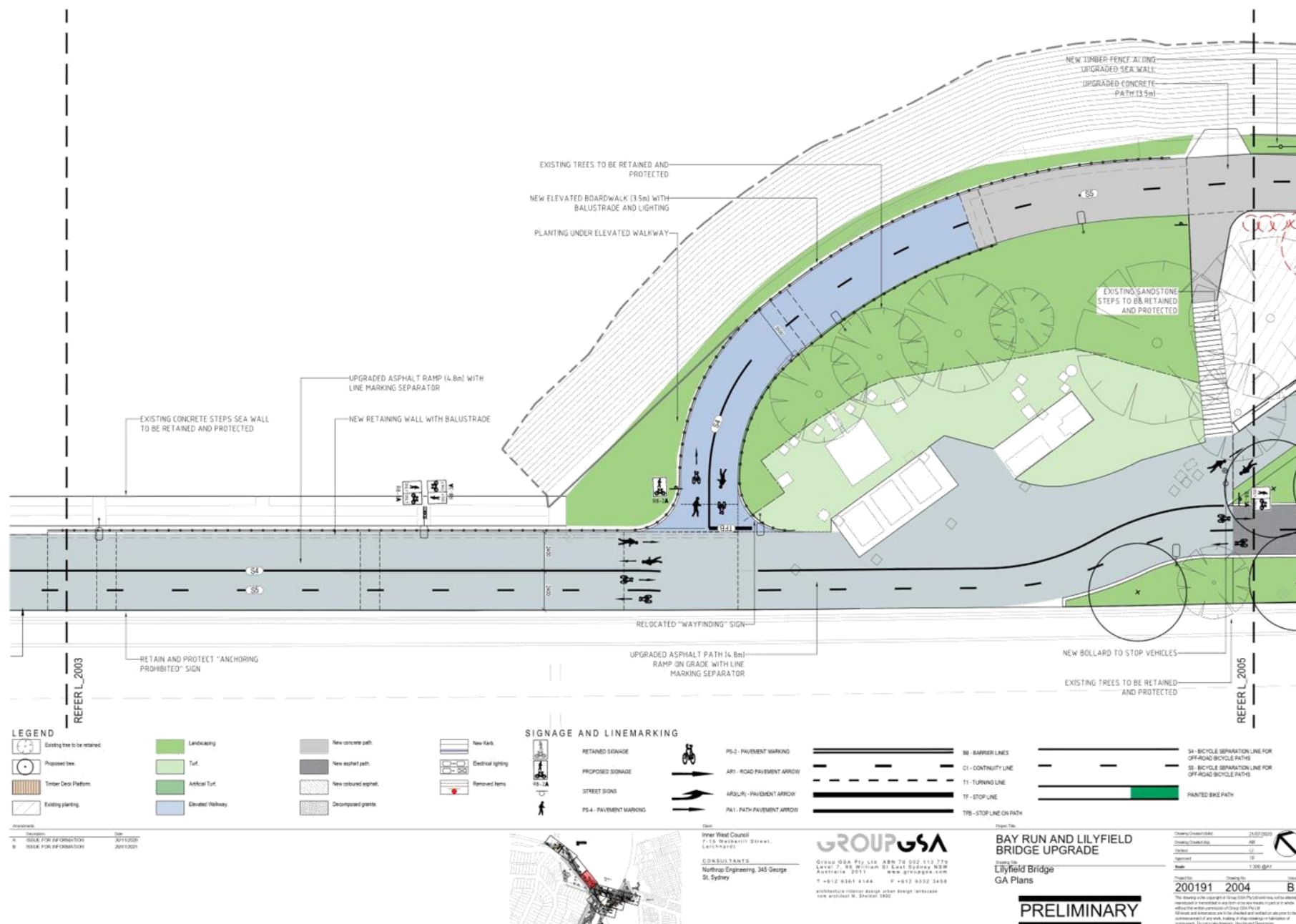
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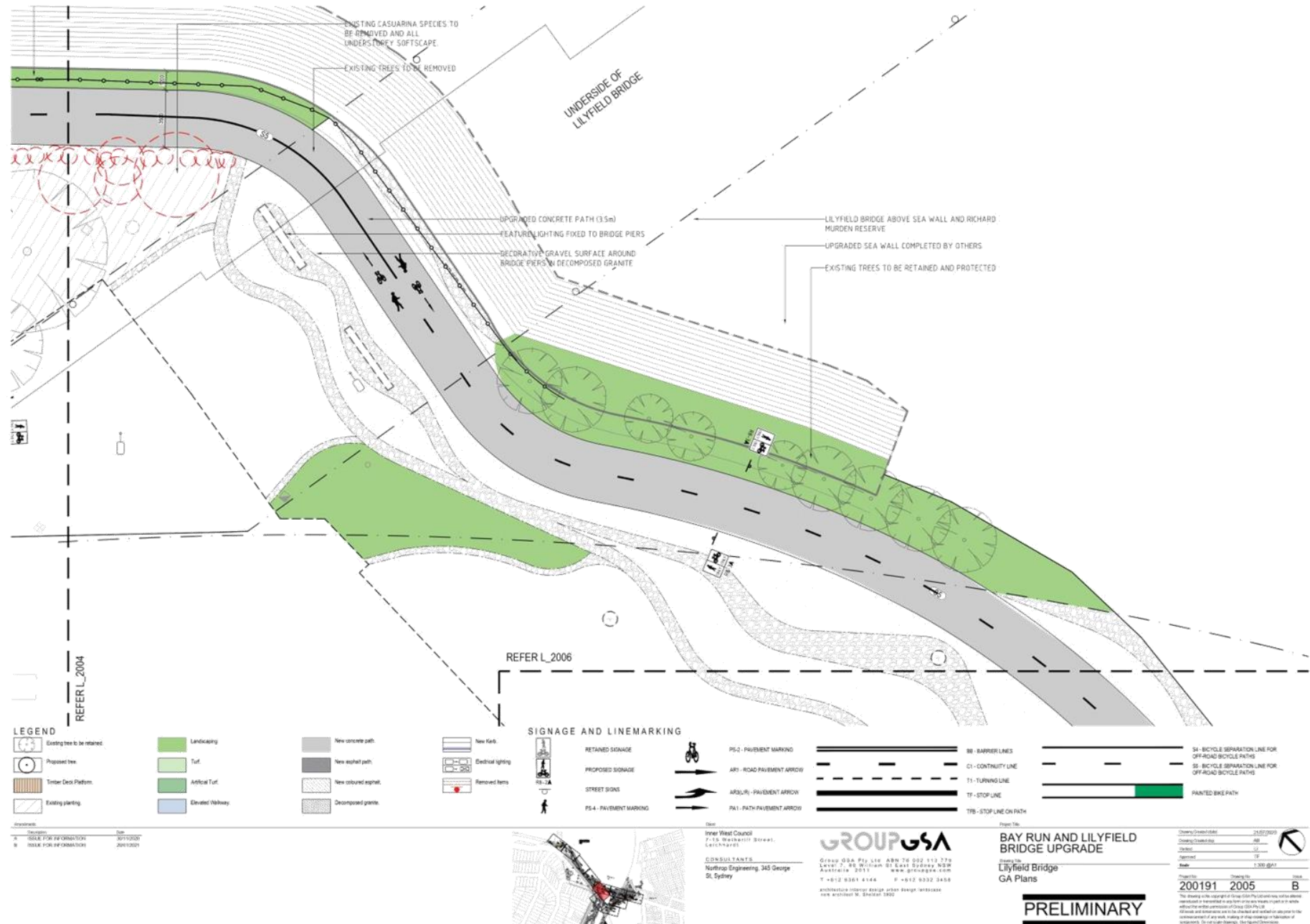




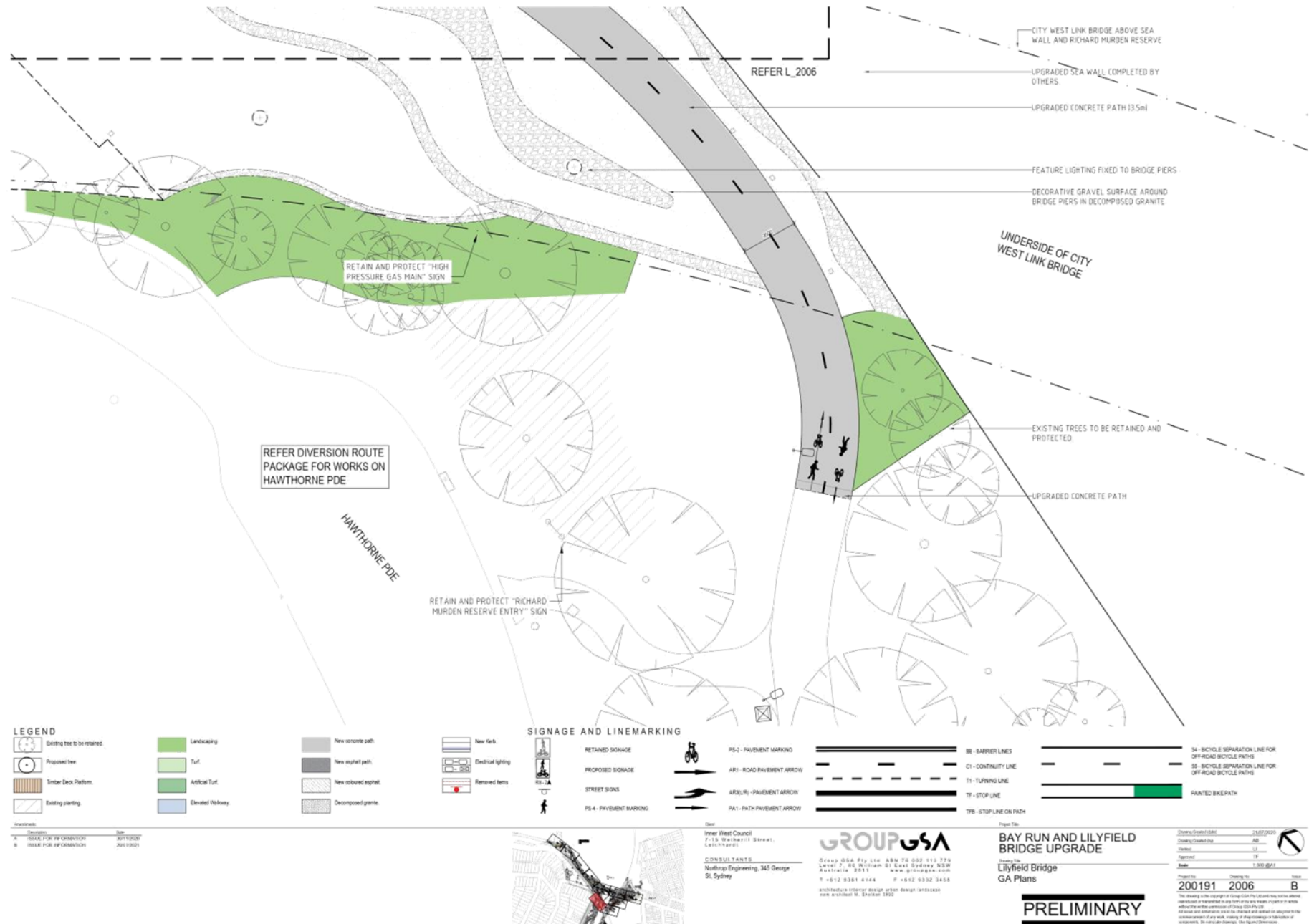


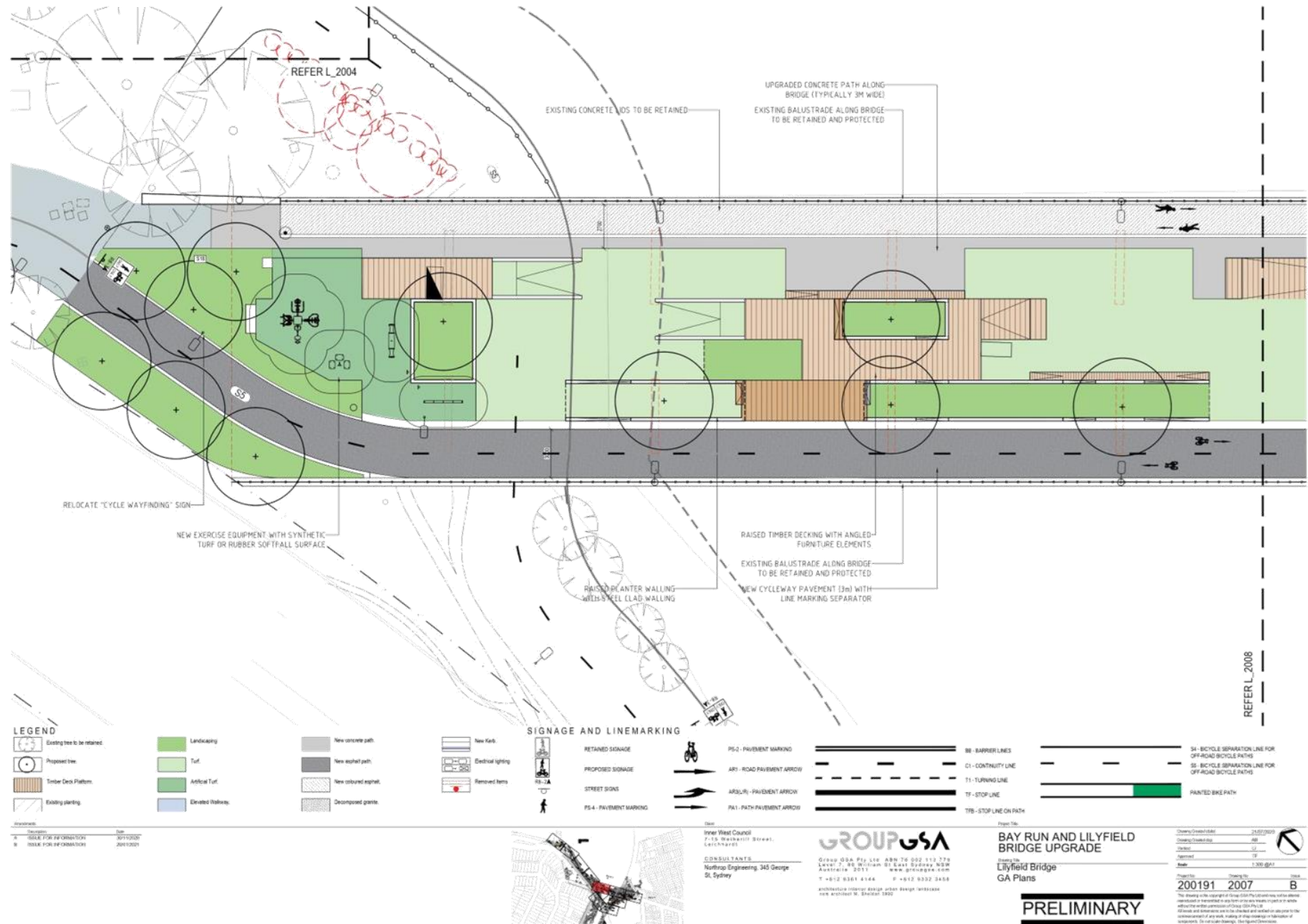


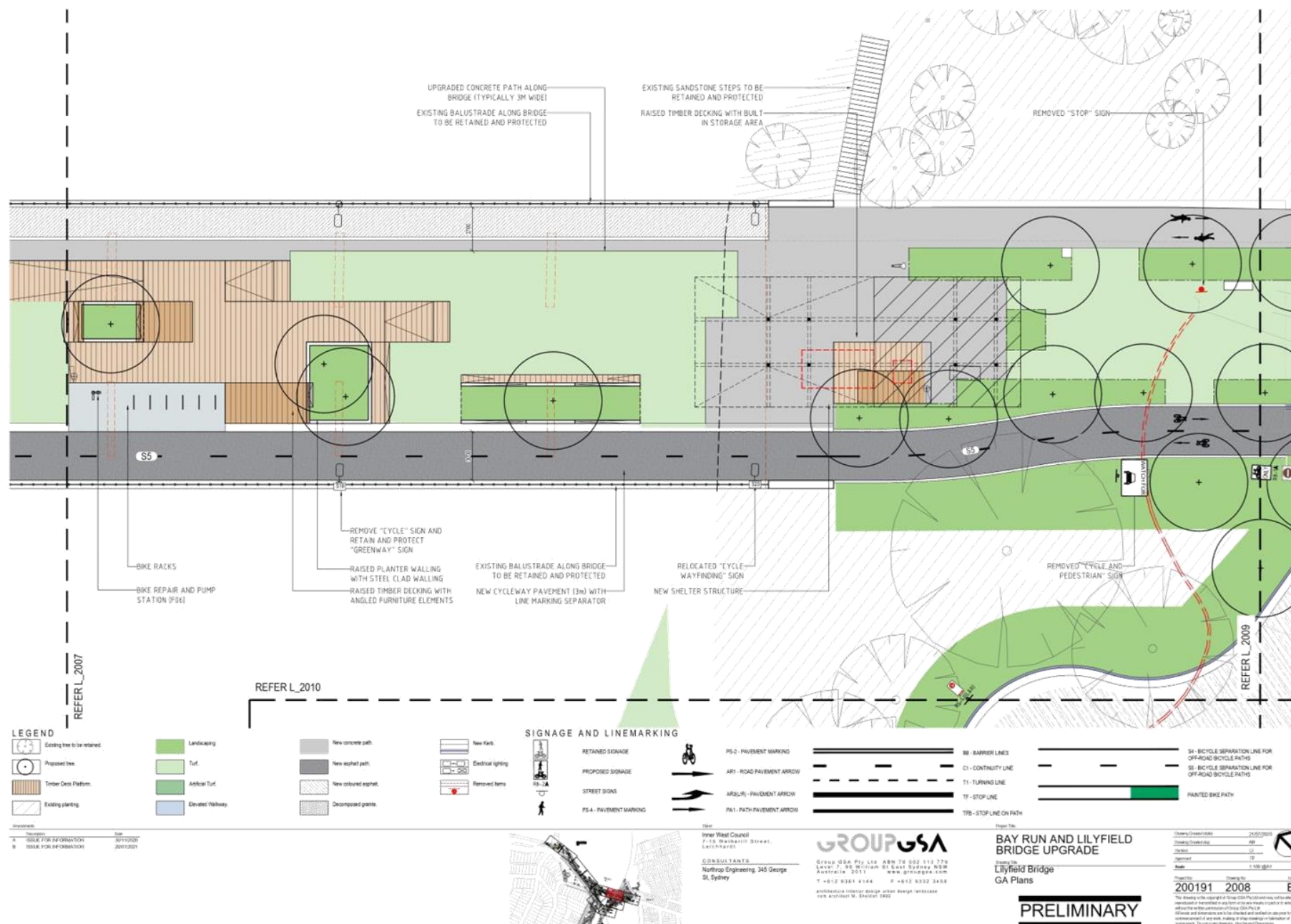




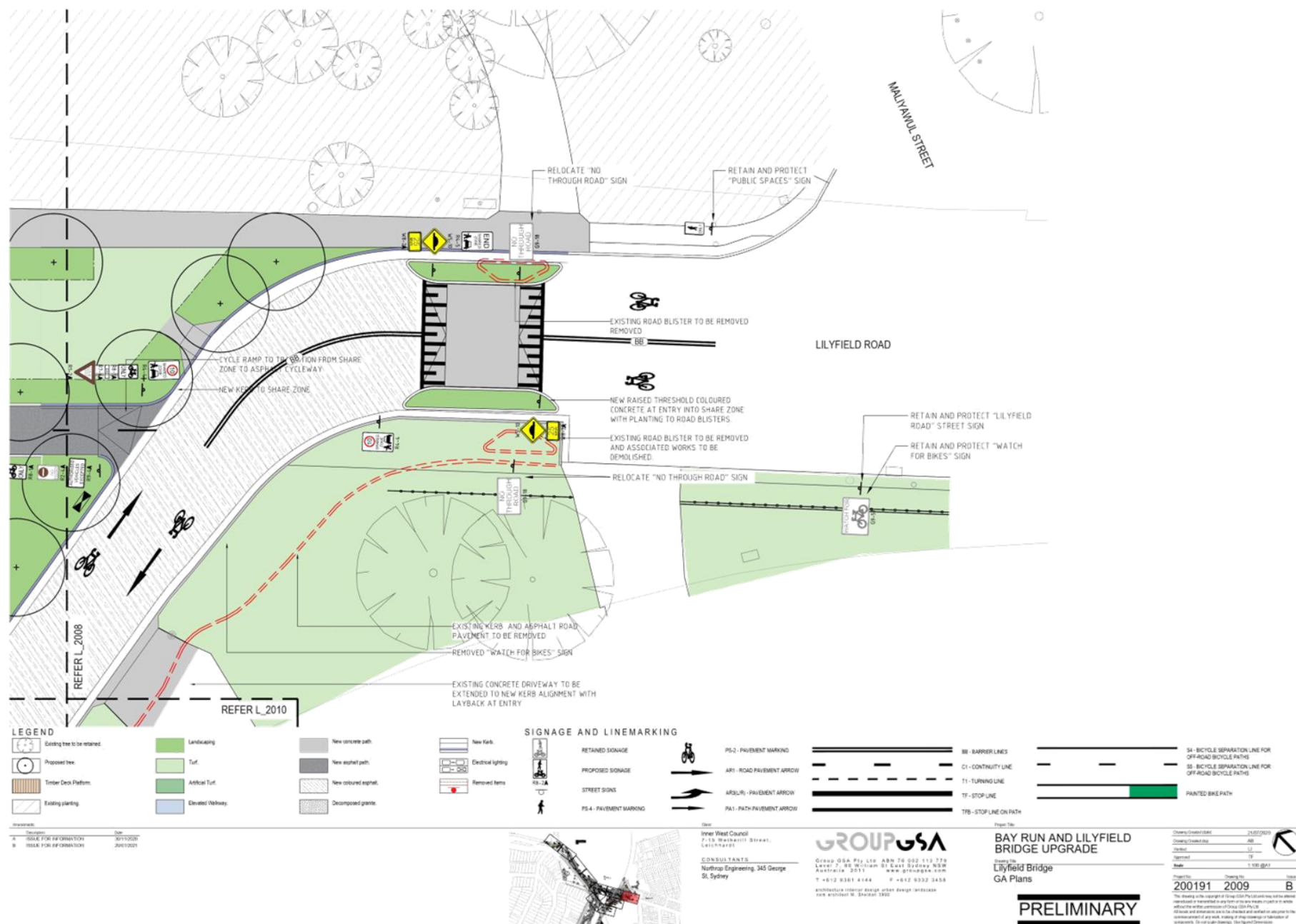


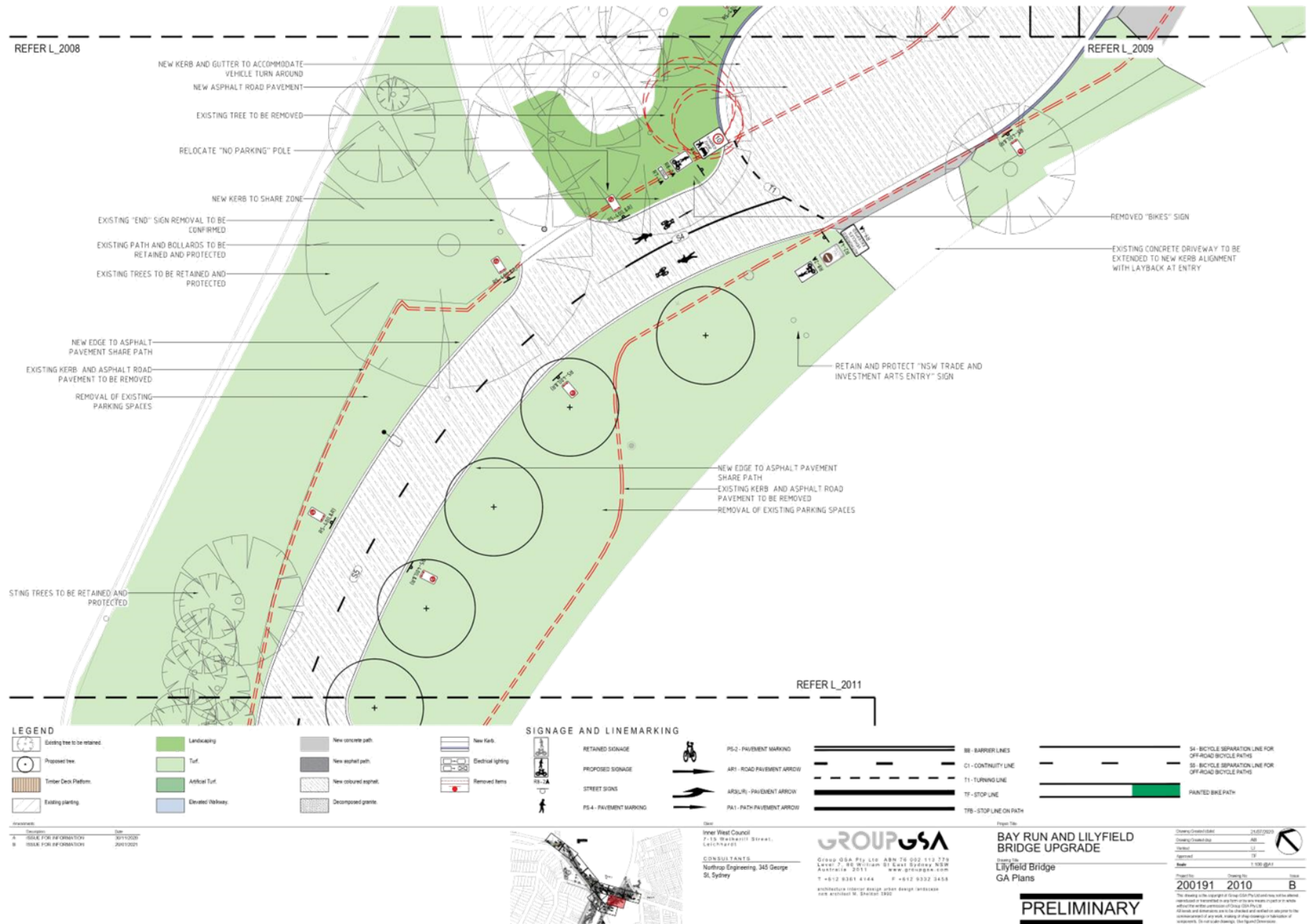


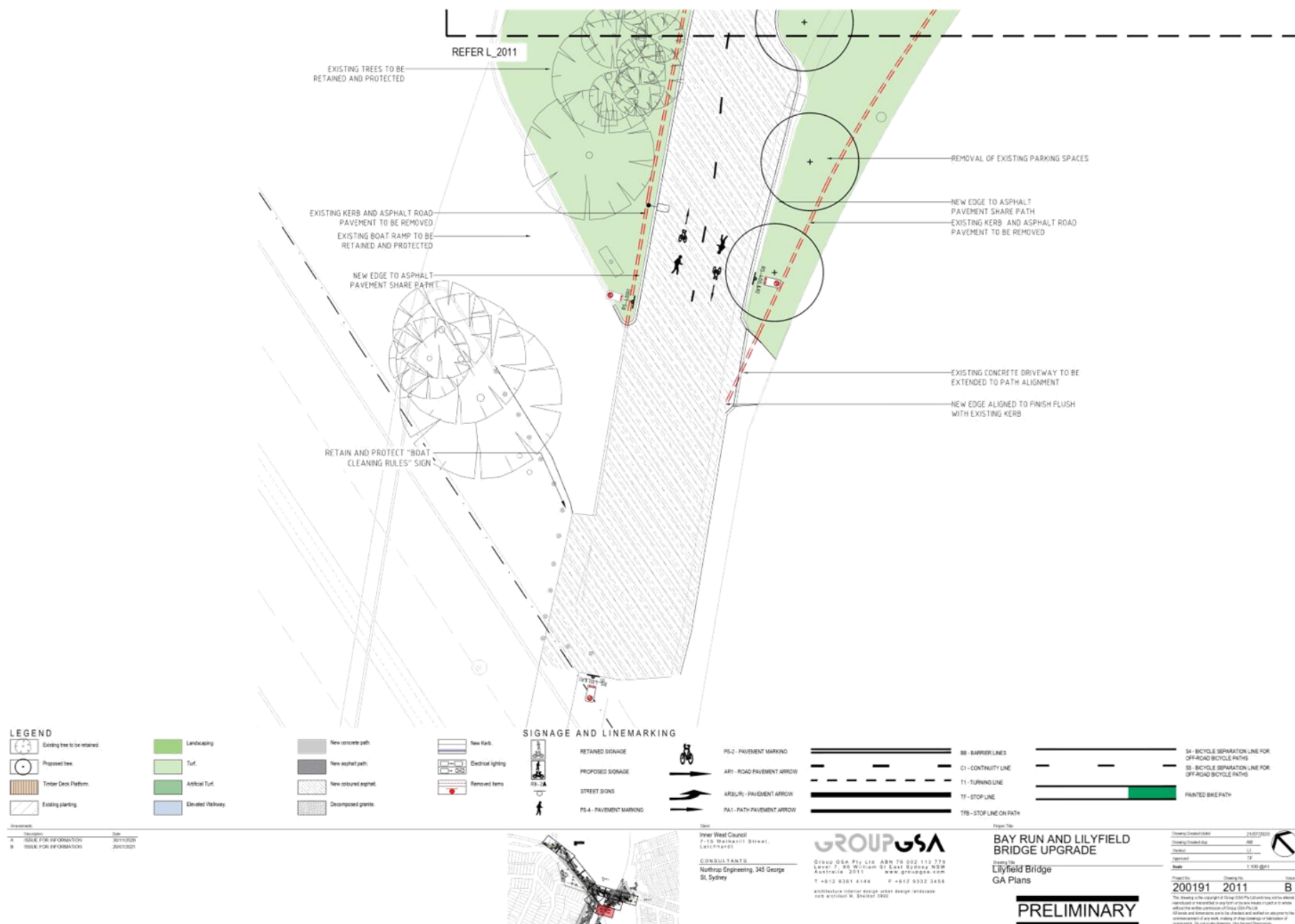








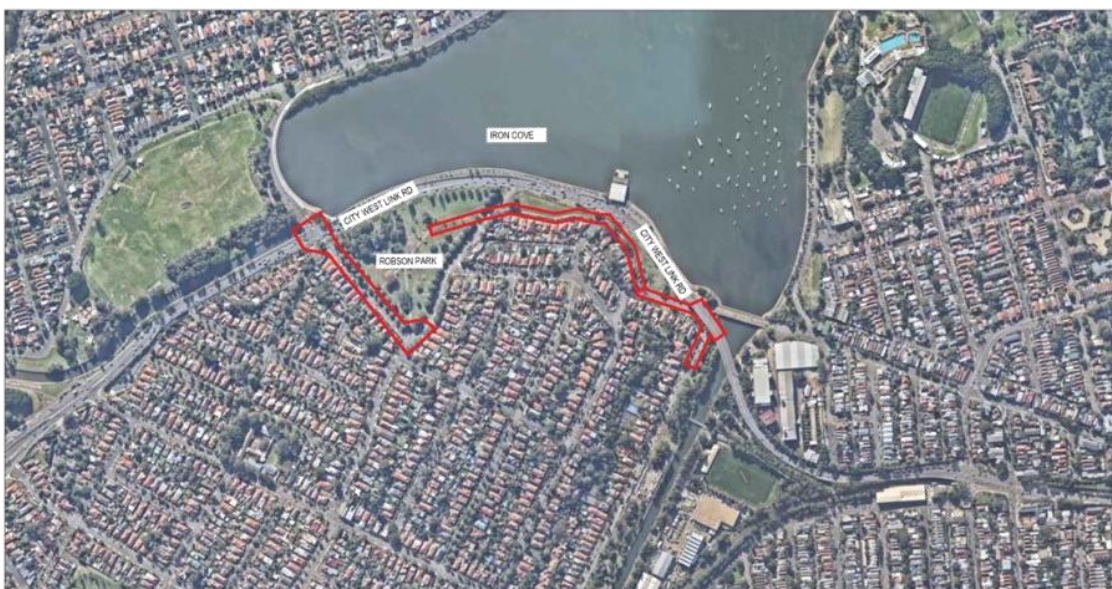






## BAY RUN AND LILYFIELD BRIDGE UPGRADE

### DIVERSION ROUTE UPGRADE CONSULTATION PLANS



SCOPE OF WORKS

#### DRAWING LIST

- 0000 COVER SHEET
- 0100 CONTEXT PLAN
- 2001 GA PLAN
- 2002 GA PLAN
- 2003 GA PLAN
- 2004 GA PLAN
- 2005 GA PLAN

Amendment	Issue Description	Date
A	ISSUE FOR INFORMATION	20/10/2020
B	ISSUE FOR INFORMATION	20/10/2021

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urban ecology & drainage

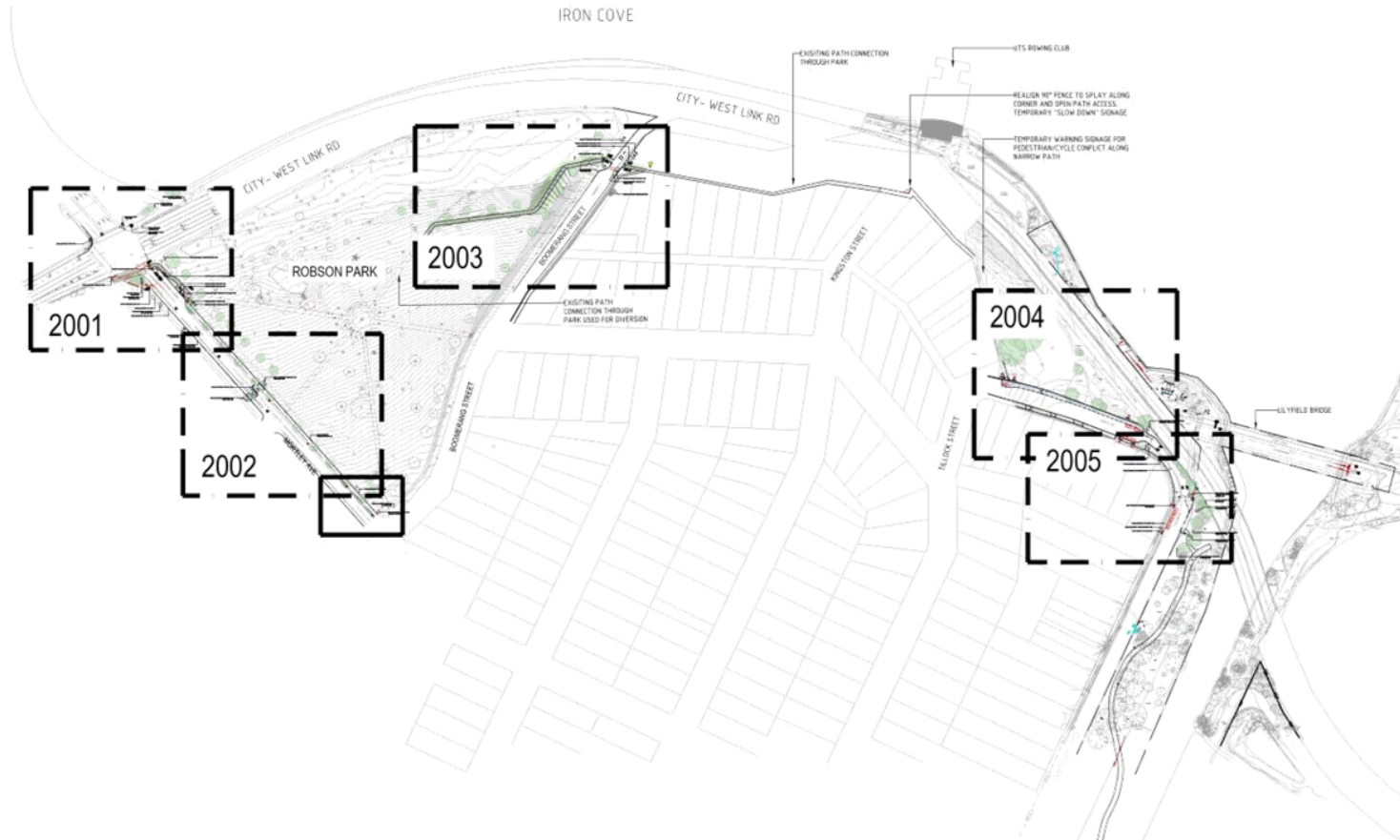
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BRIDGE UPGRADE**

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Amendment	Issue Description	Date
A	ISSUE FOR INFORMATION	20/10/2020
B	ISSUE FOR INFORMATION	20/01/2021

LEGEND	
	Existing tree to be retained
	Proposed tree
	Existing planting
	Landscaping
	Turf
	New concrete path
	New asphalt path
	Removed parking
	New kerb
	Removed kerb

SIGNAGE/LINEMARKING	
	RETAINED SIGNAGE
	PROPOSED SIGNAGE
	STREET SIGN
	PS4 - PAVEMENT MARKING
	PS2 - PAVEMENT MARKING
	AR1 - ROAD PAVEMENT ARROW
	AR3/AR1 - PAVEMENT ARROW
	PA1 - PATH PAVEMENT ARROW
	PAINTED BIKE PATH

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Project Title  
**BAYRUN AND LILYFIELD  
BRIDGE UPGRADE**

Drawing Title  
**LANDSCAPE PLAN  
GENERAL ARRANGEMENT  
PLAN**

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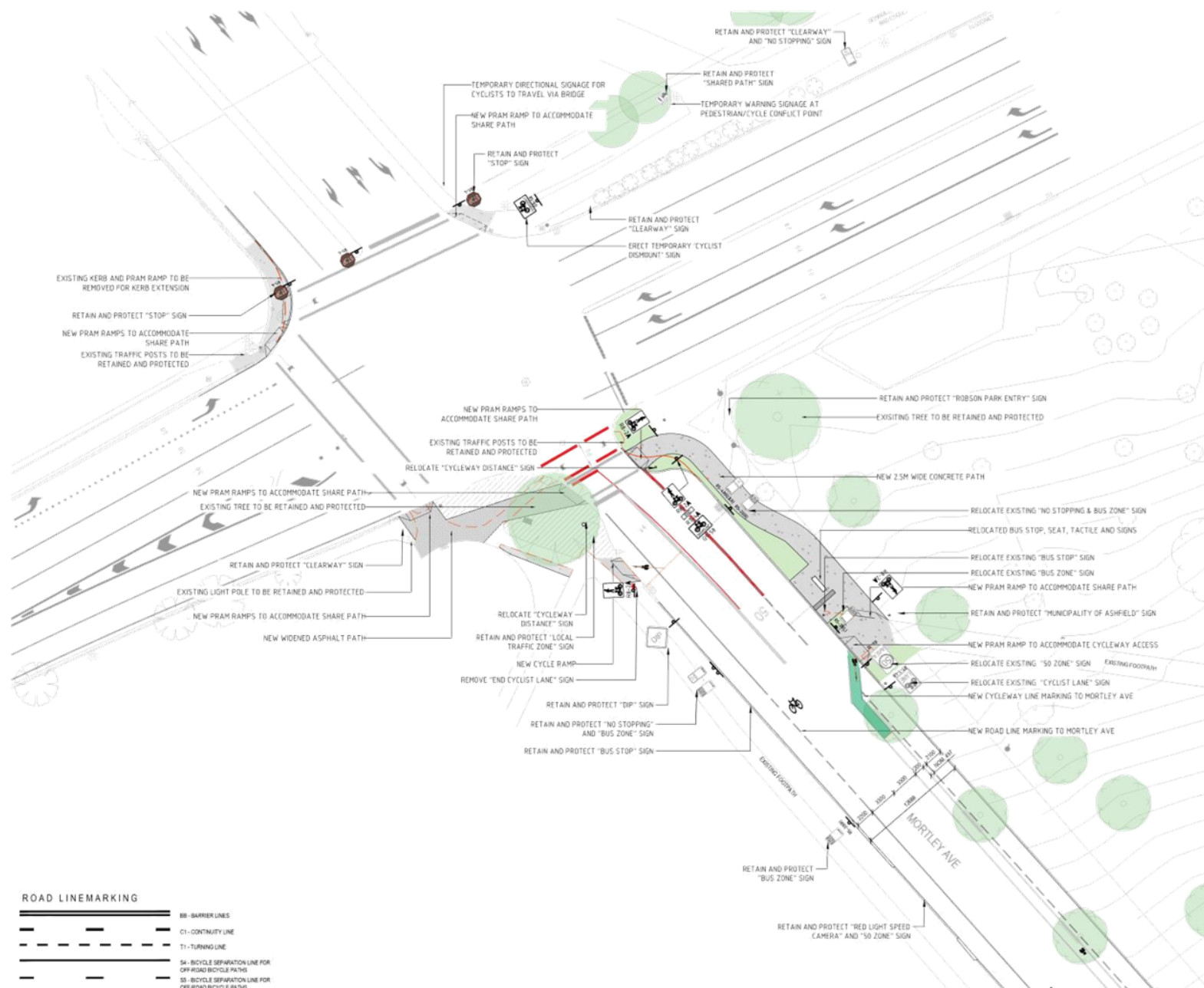
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**PROJECT TITLE**  
BAYRUN AND LILYFIELD  
BRIDGE UPGRADE

**DRAWING TITLE**  
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GENERAL ARRANGEMENT  
PLAN

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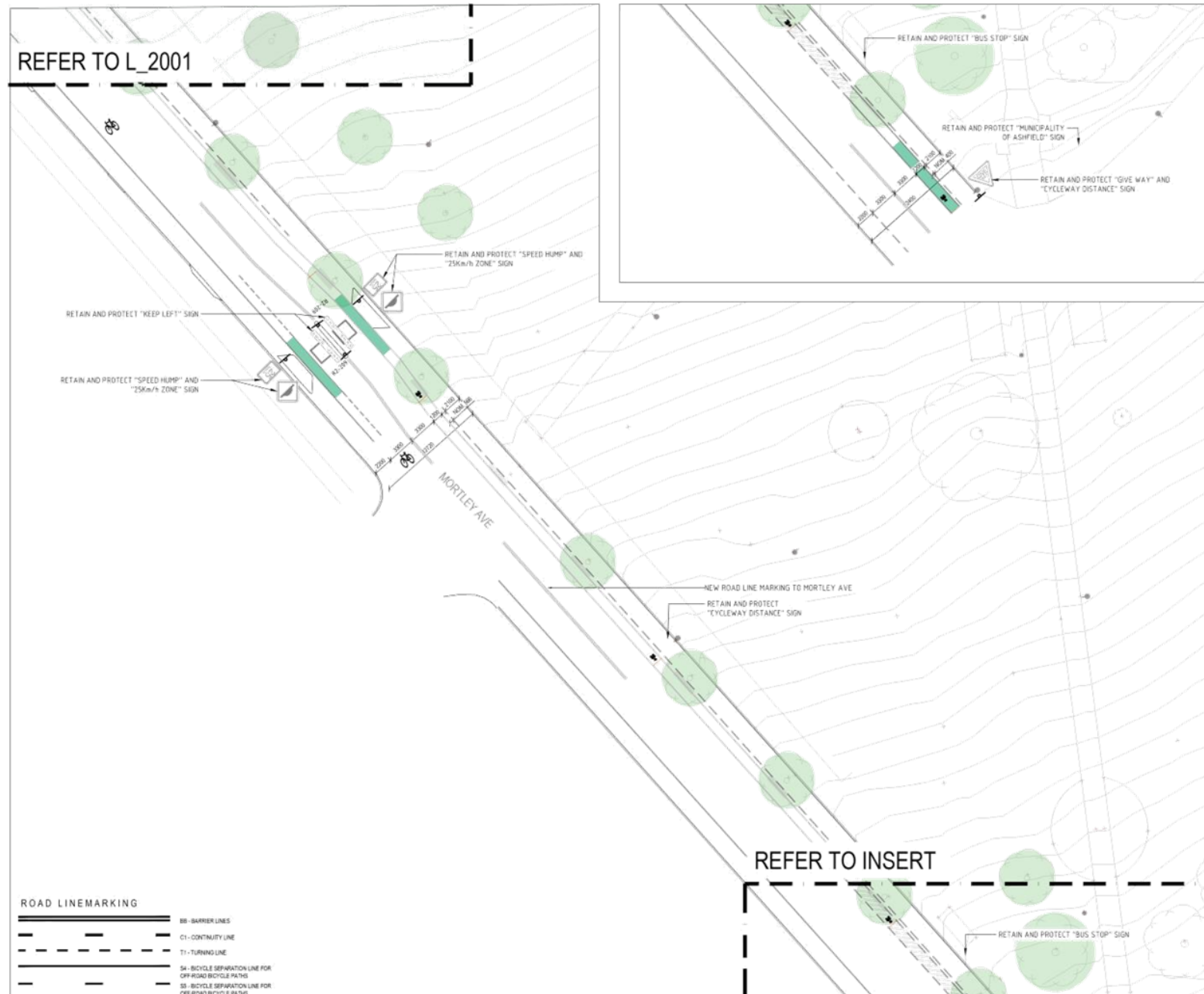
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Amendment	Issue Description	Date
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B	ISSUE FOR INFORMATION	20/05/2021

LEGEND	
	Existing tree to be retained
	Proposed tree
	Existing planting
	Landscaping
	Turf
	New concrete path
	New asphalt path
	Removed parking
	New Kerb
	Removed kerb

SIGNAGE/LINEMARKING	
	RETAINED SIGNAGE
	PROPOSED SIGNAGE
	STREET SIGNS
	PS-4 - PAVEMENT MARKING
	PS-2 - PAVEMENT MARKING
	AR1 - ROAD PAVEMENT ARROW
	AR3(L/R) - PAVEMENT ARROW
	PA1 - PATH PAVEMENT ARROW
	PAINTED BIKE PATH

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GENERAL ARRANGEMENT  
PLAN**

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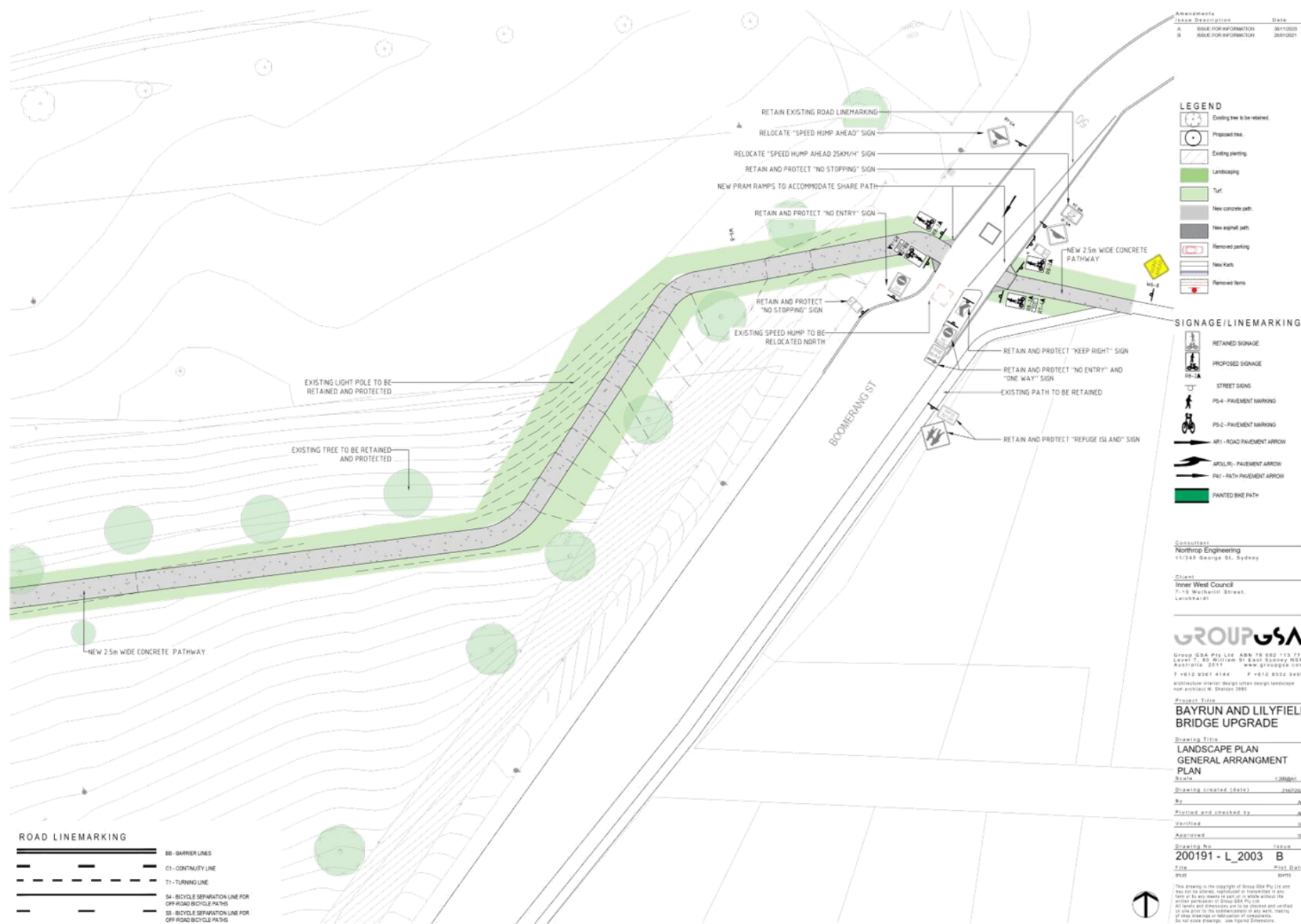
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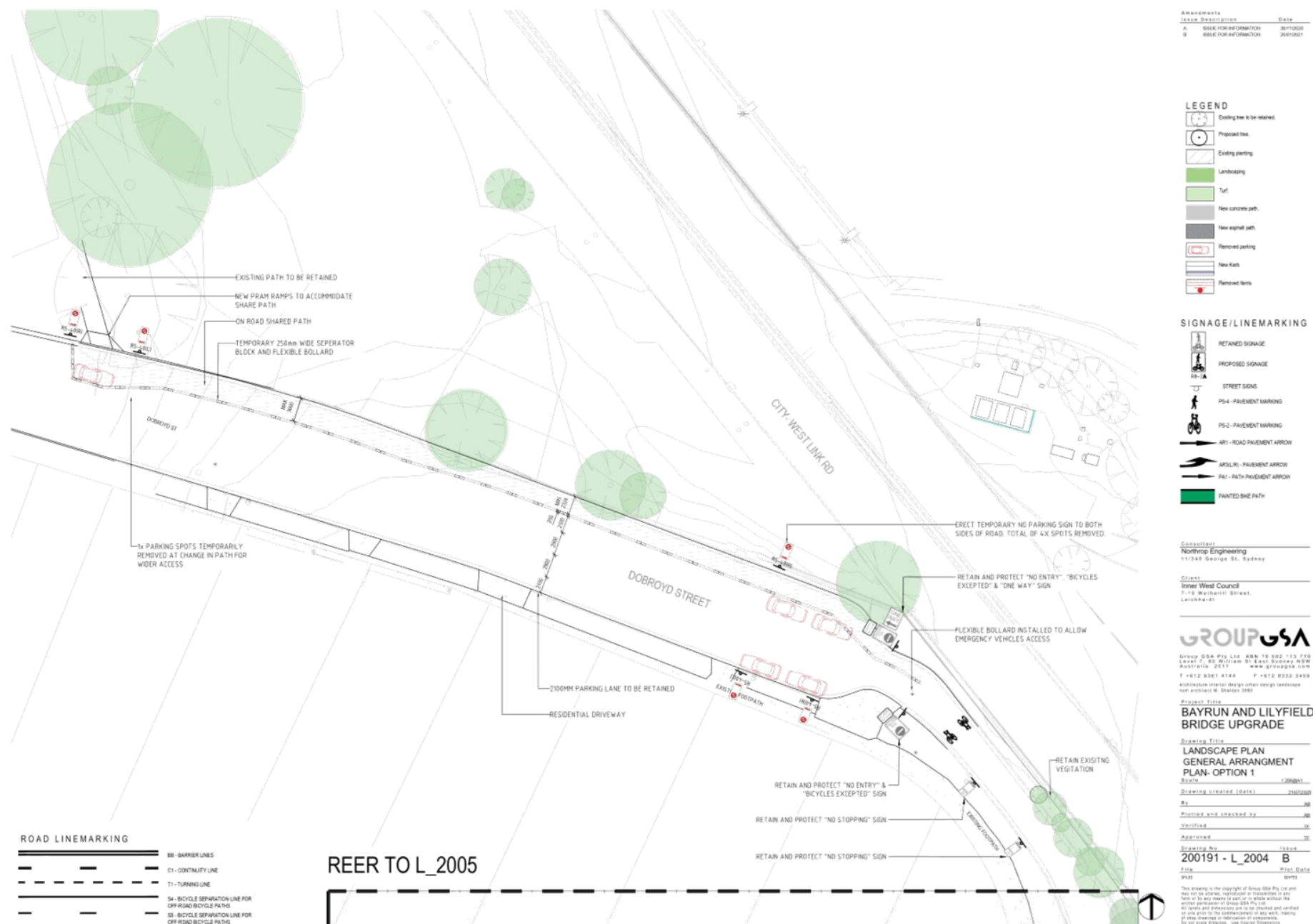
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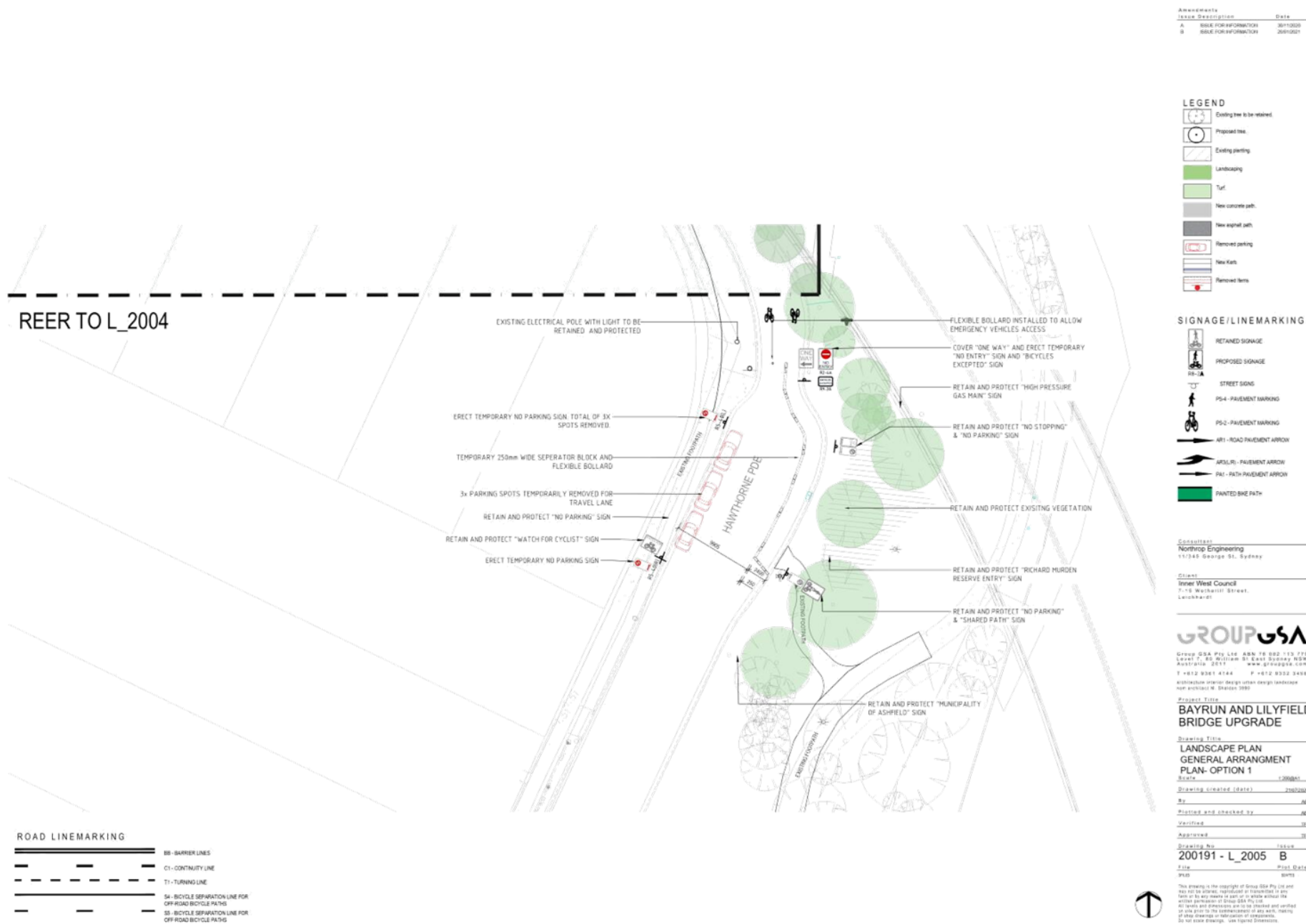
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**Item No:** LTC0521(1) Item 7

**Subject:** REVIEW- DETAILED WORKS (TRAFFIC & PARKING) IN TRAFALGAR STREET, PETERSHAM-RSL PETERSHAM DEVELOPMENT- DA201800173& IMPLEMENTATION OF REGIONAL BICYCLE ROUTE 7 (RR7).  
(STANMORE WARD- DAMUN/NEWTON ELECTORATE/INNER WEST PAC)

**Prepared By:** Boris Muha - Engineer – Traffic and Parking Services

**Authorised By:** Manod Wickramasinghe - Traffic and Transport Planning Manager

## SUMMARY

Council at its meeting on the 13 April 2021 resolved that the detailed works pertaining to the above subject matter go back to the Local Traffic Committee (LTC) for further review.

The detailed works were explained in the report to the Local Traffic Committee at its meeting on the 15 March 2021 (Item 3) with attachments and the recommendation below as supported by the Traffic Committee.

Concerns by Council were only raised with regards to recommendation point No. 5 of the Local Traffic Committee recommendation, that is, as to the need and necessity in applying permanent 'No Right Turn 3.00pm-7.00pm Monday-Friday' restrictions from New Canterbury Road into Regent Street with the affect and need to detour long distances around the area.

Further information is provided in this report in relation to recommendation point no. 5, including comments from TfNSW regarding this requirement.

## RECOMMENDATION

### THAT:

#### 1. The report be received and noted;

In reference to the report to the Local Traffic Committee meeting on 15 March 2021:

2. The detailed proposed traffic and parking works in Trafalgar Street and its intersection with Regent Street, Petersham, on attached drawing by Norton-Jago/ACE Pty Ltd- signage and line marking plan revision C Sheet 1 drawing No. C46.8, be APPROVED, subject to the following;
  - a) Double barrier BB lines be extended in Trafalgar Street right through across the RSL carpark access and Forzzard Lane;
  - b) An additional 'No Right Turn' sign be placed on the eastern footway side of the RSL carpark facing east bound traffic in Trafalgar Street;
  - c) The 'No Right Turn' as shown on the northern kerb side island in Trafalgar Street opposite the RSL carpark and Forzzard Lane, have a supplementary sign underneath reading 'INTO RSL CAR PARK & FOZZARD LANE';
  - d) The 'No Stopping (arrow right)' sign on the southern side of Trafalgar Street, to the west of Fozzard Lane be changed to read 'No Stopping (arrow left)' and the 'No Parking' restrictions be reinstated over the driveways between Fozzard Lane and no. 313-315 Trafalgar Street;
  - e) No trees be planted along the footway within the Bus Zone due to the likelihood of interference to the operation of the Bus Stop;
  - f) The builder prepare/provide for the bus pad/footings and other infrastructure works for the relocation of the bus stop and Council approved shelter as shown on the plan to the satisfaction of Council;

- g) The builder is to install all signs and line marking as per attached drawing by Norton-Jago/ACE Pty Ltd- signage and line marking plan revision C Sheet 1 drawing No. C46.8, including all modifications (a) to (f) as listed above to the satisfaction of Council;
3. The revised drawing C46.8 with modifications be submitted to TfNSW for its review and approval;
4. The temporary relocation of the Bus Stop on the southern side of Trafalgar Street, to a position further east of Regent Street between No. 273 and No. 265 Trafalgar Street be approved, subject to specific dates being provided to Council's Traffic & Transport Planning Manager and affected residents being notified at least two weeks prior to the changes; and
5. The Traffic Management Plan prepared by Barker Ryan Steward in Attachment 11 for the installation of 'No Right Turn 3.00pm-7.00pm Mon-Fri.,' from New Canterbury Road into Regent Street, be submitted to TfNSW for consideration and approval, and that the builder work with the TfNSW to install the required signs.

## BACKGROUND

The RSL development comprises of 3 sites, 2 with residential developments fronting Regent Street, Fisher Street and New Canterbury Road. The 3<sup>rd</sup> and major site being the new RSL Club with residential apartments fronts Trafalgar Street, Fisher Street and Regent Street.

Council at its meeting on the 28 April 2018 approved the final detail plans on the Regional Bicycle Route 7 (RR7). The detailed bicycle plans and adjacent works to the RSL were provided in the report to the Local Traffic Committee meeting of the 15 March 2021.

## FINANCIAL IMPLICATIONS

All cost of works required under development consent will be borne by the developer.

## OTHER STAFF COMMENTS

The part-time afternoon peak 'No right turn' restrictions form part of the development consent for the RSL sites under the requirement of TfNSW to prevent the likelihood of excessive queuing of vehicles back to the traffic signals at Crystal Street. This would be on account of increased right turn movements from New Canterbury Road into Regent Street, from traffic generated from the development. The traffic peak hour and the RSL peak hours occur at approximately the same time.

The Traffic Management Plan provided by Barker Ryan Steward (again shown here as **Attachment 1**) addresses the modelling performance of the intersection and quotes the following:

*'The results above show that the main impact of removing the right turn movements from New Canterbury Road to Regent Street is to reduce the 95 % queue on the east approach from 193 metres to 125 metres.*

*The reduced queue length will improve the performance of the New Canterbury Road / Crystal Street intersection which is 120 metres east of Regent Street. The reduced queue lengths will reduce the incidence of westbound vehicles on New Canterbury Road queuing back through the Crystal Street intersection and creating residual queues that reduce intersection performance.*

Clearway restrictions (3.00pm-7.00pm Mon-Fri) exist on the southern side of New Canterbury Road. The PM peak 'No right turn' restrictions together with the clearway restrictions would

also allow for unimpeded, safe and proper efficient 2 lane traffic flow through New Canterbury Road.

The “No Right Turn’ into Regent Street from New Canterbury Road also serves as a road safety measure as the additional right turning vehicles in peak hour would increase the risk of accidents at this location as well as further upstream due to the queuing affect. TfNSW have provided the following comments:

- The condition for the “No Right Turn 3pm-7pm Mon-Fri” was incurred on the development in the Planning Proposal stages
- With motorists waiting to turn right from New Canterbury Road onto Regent Street, the queues can extend back to the traffic signals at Crystal Street, where motorists may queue over the pedestrian crossing, impacting safety of pedestrians.
- additional queuing can lead to added frustration and incur further illegal manoeuvres from motorists.
- Limiting this right turn will also reduce the number of conflicting movements at this intersection. Where at other times, motorists are turning right out of Regent Street as well as those turning right into Regent Street.

Council Officers and TfNSW have also noted that detours for the duration of the timed right turn ban are also safer. Motorists that wish to enter Regent Street from New Canterbury Road may instead turn right at the intersection of Crystal Street, then turn left onto Trafalgar Street and then access Regent Street. This is much safer, as there is a dedicated phase for westbound motorists on Stanmore/New Canterbury Road to make the right turn onto Crystal Street, without facing oncoming traffic. Furthermore, there are also other alternatives by left turn at Shaw Street, right turn into Chester Street (or Oxford Street), right turn into Audley Street (across New Canterbury Road at the traffic signals) and right turn Fisher Street to access Regent Street. These two detour movements will not add significant delay to the trip and are not considered tedious.

## PUBLIC CONSULTATION

A general notification letter was mailed out to the approx. 290 residing residents/property owners/businesses advising on all traffic and parking works, including that of the ‘No right turn 3.00pm-7.00pm Mon-Fri’ at the intersection of New Canterbury Road and Regent Street. Residents were given the opportunity to comment. Only (1) response was received concerning the No Right turn, however the resident was advised of the alternate routes available.

## ATTACHMENTS

1. [Traffic Management Plan- No right turn 3.00pm-7.00pm Mon-Fri.](#)



PLANNING  
PROJECT MANAGEMENT  
ENGINEERING  
CERTIFICATION

Our Ref: SY150139

21 January 2021

Ms Poonam Chauhan  
Deicorp Pty Ltd  
161 Redfern Street  
REDFERN NSW 2010

Dear Poonam,

## Petersham RSL Public Domain Works, New Canterbury Road / Regent Street, Petersham

This letter is provided in response to Inner West Council's requirements for a Traffic Management Plan (TMP) for public domain works.

Inner West Council has forwarded the following requirement from Transport for NSW regarding the restriction of right turns from Canterbury Road into Regent Street.

*"TfNSW (Network and Safety) advise of the need to provide No Right Turn PM peak hours from New Canterbury Road into Regent Street. RSL traffic generation will exacerbate problems due to right turning into Regent Street with motorists queuing back to the traffic lights." (at Crystal Street).*

*"The proponent will need to provide and show on inset plan NRT (PM) peak signs on the southern side of New Canterbury Road and NW corner of New Canterbury Road and Regent Street. The proponent would need to provide a TMP to address this No Right Turn proposal."*

*"No action to No Right Turn from Regent Street to New Canterbury Road."*

As part of the development application process for the Petersham RSL proposal, Barker Ryan Stewart conducted network modelling using Sidra software to assess the impacts of traffic generated by the proposed development on the surrounding road network. The intersections included in the network modelling were:

- Trafalgar Street / Audley Street;
- Trafalgar Street / Regent Street;
- Trafalgar Street / Crystal Street;
- New Canterbury Road / Audley Street;
- New Canterbury Road / Regent Street; and
- New Canterbury Road / Crystal Street.

The locations of these intersections in relation to the proposed development are shown below in Figure 1.

SYDNEY  
P (02) 9659 0005  
E [sydney@brs.com.au](mailto:sydney@brs.com.au)

CENTRAL COAST  
P (02) 4325 5255  
E [coast@brs.com.au](mailto:coast@brs.com.au)

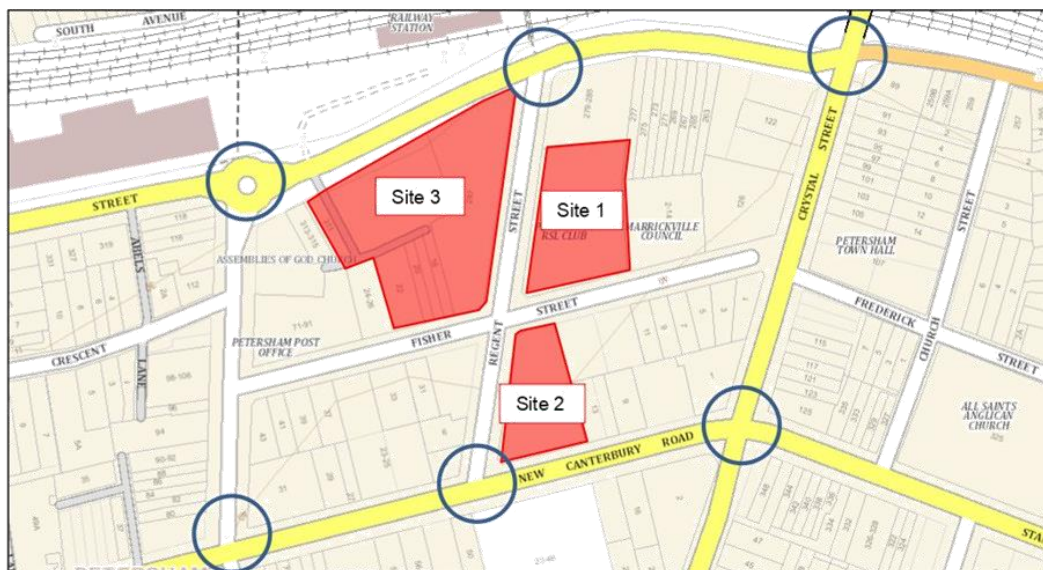
HUNTER  
P (02) 4966 8388  
E [hunter@brs.com.au](mailto:hunter@brs.com.au)

SOUTH EAST QUEENSLAND  
P (07) 5582 6555  
E [seql@brs.com.au](mailto:seql@brs.com.au)

[brs.com.au](http://brs.com.au)

SY150139 Public Domain Works New Canterbury Road And Regent St.Docx





**Figure 1:** Road network surrounding the Petersham RSL development

The network modelling has been revised to assess the impact of removing right turns from New Canterbury Road into Regent Street and the post development results for this intersection are shown in the table below.

Approach	Performance Measure	Post Development	Post Development NRT
New Canterbury Road (East)	Deg Satn	0.376	0.324
	Average Delay Sec)	11.4	0
	LoS	A	A
	95% Queue (m)	193.2	125.1
Regent Street	Deg Satn	0.723	0.611
	Average Delay Sec)	126.6	90.4
	LoS	F	F
	95% Queue (m)	9.6	7.7
New Canterbury Road (West)	Deg Satn	0.22	0.22
	Average Delay (Sec)	4.6	6.8
	LoS	A	A
	95% Queue (m)	40	49.9

The results above show that the main impact of removing the right turn movements from New Canterbury Road to Regent Street is to reduce the 95 % queue on the east approach from 193 metres to 125 metres. The reduced queue length will improve the performance of the New Canterbury Road / Crystal Street intersection which is 120 metres east of Regent Street. The reduced queue lengths will reduce the incidence of westbound vehicles on New Canterbury Road queuing back through the Crystal Street intersection and creating residual queues that reduce intersection performance.

As specified in the advice received from TfNSW, "No Right Turn" signs (R2-6n\_r) and supplementary plates "3pm - 7pm Mon - Fri" should be installed on the southern side of New Canterbury Road opposite Regent Street and on the north-west corner of the New Canterbury Road / Regent Street intersection.

The required signs and their proposed locations are shown in Figure 2 below.



Figure 2: Location of No Right Turn Signs

Should you require any further information regarding this Traffic Management Plan, please do not hesitate to contact me.



Robert Day | Traffic and Transport Manager  
Barker Ryan Stewart Pty Ltd

SY150139 Public Domain Works New Canterbury Road And Regent St.Docx



**Item No:** LTC0521(1) Item 8

**Subject:** TRAFALGAR LANE, ANNANDALE - PROPOSED 'NO PARKING' RESTRICTIONS (BALUDARRI-BALMAIN/BALMAIN ELECTORATE/LEICHHARDT PAC)

**Prepared By:** Vinoth Srinivasan - Engineer - Traffic and Parking Services

**Authorised By:** Sunny Jo - Coordinator Traffic Engineering Services (North)

## SUMMARY

Council has received concerns from a resident of Trafalgar Street, Annandale regarding vehicles parking on the eastern side of Trafalgar Lane, Annandale and subsequently obstructing rear driveway access for properties No.195 and No.197 Trafalgar Street, Annandale. An investigation has now been completed and is presented in this report.

## RECOMMENDATION

**THAT a 11.1m 'No Parking' zone be installed on the eastern side of Trafalgar Lane between the rear access driveways of No.222 and No.226 Nelson Street and opposite to the rear garage of No.195 and No.197 Trafalgar Street, Annandale.**

## BACKGROUND & OTHER STAFF COMMENTS

Council has received concerns from a resident of Trafalgar Street, Annandale regarding vehicles parking on the eastern side of Trafalgar Lane, Annandale and subsequently obstructing rear driveway access for properties No.195 and No.197 Trafalgar Street, Annandale.

In order to alleviate this issue, it is proposed to install to install a 11.1m 'No Parking' zone in the eastern side of Trafalgar Lane between the rear access driveways of No.222 and No.226 Nelson Street and opposite the rear garages of No.195 and No.197 Trafalgar Street, Annandale.

The proposal is shown on the following plan.



## FINANCIAL IMPLICATIONS

Nil.

## PUBLIC CONSULTATION

A letter outlining the above proposal was mailed out to the affected properties (8 properties) in Trafalgar Street and Nelson Street, Annandale.

3 responses were received with 1 in general support with request for changes and 2 in objection.

The main concerns raised by the residents are outlined below:

- Due to the steep incline of the properties on Nelson Street, we use this space at the rear of our property on Trafalgar Lane to gain access to our property.
- Tradesmen and visitors of Nelson Street residents use this kerb space to park their vehicle.
- A household member has a mobility issue and uses this space for easier access into our property on Nelson Street.
- The proposed 'No Parking' restrictions on the eastern side of Trafalgar Lane needs to be extended further southwards to cover the kerb space opposite the rear of No.191 Trafalgar Street as parked vehicles make it difficult to exit my property.

Council officers have provided the following response in regard to the residents' concerns:

- The 'No Parking' restrictions are required to provide unobstructed access to off-street parking within the laneway which is one of the primary functions of a lane.
- Residents of Nelson Street with mobility issues are eligible to apply for mobility parking permits and associated mobility parking space applications, and these are considered in accordance to Council's Public Domain Parking Policy.
- The installation of 'No Parking' restrictions on the eastern side of Trafalgar Lane, opposite No.191 Trafalgar Street will be investigated as a separate matter as any potential loss of on-street parking will need to be consulted with affected residents.

## ATTACHMENTS

Nil.